

# Kingborough



## COUNCIL MEETING AGENDA

NOTICE is hereby given that an Ordinary meeting of the Kingborough Council will be held in the Kingborough Civic Centre, 15 Channel Highway, Kingston on  
Monday, 2 March 2026 at 5.30pm

# Kingborough Councillors 2022 - 2026



**Mayor**  
**Councillor Paula Wriedt**



**Deputy Mayor**  
**Councillor Clare Glade-Wright**



**Councillor Aldo Antolli**



**Councillor David Bain**



**Councillor Gideon Cordover**



**Councillor Kaspar Deane**



**Councillor Flora Fox**



**Councillor Amanda Midgley**



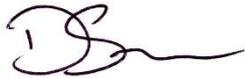
**Councillor Mark Richardson**



**Councillor Christian Street**

# QUALIFIED PERSONS

In accordance with Section 65 of the *Local Government Act 1993*, I confirm that the reports contained in Council Meeting Agenda No. 4 to be held on Monday, 2 March 2026 contain advice, information and recommendations given by a person who has the qualifications or experience necessary to give such advice, information or recommendations.



Dave Stewart  
CHIEF EXECUTIVE OFFICER  
being the General Manager as appointed by the  
Kingborough Council pursuant to section 61 of the  
*Local Government Act 1993* (TAS)

Tuesday, 24 February 2026

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## GUIDELINES FOR PUBLIC QUESTIONS

### Division 4 of the *Local Government (Meeting Procedures) Regulations 2025*

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This guide helps community members understand how to ask questions during Public Question Time at a Council meeting or sending in questions to be placed on the meeting Agenda, based on the [Local Government \(Meeting Procedures\) Regulations 2025](#), as well as any other determinations made by Council.

Please remember, this time is for asking questions only—there will be no discussion or debate about the questions or the answers.

|   |  |
|---|--|
| <b>How to Ask a Question:</b>                       | <p>You can ask a question either:</p> <ul style="list-style-type: none"><li>• <b>In writing</b> (before the meeting) (see questions on notice below), or</li><li>• <b>In person</b> at a regular Council meeting (see questions without notice below).</li></ul> <p>Your question must be about <b>Council activities only</b>.</p>  |
| <b>Purpose of Question Time:</b>                    | <ul style="list-style-type: none"><li>• This time is for asking questions, <b>not for debating</b> them.</li><li>• Answers will be given, but there won't be any discussion.</li></ul>   |
| <b>Written Questions (Questions on Notice):</b>     | <ul style="list-style-type: none"><li>• Must be sent <b>at least 7 days before</b> the meeting.</li><li>• The 7-day period includes weekends and public holidays, but <b>not</b> the day you submit the question or the day of the meeting.</li><li>• Title your submission clearly as <b>“Question/s on Notice.”</b></li></ul>  |
| <b>Verbal Questions (Questions Without Notice):</b> | <ul style="list-style-type: none"><li>• At least <b>15 minutes</b> will be set aside during the meeting for these.</li><li>• A <b>maximum of three (3) questions</b> will be allowed per person, per meeting.</li><li>• You <b>can't ask about topics already on the meeting agenda</b>.</li><li>• If your question can't be answered right away, it will be answered at the next meeting or as soon as possible.</li></ul>  |
| <b>Rules for Asking Questions:</b>                  | <p>Your question should:</p> <ul style="list-style-type: none"><li>• Be <b>short and clear</b>;</li><li>• <b>Not be a statement</b>;</li><li>• Have <b>little or no introduction</b>.</li></ul> <p>The Chairperson may <b>reject your question</b> if it:</p> <ul style="list-style-type: none"><li>• Is offensive, defamatory, or illegal;</li><li>• Doesn't relate to Council business;</li><li>• Is unclear, repetitive, or about confidential matters.</li></ul> |

**AGENDA of an Ordinary Meeting of Council**  
Kingborough Civic Centre, 15 Channel Highway, Kingston  
Monday, 2 March 2026 at 5.30pm

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**WELCOME**

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The Chairperson will declare the meeting open and welcome all in attendance. The Chairperson will advise all persons attending the meeting that they are to be respectful of, and considerate towards, other persons attending the meeting.

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**AUDIO RECORDING**

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The Chairperson will advise that Council meetings are recorded and made publicly available on its website. In accordance with Council's policy the Chairperson will request confirmation that the audio recording has commenced.

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**1 ACKNOWLEDGEMENT OF TRADITIONAL CUSTODIANS**

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The Chairperson will acknowledge the traditional custodians of this land, pay respects to elders past and present, and acknowledge today's Tasmanian Aboriginal community.

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**2 ATTENDEES**

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**Councillors:**

Mayor Councillor P Wriedt  
Deputy Mayor Councillor C Glade-Wright  
Councillor A Antolli  
Councillor D Bain  
Councillor G Cordover  
Councillor K Deane  
Councillor F Fox  
Councillor A Midgley  
Councillor M Richardson  
Councillor C Street

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**3 APOLOGIES**

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**4 CONFIRMATION OF MINUTES**

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**RECOMMENDATION**

That the Minutes of the open session of the Council Meeting No. 3 held on 16 February 2026 be confirmed as a true record.

**5 WORKSHOPS HELD SINCE LAST COUNCIL MEETING**

| Date        | Topic   | Detail   |
|-------------|---|--|
| 23 February | 1. Introduction to Asset Management<br>2. C Cell Shares | 1. An informational session on strategic asset management<br>2. Discussions relating to Councils holdings of shares in the Copping C Cell. |

**6 DECLARATIONS OF INTEREST**

In accordance with Regulation 10 of the *Local Government (Meeting Procedures) Regulations 2025* and Council’s adopted Code of Conduct, the Mayor requests Councillors to indicate whether they have, or are likely to have, a pecuniary interest (any pecuniary benefits or pecuniary detriment) or conflict of interest in any item on the Agenda.

**7 TRANSFER OF AGENDA ITEMS**

Are there any items, which the meeting believes, should be transferred from this agenda to the closed agenda or from the closed agenda to the open agenda, in accordance with the procedures allowed under Section 15 of the *Local Government (Meeting Procedures) Regulations 2025*.

**8 QUESTIONS WITHOUT NOTICE FROM THE PUBLIC**

**9 QUESTIONS ON NOTICE FROM THE PUBLIC**

*Council has determined that questions on notice or questions taken on notice from a previous meeting should not contain lengthy preambles or embellishments and should consist of a question only. To this end, Council reserves the right to edit questions for brevity so as to table the question only, with some context if need be, for clarity.*

**9.1 Biodiversity Offsets**

At the Council meeting on 16 February 2026, **Mr Peter Creef** asked the following question without notice to the Chief Executive Officer, with a response that the question would be taken on notice:

*I found it interesting that planning had figures on how many properties have paid the biodiversity offset, and it was at the fingertips of Cr Cordover at the time when he asked the question, but we still haven’t had any response as to how many properties have had a part 5 agreement and what was the value of the biodiversity of those properties prior to going into part 5 and there was no response to that in the Minutes. So just wondering if we get some sort of further information about that?*

**Officer’s Response:**

It is understood that Mr Creef’s question relates to matters previously raised by Ms Nitisha Knight at the Council Meeting of 2 February 2026. Ms Knight sought clarification regarding biodiversity offset payment values and, over the past five years, how many properties have entered into a Part 5 Agreement in lieu of paying a biodiversity offset. She also asked whether Council could provide spatial mapping identifying the location of those Part 5 areas.

Council apologises that a response to this matter was not provided at the previous Council meeting.

A similar question on notice was submitted for the Council Meeting of 18 August 2025. Relevant information from that response is reproduced below, as it provides indicative data over a 10-year period (2010–2024).

Over the period 2010–2024:

1. Financial Biodiversity Offsets

- A total of 532 applicants undertaking development or tree removal on private land contributed a financial biodiversity offset to the Kingborough Environmental Fund (KEF). [KEF Contributions Reconciliation table 2025.pdf](#)
- Financial offset contribution data is available on Council's website

2. Part 5 Agreements

- 57 applications for development on private land resulted in a Part 5 Agreement serving as the biodiversity offset.
- The term "development/works on private land" has been used as a surrogate for private landowner applicants, noting that development types vary significantly. These include commercial developments, subdivisions, unit developments, quarry operations, visitor accommodation, single dwellings, private education infrastructure and retirement villages.
- These offsets also apply to developments and works assessed under both the Kingborough Interim Planning Scheme and Council's by-laws.

A high-level assessment of offset payment data from 2016 to the present has been undertaken. Payments associated with larger developments (e.g. subdivisions, unit complexes and developments by commercial entities / government agencies) were excluded. The average offset payment for smaller-scale private development - such as single dwellings, ancillary dwellings/visitor accommodation, outbuildings and renovations - is approximately \$700. Further information about offset payments is available on Council's website (see link above).

For clarity, Part 5 Agreements are not entered into in lieu of payment. Under the [Kingsborough Biodiversity Offset Policy](#), offset options follow a hierarchy - on-site offsets, off-site offsets, and then financial offsets - consistent with State guidance on applying vegetation offsets. The biodiversity value for an offset is determined using established assessment methodologies, including the *TASVEG VCA Manual: A Manual for Assessing Vegetation Condition in Tasmania*, along with published scientific literature and/or advice from the Threatened Species Section (NRE) or the Threatened Species Scientific Advisory Committee (TSSAC). The appropriate option is determined case-by-case, in accordance with the Policy and based on the characteristics of the proposed development and the property in question.

Offsets, when applicable (i.e. an exemption does not apply), are only for when a proposed development impacts areas identified as having high or moderate biodiversity values (as defined in Table E10.1 of the E10.0 Biodiversity Code of the Kingborough Interim Planning Scheme 2015). The biodiversity value/category for an impacted area is to be determined in accordance with the *Guidelines for Natural Values Assessment* (DPIPWE, July 2009). This assessment occurs at the time of a development application and involves field verification.

Council is unable to provide spatial mapping for Part 5 Agreement locations, as this would release private information relating to individual landowner properties.

*Deleeze Chetcuti, Director Environment, Development & Community Services*

## 9.2 Vegetation Management

Mr Rob Cooper submitted the following question on notice:

1. Is Council aware that vegetation clearance along boundary fence lines is:
  - Lawfully undertaken pursuant to the Boundary Fences Act 1908 (Tasmania); and
  - Exempt from requiring a planning permit under the Kingborough Interim Planning Scheme 2015?
2. Has Council obtained independent legal advice confirming that imposing biodiversity offset payments on landowners undertaking lawful fence-line vegetation management:
  - Is within Council's bylaw-making powers under the Local Government Act 1993 (Tasmania); and
  - Does not conflict with existing statutory exemptions?
3. Has Council undertaken a formal bushfire risk assessment evaluating whether introducing financial penalties for removing vegetation along boundary fences adjoining municipal road reserves may:
  - Discourage vegetation management;
  - Increase roadside fuel loads; and
  - Potentially compromise evacuation safety during a major bushfire event?
4. In the event of a significant bushfire impacting Kingborough, does Council accept that discouraging vegetation management along municipal road corridors could be scrutinised in any post-incident review as contributing to evacuation risk?
5. Where biodiversity objectives conflict with community safety considerations, can Council confirm whether its primary obligation under section 20 of the Local Government Act 1993 (Tasmania) to promote the health, safety and welfare of the community takes precedence?

### Officer's Response:

1.
  - (a) Yes, noting that the *Boundary Fences Act 1908* must be read in conjunction with other legislation which may control the clearing of vegetation.
  - (b) If it meets the exemption provided for under Clause 5.4.1 (h) as follows: within 1.5m of a lot boundary for the purpose of erecting or maintaining a boundary fence.
2.
  - (a) Where lawful fence-line vegetation management is exempt from requiring approvals under any applicable legislation, there is no mechanism for requiring an offsets.
  - (b) Section 145 of the *Local Government Act 1993* provides Councils with a general power to make by-laws in respect of any act, matter or thing for which a council has a function or power under this or any other Act, including regulating vegetation removal which does not otherwise require a planning permit. Council does not currently have such a by-law.
  - (c) Legal advice confirms that there is no statutory inconsistency between regulation of vegetation removal via a by-law where this vegetation removal is exempt under the planning scheme.

3. No, Council has not assessed whether using financial penalties for removing native vegetation adjacent to boundary fences would discourage active management, increase fuel loads and compromise evacuation safety.
4. Council does not discourage vegetation management along road corridors where it is a reasonable risk mitigation measure. As various regulations may apply to the removal of native vegetation, it is important to confirm that no approvals are required before commencing any works. Following the advice of the Tasmanian Fire Service and leaving early is the safest option to protect yourself when there is threat of bushfire. Leaving early means leaving the area before the fire hits and is impacting evacuation routes.
5. Managing biodiversity is a responsibility of great importance for both current and future generations. The term 'biodiversity' encompasses all the complex interactions between living organisms that form the perfectly balanced ecosystems which support life. More simply that is, the air we breathe, the water we drink, the food we eat. There is no human health or welfare without biodiversity.

Greater Hobart is one of the highest risk cities in the world for bushfire risk. As a municipality Kingborough is mapped by the State as almost entirely bushfire prone. Council understands and plans for on the risk bushfire presents for our community and acts within our sphere of control and influence to mitigate that risk.

An inquiry commissioned by the Council of Australian Governments following loss of life and property in 2003<sup>1</sup> recommended eight principles, including the importance of 'shared responsibility'. Where community, government and fire agency preparedness are all crucial parts of reducing the potential impacts of bushfire.

Council must balance each of the priorities in Section 20 of the *Local Government Act 1993*. This means we must provide for the health and welfare of the community as well as their safety, this requires careful consideration of priorities. Council's investment in maintaining a fuel break network is an example of prioritising risk reduction through reducing fuel loads in asset protection zones.

<sup>1</sup> [Shared responsibility, community engagement and resilience: international perspectives | AJEM Research.](#)

*Liz Quinn, Manager Environmental Services*

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## 10 QUESTIONS WITHOUT NOTICE FROM COUNCILLORS

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## 11 QUESTIONS ON NOTICE FROM COUNCILLORS

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### 11.1 Bus Stops

At the Council meeting held on 16 February 2026, **Cr Midgley** asked the following question without notice to the Chief Executive Officer, with a response that the question would be taken on notice:

*A couple of years ago, the State Government did a bus stop relocation and upgrade installation project and as part of that, they found a lot of bus stops that needed upgrading, and then they had to*

*look at the DDA compliance around that. I know that then they chose a couple of bus stops to do across Kingborough. What I'm wanting to know is, have they all been completed, the ones that were in that budget? There was a long list of others, is there any plan for future upgrades?*

**Officer's Response:**

The Department of State Growth (DSG) has committed to upgrading bus stops in Kingborough to improve accessibility and bring them into compliance with Disability Discrimination Act (DDA) requirements. The bus stops included in the first two stages of the program have now been completed and a further stage of upgrades is planned.

These works are being delivered as part of the Kingston Congestion Package under the Hobart City Deal. In 2023 DSG undertook an audit of 90 bus stops within the municipality. Following consultation with Metro Tasmania and Council, 89 bus stops were identified for upgrade based on their usage, accessibility, and location. The upgrades are intended to improve accessibility as well as safety and comfort for passengers.

The program is being delivered in three stages. The first two stages were completed in 2024 and 2025 with a total of 37 bus stops upgraded across the municipality. These works included key locations in Tarooma, Kingston, Kingston Beach, Blackmans Bay, Margate, and Woodbridge.

A third stage is currently in the design phase and will deliver upgrades to a further 52 bus stops across Kingborough. Construction of this stage is expected to commence in mid-2026.

The bus stop upgrades are planned and delivered by DSG with Council providing input during the planning and design process. Council will continue to work with DSG to support the delivery of future upgrades.

*Renai Clark, Senior Roads Engineer*

**OPEN SESSION ADJOURNS**

**PLANNING AUTHORITY IN SESSION**

**12 OFFICERS REPORTS TO PLANNING AUTHORITY**

**12.1 DEVELOPMENT APPLICATION FOR 26 MULTIPLE DWELLINGS AND ACCESS WORKS OVER ADJOINING ROAD RESERVES AT 19 HYSOP ROAD AND ADJOINING ROAD RESERVES, MARGATE**

**File Number:** DA-2025-371  
**Author:** Brian Huang, Senior Planner  
**Authoriser:** Sarah Silva, Senior Statutory Planner

|   |  |
|---|--|
| <b>Applicant:</b>                                 | S J M Property Developments Pty Ltd  |
| <b>Owner:</b>                                     | 76 Esplanade Pty Ltd   |
| <b>Subject Site:</b>                              | 19 Hyssop Road and adjoining road reserves, Margate (CT 188765/1 and CT 30739/5)   |
| <b>Proposal:</b>                                  | 26 multiple dwellings and access works over adjoining road reserves  |
| <b>Planning Scheme:</b>                           | Kingborough Interim Planning Scheme 2015<br><i>Assessment is based on KIPS2015 and provisions of PD8 (which commenced 22 Feb 2022)</i>   |
| <b>Zoning:</b>                                    | Low Density Residential (Area C)   |
| <b>Codes:</b>                                     | E1.0 Bushfire Prone Area<br>E5.0 Road and Railway Assets<br>E6.0 Parking and Access<br>E7.0 Stormwater Management<br>E10.0 Biodiversity<br>E15.0 Riverine Flood Risk   |
| <b>Use Class/Category:</b>                        | Residential (26 multiple dwellings)  |
| <b>Discretions:</b>                               | <ul style="list-style-type: none"> <li>- Clause 12.4.2 A4 - Setbacks and building envelope</li> <li>- Clause 12.4.3 A2 - Site coverage and private open space</li> <li>- Clause 12.4.9 A1 - Residential density for multiple dwellings</li> <li>- Clause E5.5.1 A3 - Existing road accesses and junctions</li> <li>- Clause E5.6.4 A1 - Sight distance at accesses, junctions and level crossings</li> <li>- Clause E10.7.1 - Buildings and works</li> </ul> |
| <b>Public Notification:</b>                       | Public advertising was undertaken between 17 January 2026 and 2 February 2026 in accordance with section 57 of the <i>Land Use Planning and Approvals Act 1993</i>   |
| <b>Section 52(1B) Owner consent requirements:</b> | The proposal includes a new access over Incana Road, which is under Council's administration and therefore General Manager Consent under section 52(1B) of the <i>Land Use Planning and Approvals Act 1993</i> was required and was obtained. That land was included in the application and referred to in the public notice.  |
| <b>Representations:</b>                           | 13 representations   |
| <b>Recommendation:</b>                            | Approval subject to conditions.  |

# 1. PROPOSAL

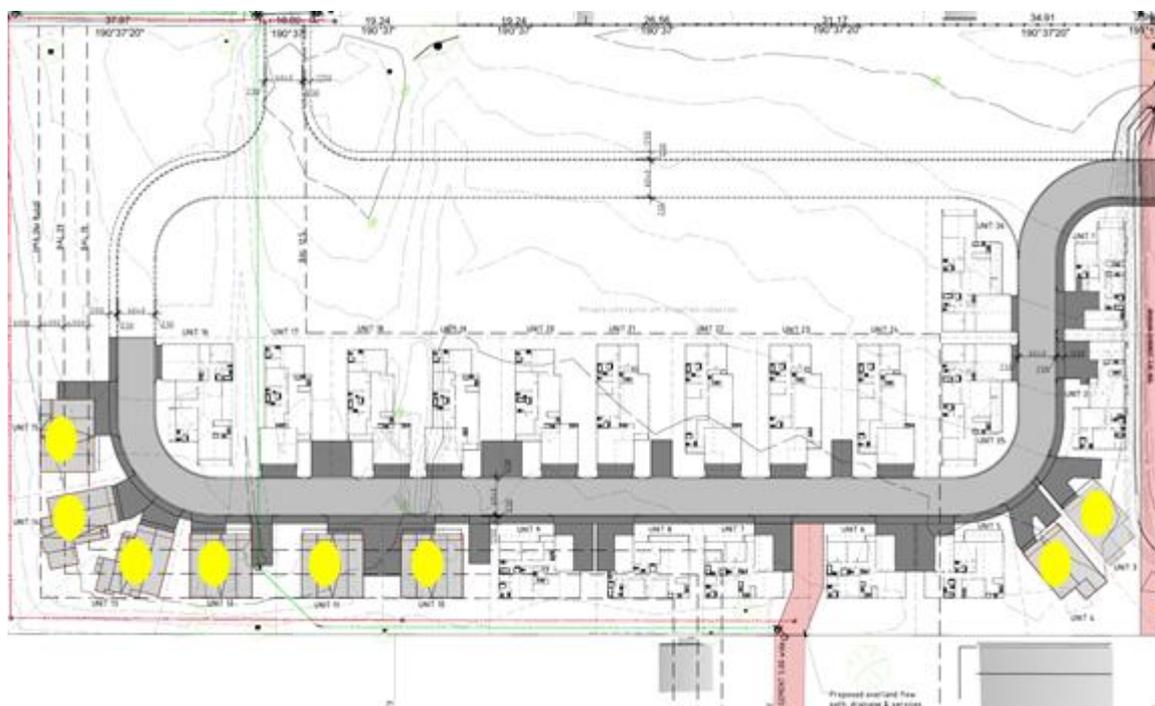
## 1.1 Description of Proposal

The application seeks a planning permit for the construction of **26 multiple dwellings** on a 1.93-hectare site. The site is a fully serviced area capable of connecting to reticulated water, sewerage, and stormwater networks.

The development consists of 26 dwellings with the following configurations:

- Two-storey Dwellings (4-bedroom): 6 units: (units 10 to 15)
- Two-storey Dwellings (3-bedroom): 2 units. (units 3 and 4)
- Single-storey Dwellings (3-bedroom): 18 units.

The dwelling proposed to be two-storey in height are identified in Drawing No. 3 of the proposed development plans. The subject dwellings are also highlighted in Figure 1 below. The two-storey dwellings are confined to the south-eastern and north-eastern corners of the site.



**Figure 1: proposed two-storey dwellings highlighted in yellow.**

## 1.2 Staging

The development is intended to be delivered in coordinated stages:

- Stage 1: Construction of 16 dwellings. This stage will include the construction of a new crossover and driveway from Incana Road and extensions to the existing pedestrian footpaths in the Incana Road road reservation in addition to the relocation of an existing power pole also in the Incana Road road reserve.
- Stage 2: Construction of 10 dwellings. This stage will include an extension to the driveway constructed in Stage 1 and a turning head, that should the future stage proceed would be replaced.
- Future Stage : This stage proposes a continuation of the driveway to service future dwellings.

**This application relates solely to Stages 1 and 2 of the development. Any future stages are not included in the current proposal and do not form part of this planning assessment. Such stages will be subject to separate applications and will be assessed at the time of lodgement in accordance with the operative planning provisions in force at that time.**

It is highlighted that the current application is for Stages 1 and 2 of the development only. Any future development for Future Stage 3 will be subject to a separate planning application.

For the 26 multiple dwellings proposed in Stages 2 and 3, the architectural design incorporates a variety of building heights, with two-storey units reaching a maximum height of 8.5m above natural ground level. External materials are selected to integrate with the suburban context, including Colorbond roof cladding, JH Oblique external cladding, and Tas Oak vertical board accents.

**Parking and Access:** Each dwelling is provided with two dedicated car parking spaces. A total of seven (7) visitor parking spaces are proposed, which will be distributed throughout the site for communal use. The parking areas will be accessed via a single driveway constructed from Incana Road. No access is proposed to Hyssop Road as part of this current planning application. The proposed plans indicate a future road through to Hyssop Road although this will be subject to a future planning application.

**Private Open Space:** Each dwelling is allocated a minimum total POS of 60m<sup>2</sup>. Primary POS areas are designed with a minimum dimension of 4m, gradients less than 1:10, and are directly accessible from habitable rooms.

**Waste Management:** Each unit accommodates an individual bin storage area of at least 1.5m<sup>2</sup>. A private contractor will facilitate off-street bin collection. It is noted that private waste collection is included as a condition of approval in the recommendation of this report.

**Vegetation and Privacy:** The proposal includes a mix of feature planting (e.g., Leucadendron) and shrubs/groundcovers (e.g., Lilly Pilly, Dwarf Rosemary) to enhance visual amenity and provide landscaped buffers. High-conservation value trees identified on site are to be retained and protected via established Tree Protection Zones. It is noted that the landscaping plan submitted with the application is difficult to read. The provision of a detailed landscaping plan is included as a condition of approval in the recommendation of this report. A condition is also included requiring the protection of any high conservation trees on the site.

**Boundary Treatment:** A 1200mm (F2) high vertical lapped timber paling fence is proposed for the northern boundary fronting the road and a 1500mm vertical lapped timber fence along the shared boundary with the northern neighbouring land. Other standard internal fencing includes 1800mm high (F1) vertical lapped timber paling fence. It is noted that the recommended condition requiring the provision of a new landscaping plan will also require additional detail in relation to fencing, as the F1 fence is not adequately shown of the plan.

**Bushfire Management:** The proposal includes designated Bushfire Hazard Management Areas (HMA) and BAL-rated setbacks ranging from BAL-12.5 to BAL-29, all contained within the lot boundaries.

**Reticulated Services:** The development will connect to reticulated water, sewerage, and stormwater networks designed to TasWater and Council standards.

**Stormwater Strategy:** The design incorporates a 5.0m wide overland flow path, drainage, and services easement along with detention tanks to manage site runoff.



**Figure 2: Proposed site plan with aerial underlay.**

### 1.3 Description of Site

The subject site at 19 Hyssop Road comprises a substantial land parcel that is considerably larger than the surrounding developed residential lots. Based on the cadastral information available from the Kingborough Council planning map, the site presents as a broadly rectangular parcel. The site has two road frontages: western frontage to Hyssop Road and norther frontage to Incana Road.

The surrounding land to the west is established residential primarily single dwelling uses that are residential in character. Land to the east is comprised of larger lots more reflective of low-density residential character. Land to the south is underdeveloped light industrial land.

The subject land represents one of the few larger vacant residential parcels remaining within this part of the Margate settlement.

Topographically, the land is generally flat throughout, which influences the way development can be accommodated. The slope is consistent with the broader landform character of the Margate area and is not unusual within this locality.

The site is not subject to coastal frontage and does not directly adjoin public open space. Existing services including reticulated water, sewerage, electricity and telecommunications are available within Hyssop Road and the surrounding urban area, enabling connection for future development.

The surrounding locality is characterised by a predominantly residential built form, comprising a mix of single dwellings and small-scale multi-dwelling developments. Lot sizes within the immediate area vary, with larger legacy allotments interspersed with smaller residential lots created over time. A large Light Industrial zoned allotment is located to the south of the site, representing a localised non-residential interface within an otherwise residential context.

Development within the locality generally consists of single storey dwellings, with building setbacks, roof forms, and materials reflecting a typical suburban residential character. The

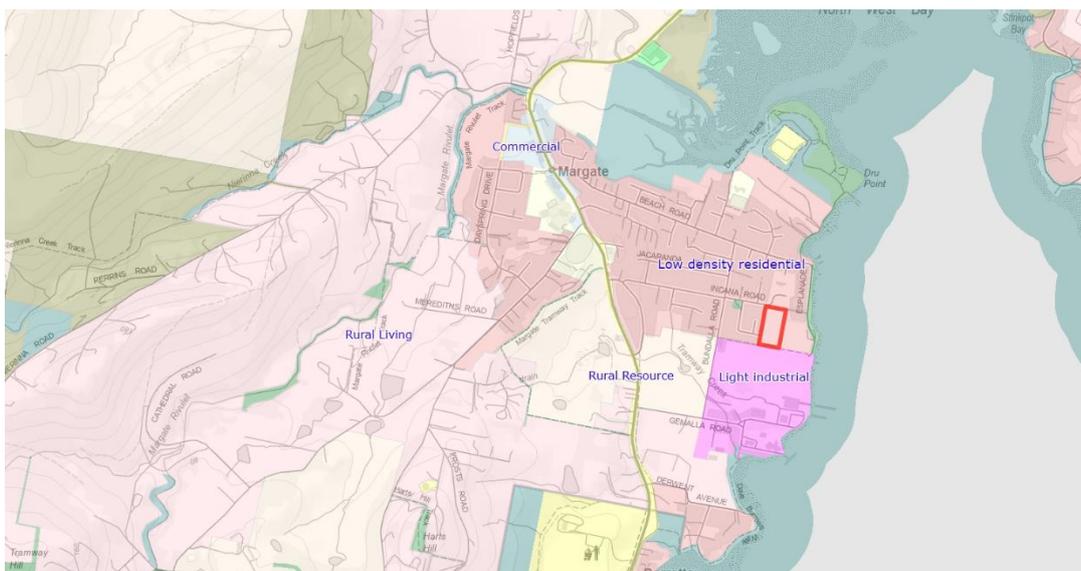
area exhibits a gradual transition from lower-density housing to more intensive residential forms.

Hyssop Road and Incana Road functions as a local residential street, providing access to adjoining properties and connecting to the wider local road network. Traffic volumes are typical of a residential area and do not indicate a high-order transport function.

The site is located within reasonable proximity to local services, community facilities, and public transport routes within Margate and Kingston. This includes access to local shops, schools, recreational facilities, and coastal amenities, supporting the suitability of the area for residential intensification. A site locality plan is provided in **Figure 3** and **Figure 4** below.



**Figure 3: Subject site highlighted in yellow (LISTmaps)**



**Figure 4: Site locality plan. Subject site highlighted in red.**

There are 29 trees both adjacent and within the subject site, including:

- five (5) *E. ovata* (black gums) trees of very high conservation value (DBH >70cm), which provide potential nesting habitat and potential swift parrot foraging habitat;
- ten (10) *E. ovata* (black gums) trees of high conservation value (DBH 40-70cm), which provide potential swift parrot foraging habitat;
- eleven (11) other native trees which are below the threshold of a high conservation value tree; and
- three (3) other trees which are not native to Tasmania.

Most of these trees are located on adjacent land, including the Incana road easement to the north and private properties to the east. Only trees 40 and 44-48 are located within the site. All trees potentially affected by the development are proposed for retention.

There was also a small group of trees located within the footprint of the proposed development. These trees were removed prior to the lodgement of the application without prior approval. Based on the available information, none of these trees were of a sufficient size to be high conservation value. The retrospective removal of these trees forms part of the application.

While no trees are proposed for removal, the access, placement of controlled fill, fencing, upgrading of the existing public gravel path, stormwater works and some units encroach into the tree protection zones of 22 trees, including trees 1, 3, 4, 10, 11, 12, 14, 15, 19, 22, 26, 28, 29, 30, 38, 39, 40, 41, 42, 43, 44, 45. Of the trees impacted, four (4) are very high conservation value and five (5) are high conservation value.

#### 1.4 Background

There is no relevant planning history for the site.

## 2. ASSESSMENT

### 2.1 State Policies and Act Objectives

The proposal is consistent with the outcomes of the State Policies.

The proposal is consistent with the objectives of Schedule 1 of the *Land Use Planning and Approvals Act 1993*.

### 2.2 Strategic Planning

The relevant strategies associated with the Scheme are as follows:

#### *Zone Purpose Statements of the Low Density Residential Zone*

The relevant zone purpose statements of the Low Density Residential Zone are to:

- 12.1.1.1 To provide for residential use or development on larger lots in residential areas where there are infrastructure or environmental constraints that limit development.
- 12.1.1.4 To provide for existing low density residential areas that usually do not have reticulated services and have limited further subdivision potential.

It is highlighted that the existing low density residential zone is not reflective of the zone purpose statements as the character of the area predominantly comprises smaller lots than normally permitted in the Low Density Residential Zone. This is reflective of the fact

that the area is fully serviced. There are no infrastructure or environmental constraints that limit development on the subject site, nor in the surrounding area.

Given the serviceability of the area, the character of the area is more reflective of the General Residential Zone. This is recognised in the draft Kingborough Local Provisions Schedule which proposes a transition of the site to the General Residential Zone with a permitted site density of 325m<sup>2</sup> per dwelling and 450m<sup>2</sup> minimum lot size for subdivision.

Regardless of the future strategic intent for the site, the proposed development must be assessed against the planning scheme in effect currently, the Kingborough Interim Planning Scheme 2015 and the Low Density Residential Zone.

*Clause 12.1.2 – Local Area Objectives and Desired Future Character Statements*

The Scheme details separate Local Area Objectives and Desired Future Character Statements for the main towns in the municipal area. The following Local Area Objectives and Desired Future Character Statements are relevant to the assessment of this application.

| Local Area Objectives               |   | Implementation Strategy |   |
|-------------------------------------|---|-------------------------|---|
| (a)                                 | Other than those areas that are in close proximity to the town's commercial centre, Margate should be maintained as a residential community with a relatively lower housing density.  | (a)                     | Future residential development is to be directed towards suitable new areas, rather than encouraging the infill of existing suburban areas at higher densities, except where this infill is in close proximity to the town's commercial centre. |
| (b)                                 | Margate is zoned Low Density Residential in order to reflect existing settlement patterns and to retain existing coastal and/or visual landscape and natural environmental values.    | (b)                     | Existing larger lot sizes are to be retained in order that to reflect existing settlement patterns and in some cases to retain coastal/landscape amenity.   |
| (c)                                 | Local residential roads, junctions and verges should be designed and provided for in a manner that facilitates the free flow of traffic and encourages bicycle and pedestrian access. | (c)                     | The application of development standards should where possible retain native vegetation and protect local residential amenity.  |
| Desired Future Character Statements |   | Implementation Strategy |   |
| (a)                                 | There should be a mix of housing choice within Margate, while still retaining the residential amenity afforded by off-street parking, ample gardens and street setbacks.              | (a)                     | Multi-unit housing and aged care facilities should be limited to suitable areas closer to the town's main commercial area, with good access provided to local services.   |
| (b)                                 | The existing neighbourhood character that is associated with the area's settlement pattern, landscape and environmental values should be protected.                                   | (b)                     | The larger lots within this zone enable the existing local character of the area to be retained. Some coastal areas are serviced, but this zone will enable the protection of existing coastal, landscape and environmental values.             |

The proposal aligns with the above-mentioned statements and objectives as follows:

- The subject site is a fully serviced infill site that is within walking distance of Margate's commercial centre.
- The subject site is large in site however the existing settlement pattern in the area, specifically to the north and west, is reflective of general residential land use, rather than low density residential.
- With the exception of eight (8) multiple dwellings, all dwellings on the site are proposed to only be single storey.
- The site has two (2) very small road frontages and is will not be easily visible from any public spaces. The site will not be visible from the coast. For these reasons the development is expected to retain coastal/landscape amenity.
- No non-residential elements are proposed. The proposed use is residential only.
- The closest Rural Resource zoned land is located 200m to the west and is already directly adjacent to other established residential uses. There is no Significant Agricultural zoned land proximate to the site.
- The Desired Future Character for Margate anticipates a mix of housing choice while retaining residential amenity, including off-street parking, and generous setbacks. The proposal is consistent with this outcome by providing residential development that provides for additional housing options. The site is only a 1km level walk to the Margate Town Centre and a 400m walk to the nearest bus stop.
- Only Stages 1 and 2 form part of this planning approval. The site density proposed within these stages is not out of character with the surrounding area, noting that any future stage will be subject to further planning approval. The more detailed assessment is provided in the following sections.

For the reasons above, it is considered that the proposed development can align with the Local Area Objectives and Desired Future Character Statements for Margate.

### **2.3 Statutory Planning**

The use is categorised as Residential (Multiple Dwelling) under the Scheme, which is a use that requires Permitted assessment in the Low Density Residential Zone. Whilst the application is classified as a Permitted use, it relies on Performance Criteria to comply with the Scheme provisions and is therefore discretionary.

### **2.4 Exemptions**

It is noted that proposal includes a strata subdivision to be undertaken following approval of the development.

Pursuant to Clause 5.6.8 of the Planning Scheme, division by strata titles of lawfully constructed or approved buildings, for a use granted a permit under this planning scheme, is exempt from planning and is not considered further in this report.

### **2.5 Other Considerations**

Council's assessment of this proposal should also consider the issues raised in the representations, the outcomes of any relevant State Policies and the objectives of Schedule 1 of the *Land Use Planning and Approvals Act 1993*.

### **2.6 Use and Development Standards**

The proposal satisfies the relevant Acceptable Solutions of the Scheme (see checklist in Attachment 1), with the exception of the following:

**Low Density Residential Zone**  
**Clause 12.4.2 Setbacks and building envelope**

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| <b>Acceptable Solution A4</b>  |
| <b>A4</b> - No trees of high conservation value will be impacted.  |
| <b>Performance Criteria</b>  |
| <b>P4</b> - Buildings and works are designed and located to avoid, minimise, mitigate and offset impacts on trees of high conservation value.  |
| <b>Proposal</b>  |
| <p>An arboricultural assessment (Philip Jackson December 2025 version 2) and addendum (17 December 2025 version 2) confirm that the proposed development has:</p> <ul style="list-style-type: none"> <li>• an acceptable impact on two (2) very high and one (1) high conservation value trees;</li> <li>• a moderate and tolerable impact on one (1) very high and one (1) high conservation value trees; and</li> <li>• an unacceptable impact on one (1) very high and three (3) high conservation value trees.</li> </ul> <p>As the development impacts on high and very high conservation value trees, it requires assessment against the performance criteria.</p> |

The proposed development can be supported pursuant to this Performance Criteria for the following reasons:

- An arboricultural assessment (Philip Jackson December 2025 version 2) and addendum (Philip Jackson 17 December 2025 version 2) were submitted in response to further information requests assessing the impact of the development on the trees. The first more detailed assessment (Philip Jackson December 2025 version 2) encompasses the impacts of the access, placement of controlled fill, fencing, upgrading of the existing public gravel path, internal service lines and encroachment from some units. The addendum to this assessment (Philip Jackson 17 December 2025) assesses the impact of piping and filling the proposed stormwater line.
- These assessments confirm that, while impacts range from acceptable to unacceptable, all trees are feasible for retention providing the recommended mitigation measures are implemented. These measures include:
  - Retaining all trees and protecting them during construction in accordance with the Tree Protection Plan (TPP) in Appendix 1 and Item 6.3 of the Tree Protection Schedule (TPS).
  - Constructing the upgraded path in the Incana road casement above the present grade using no-dig methods, including no excavation or compaction, in accordance with Item 6.6 of the TPS.
  - Excavating fence post footings by hand in the notional root zone (NRZ) and structural root zone (SRZ) and offsetting posts to avoid significant roots (>40mm in diameter) in accordance with sub-item 6.4.1 of the TPS.
  - Limiting the placement of controlled fill to the footprint of the proposed units;
  - Ensuring remaining fill areas only use friable and non-compacted material equivalent in texture to the existing site topsoil in accordance with sub-item 6.4.2 of the TPS. The fill may be consolidated but must not be compacted to engineering standards.

- Installing the new stormwater pipe above the present grade and along the alignment of the existing swale drain basin, with no excavation and bedding and backfilling the pipe with gap-graded aggregate to avoid compacted fill.
- Connecting the western end of the stormwater pipe to the existing main at a culvert without excavation and outside the NRZ of any trees.
- Conditions are recommended for inclusion in any permit issued retrospectively approving the vegetation removal within the site, requiring the retention of all remaining trees and requiring implementation of the recommended mitigation measures.

**Low Density Residential Zone**

**Clause 12.4.3 Site coverage and private open space**

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| <p><b>Acceptable Solution</b></p> <p><b>A2</b> - A dwelling must have an area of private open space that:</p> <p>(a) is in one location and is at least:</p> <p>(i) 24 m<sup>2</sup>; or</p> <p>(ii) 12 m<sup>2</sup>, if the dwelling has a finished floor level that is entirely more than 1.8 m above the finished ground level (excluding a garage, carport or entry foyer); and</p> <p>(b) has a minimum horizontal dimension of:</p> <p>(i) 4 m; or</p> <p>(ii) 2 m, if the dwelling has a finished floor level that is entirely more than 1.8 m above the finished ground level (excluding a garage, carport or entry foyer); and</p> <p>(c) is directly accessible from, and adjacent to, a habitable room (other than a bedroom); and</p> <p>(d) is not located to the south, south-east or south-west of the dwelling, unless the area receives at least 3 hours of sunlight to 50% of the area between 9.00am and 3.00pm on the 21st June; and</p> <p>(e) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of north and 30 degrees east of north, excluding any dwelling located behind another on the same site; and</p> <p>(f) has a gradient not steeper than 1 in 10; and</p> <p>(g) is not used for vehicle access or parking.</p> |
| <p><b>Performance Criteria</b></p> <p><b>P2</b> – A dwelling must have private open space that:</p> <p>(a) includes an area that is capable of serving as an extension of the dwelling for outdoor relaxation, dining, entertaining and children's play and that is:</p> <p>(i) conveniently located in relation to a living area of the dwelling; and</p> <p>(ii) orientated to take advantage of sunlight.</p>  |
| <p><b>Proposal</b></p> <p>The private open space (POS) areas for Units 13–15 are located to the south or southeast of the site. The submitted shadow analysis demonstrates that the nominated 24m<sup>2</sup> POS for each unit receives at least 3 hours of sunlight to a minimum of 50% of the area between 9.00am and 3.00pm on 21 June. While additional open space areas are provided around each dwelling beyond the designated 24m<sup>2</sup> POS, these areas do</p>   |

not individually meet the specified solar access standard. Assessment is therefore required under the Performance Criteria.

The proposed development can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The proposal provides a clearly defined minimum 24m<sup>2</sup> private open space (POS) area for each dwelling, identified on the advertised plans as the primary outdoor living area. This area is directly accessible from the main internal living spaces, allowing for convenient and functional integration between indoor and outdoor environments. The physical proximity and clear line of access ensure that the POS operates as a practical extension of the dwelling for day-to-day living.
- The submitted shadow analysis confirms that the nominated 24m<sup>2</sup> POS for each dwelling receives at least 3 hours of sunlight to a minimum of 50% of its area between 9.00am and 3.00pm on 21 June. This demonstrates that the principal outdoor areas are appropriately oriented to capture winter sunlight, ensuring usability during the period when solar access is most critical.
- In terms of size and functionality, 24m<sup>2</sup> provides sufficient area to accommodate outdoor relaxation, dining settings, seating, children’s play and clothes drying. The configuration of these areas is regular and usable, rather than fragmented or residual. This supports their role as meaningful outdoor living spaces rather than incidental yard areas.
- While some additional open space areas around each dwelling do not independently meet the winter sunlight benchmark, they are supplementary to the nominated 24m<sup>2</sup> primary POS. The requirement is for each dwelling to include an outdoor area capable of serving as an extension of the dwelling. That requirement is met through the designated and compliant 24m<sup>2</sup> POS for each unit, with additional landscaped areas further contributing to amenity.

**Low Density Residential Zone**

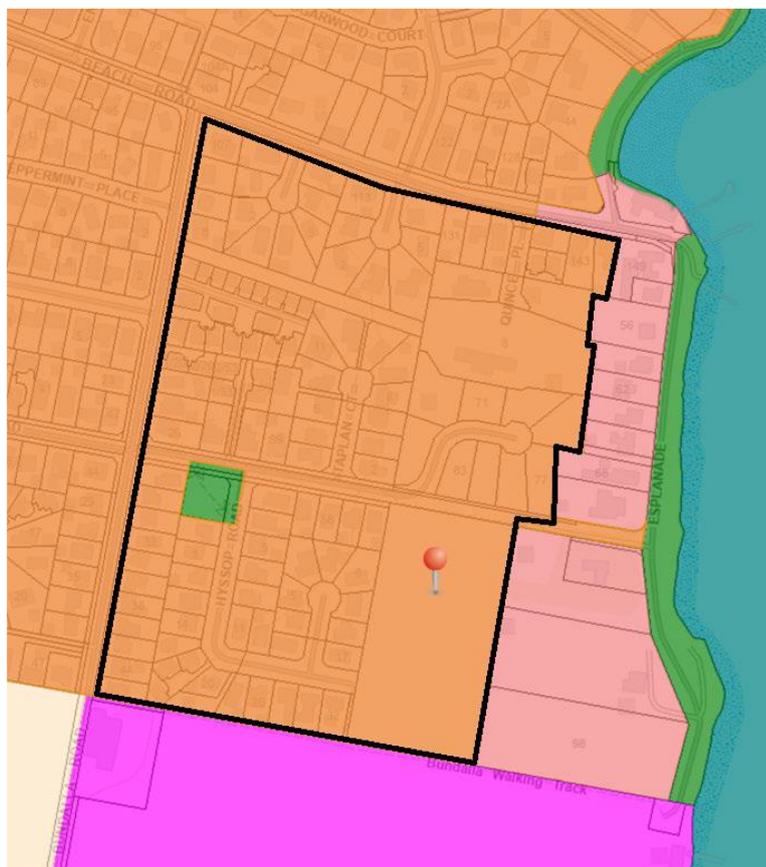
**Clause 12.4.9 – Residential density for multiple dwellings**

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| <b>Acceptable Solution</b>  |
| <b>A1</b> – Multiple dwellings must have a site area per dwelling of not less than 1000m <sup>2</sup> .   |
| <b>Performance Criteria</b>   |
| <p><b>P1</b> - Multiple dwellings may only have a site area per dwelling of less than 1000m<sup>2</sup> if the number of dwellings:</p> <ul style="list-style-type: none"> <li>(a) is not out of character with the pattern of development in the surrounding area; and</li> <li>(b) does not result in an unreasonable loss of natural or landscape values; and</li> <li>(c) does not exceed the capacity of the current or intended infrastructure services in the area.</li> </ul>   |
| <b>Proposal</b>   |
| <p>The planning scheme defines ‘site area as meaning:</p> <p><i>the lot or lots on which a use or development is located or proposed to be located.</i></p> <p>The planning scheme defines a ‘lot’ as meaning:</p> <p><i>means a piece or parcel of land in respect of which there is only one title other than a lot within the meaning of the Strata Titles Act 1998.</i></p> <p>Considering the above, regardless of the proposed building layout on the site. The density must be calculated by dividing the site area within CT 188765/1 (1.93ha by the number of dwellings proposed (26). The fact that the dwellings will be contained within smaller strata lots for Stages 1 and 2 does not form part of the assessment. It is</p> |

highlighted that strata subdivision is exempt under the planning scheme. Accordingly, the proposed density is 739m<sup>2</sup> per lot, less than the required 1000m<sup>2</sup>. For this reason, the proposal must be assessed under relevant performance criteria.

The proposed development can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- The proposal achieves a density outcome that is consistent with the broader established development pattern in Margate, particularly within the serviced and urbanised parts of the Low-Density Residential Zone. Importantly, “pattern of development” is not defined narrowly by the immediate streetscape only; it includes how density is experienced across connected streets and residential neighbourhood blocks.
- The supporting planning report (All Urban Planning, 20 October 2025) adopts a reasoned approach to defining the surrounding area (broader than adjoining lots, extending across a locality bounded by key roads and zone interfaces) and provides density calculations showing a median site area per dwelling of 674 m<sup>2</sup> within that defined area. On that basis, the proposal’s density of 739 m<sup>2</sup> per dwelling is comparable with, and generally consistent with, the existing prevailing pattern, and is not a marked departure from the neighbourhood context.



**Figure 5: The defined site density comparison area as provided in the supporting planning report (All Urban Planning, 20 October 2025)**

- From an urban design and built-form perspective, the advertised plans demonstrate that the development retains a low-rise suburban character through:
  - a predominance of single-storey dwellings, with a limited number of two-storey dwellings,

- landscaped site edges and spacing between built forms that maintains a residential scale and openness.
- While the numerical density exceeds the acceptable solution benchmark, the visual and functional outcome remains consistent with surrounding character of the area.
- The site is described as generally flat and cleared, within an established suburban context, and the applicant’s Arboriculture Assessment confirms that there are no native vegetation communities currently on the site. While the site does contain individual trees of high conservation values, these trees are all proposed for retention. As such, the proposed yield does not require removal of significant natural assets and does not translate into an unreasonable loss of natural values.
- A condition of approval includes the provision of a detailed landscaping plan to assist softening the development from any public spaces. This will help ensure that the development does not materially diminish the locality’s landscape qualities or result in a visually harsh outcome inconsistent with the existing suburban setting.
- The application is supported by concept civil documentation indicating that the development can be serviced by existing infrastructure networks, including reticulated water, sewer and stormwater, designed to meet TasWater and Council standards.
- The plans show a development form that is consistent with residential servicing expectations with compliant parking on site.
- No concerns were raised by TasWater or TasNetworks in relation to servicing the future development.

**E5.0 Road and Railway Assets Code Code**

**Clause E5.5.1 – Existing road accesses and junctions**

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| <b>Acceptable Solution</b>   |
| <b>A3</b> - The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.   |
| <b>Performance Criteria</b>  |
| <b>P3</b> - Any increase in vehicle traffic at an existing access or junction in an area subject to a speed limit of 60km/h or less, must be safe and not unreasonably impact on the efficiency of the road, having regard to: <ul style="list-style-type: none"> <li>(a) the increase in traffic caused by the use;</li> <li>(b) the nature of the traffic generated by the use;</li> <li>(c) the nature and efficiency of the access or the junction;</li> <li>(d) the nature and category of the road;</li> <li>(e) the speed limit and traffic flow of the road;</li> <li>(f) any alternative access to a road;</li> <li>(g) the need for the use;</li> <li>(h) any traffic impact assessment; and</li> <li>(i) any written advice received from the road authority</li> </ul> |
| <b>Proposal</b>  |
| The proposal is in an area subject to a speed limit 60km/h or less and the number of vehicle movements will increase by more than 20% or 40 movements per day. Therefore, it must be assessed under relevant performance criteria.   |

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

The Traffic Impact Assessment (TIA) submitted with the application (Hubble Traffic

December 2025) estimates that the proposed 26 dwellings will generate approximately **169 vehicle trips per day**, with approximately **17 movements during weekday peak hours**. This represents a moderate increase consistent with typical residential development and does not constitute a high-intensity traffic load.

- The traffic generated will be residential in nature, comprising predominantly passenger vehicles with dispersed trip patterns throughout the day. There is no indication of heavy vehicle, commercial, or industrial traffic that would materially affect road safety or performance.
- The proposed access has been assessed through the submitted TIA and sight distance analysis. The junction design complies with Austroads parameters and provides adequate visibility and manoeuvring capacity. The access arrangement is considered functionally appropriate for the level of traffic anticipated.
- Incana Road is a local residential street with a carriageway width of approximately 8.5 metres between kerbs. It serves residential properties and is not classified as an arterial or high-capacity distributor road. The projected increase in traffic is consistent with the road’s residential function.
- The road operates in a low-speed residential environment ( $\leq 60\text{km/h}$ ). Given the modest peak hour increase of approximately 17 movements, the additional traffic is unlikely to materially alter traffic flow conditions or compromise safety.
- Future stages of development (not part of this application) indicate provision for additional access via Hyssop Road, which would further distribute vehicle movements. The existence of alternative access enhances network resilience and reduces reliance on a single access point.
- The access is necessary to facilitate orderly residential development of the site. Distributed access points support appropriate traffic management and are consistent with sound engineering practice.
- A detailed TIA accompanies the application and concludes that Incana Road has sufficient capacity to accommodate the projected increase in vehicle movements. Council’s engineering review has not raised concerns regarding capacity or efficiency.
- Council, as the relevant road authority, has reviewed the TIA and has not identified any safety or operational deficiencies associated with the proposed increase in traffic.
- The TIA submitted estimates the 26 units will generate 169 vehicle trips per day. 17 daily traffic movements are indicated in the TIA to occur during the weekday peak hours. The standard of Incana Road has ample capacity to cater for the increased traffic movements generated by the development.

**E5.0 Road and Railway Assets Code**

**Clause E5.6.4 – Sight distance at accesses, junctions and level crossings**

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| <b>Acceptable Solution</b>  |
| <b>A1 - Sight distances at:</b><br>(a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; |

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| <p><b>Performance Criteria P1</b></p> <p><b>P1</b> - The design, layout and location of an access, junction or rail level crossing must provide adequate sight distances to ensure the safe movement of vehicles, having regard to:</p> <p>(a) the nature and frequency of the traffic generated by the use;</p> <p>(b) the frequency of use of the road or rail network;</p> <p>(c) any alternative access;</p> <p>(d) the need for the access, junction or level crossing;</p> <p>(e) any traffic impact assessment;</p> <p>(f) any measures to improve or maintain sight distance; and</p> <p>(g) any written advice received from the road or rail authority.</p> |
| <p><b>Proposal</b></p> <p>Table E5.1 does not include sight distances for vehicle speeds less than 50 km/h. Vehicles approaching the site access from the left or cul-de-sac side will approach at estimated speed of 40km/h due to the tight curves from the cul-de-sac side. Therefore, the proposal must be assessed against the Performance Criteria.</p>   |

The proposed development can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- A detailed TIA accompanies the application.
- The proposed development of 26 dwellings is expected to generate approximately 169 daily vehicle movements, with approximately 17 movements occurring during weekday peak periods. This level of traffic generation is typical of residential development and is moderate in scale. The projected traffic volume does not represent a high-intensity or commercial traffic profile that would require enhanced sight distance standards beyond those prescribed in Austroads guidelines.
- Incana Road is a local residential street with a standard carriageway width of approximately 8.5 metres between kerbs. It services residential properties and does not function as a high-speed or arterial route. The frequency of traffic use is consistent with a suburban residential environment. Given its classification and operating speed environment, the available sight distances are appropriate to the road function.
- The access from Incana Road is necessary to facilitate the orderly development of the site and to provide distributed traffic entry and exit. Restricting access to Hyssop Road alone would concentrate traffic movements and reduce network efficiency. The proposed access supports appropriate circulation and site functionality.
- The sight distance measurements have been undertaken in accordance with standard Austroads design guideline parameters. The assessment confirms that available sight distance exceeds 100 metres from the Bundalla Road approach and provides at least 65 metres for right-turning vehicles entering the site due to the geometry of Incana Road. The TIA concludes that the access junction provides sufficient sight distance to allow vehicles to enter and leave safely and efficiently without adverse impacts on Incana Road traffic movements.
- The access design is based on measured sight triangles compliant with Austroads standards. In addition, the permit includes conditions requiring a raised concrete threshold and appropriate signage and line marking, which assist in managing vehicle speeds and enhancing pedestrian and driver awareness at the crossover.

- Council's engineering assessment has reviewed the TIA and sight distance analysis. No objections or safety concerns have been raised by Council as the relevant road authority.

**E10.0 Biodiversity Code**

**Clause E10.7.1 - Buildings and works**

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| <b>Acceptable Solution</b>   |
| <b>A1</b> - Clearance and conversion or disturbance must be within a Building Area on a plan of subdivision approved under this planning scheme.   |
| <b>Performance Criteria</b>  |
| <p><b>P1</b> - Clearance and conversion or disturbance must satisfy the following:</p> <p>(a) if low priority biodiversity values:</p> <ul style="list-style-type: none"> <li>(i) development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development;</li> <li>(ii) impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fire-resistant design of habitable buildings</li> </ul> <p>(b) if moderate priority biodiversity values:</p> <ul style="list-style-type: none"> <li>(i) development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development;</li> <li>(ii) impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fire-resistant design of habitable buildings;</li> <li>(iii) remaining moderate priority biodiversity values on the site are retained and improved through implementation of current best practice mitigation strategies and ongoing management measures designed to protect the integrity of these values;</li> <li>(iv) residual adverse impacts on moderate priority biodiversity values not able to be avoided or satisfactorily mitigated are offset in accordance with the Guidelines for the use of Biodiversity Offsets in the local planning approval process, Southern Tasmanian Councils Authority, April 2013 and Kingborough Biodiversity Offset Policy 6.10, November 2016.</li> </ul> <p>(c) if high priority biodiversity values:</p> <ul style="list-style-type: none"> <li>(i) development is designed and located to minimise impacts, having regard to constraints such as topography or land hazard and the particular requirements of the development;</li> <li>(ii) impacts resulting from bushfire hazard management measures are minimised as far as reasonably practicable through siting and fire-resistant design of habitable buildings;</li> <li>(iii) remaining high priority biodiversity values on the site are retained and improved through implementation of current best practice mitigation strategies and ongoing management measures designed to protect the integrity of these values;</li> <li>(iv) special circumstances exist;</li> <li>(v) residual adverse impacts on high priority biodiversity values not able to be avoided or satisfactorily mitigated are offset in accordance with the Guidelines for the use of Biodiversity Offsets in the local planning approval process, Southern Tasmanian Councils Authority, April 2013 and Kingborough Biodiversity Offset Policy 6.10, November 2016.</li> </ul> |

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|---|
| (vi) clearance and conversion or disturbance will not substantially detract from the conservation status of the biodiversity value(s) in the vicinity of the development.   |
| <b>Proposal</b>   |
| <b>A1 – Does not comply.</b> The proposal relies upon retrospective removal of a small copse of native vegetation and disturbance of native trees located within a Biodiversity Protection Area.<br>The proposal is unable to meet A1 as there is no building area on the title. Therefore, the proposal must be assessed against the Performance Criteria. |

The proposed variation can be supported pursuant to this Performance Criteria of the Zone for the following reasons:

- Clearing of native vegetation is limited to a small copse of trees located within the footprint of the development. This copse has already been removed without prior approval and therefore requires retrospective assessment. Based on the available information, this vegetation comprised predominantly low scrub and dense shrubs with four small eucalypt trees below the threshold of high conservation value trees.
- As this vegetation was located directly within the footprint of the units and their associated private open space, it would not have been feasible to retain this vegetation without reducing the number of units. Given the vegetation does not contain priority biodiversity values and the retention of all high and very high conservation value trees, the number and density of units is not considered to result in an unreasonable loss of natural or landscape values.
- The proposed clearing and disturbance is located to minimise impacts on moderate priority biodiversity values to the extent practicable, with all high-very high conservation value trees proposed for retention.
- An arboricultural assessment (Philip Jackson December 2025 version 2) and addendum (Philip Jackson 17 December 2025 version 2) confirm that, while impacts range from acceptable to unacceptable, all remaining trees are feasible for retention providing the recommended mitigation measures are implemented.
- No clearing of native vegetation or priority biodiversity values is relied upon for bushfire hazard management.
- Remaining values are proposed to be retained.
- No offsets are required as there is no residual loss of moderate or high priority biodiversity values.

Conditions are recommended for inclusion in any permit issued retrospectively approving the vegetation removal within the site, requiring the retention of all remaining trees and requiring implementation of the recommended mitigation measures.

**2.7 Public Consultation and Representations**

The application was advertised in accordance with the requirements of s.57 of the *Land Use Planning and Approvals Act 1993* (from 17 January 2026 to 2 February 2026). A total of 13 representations were received during the public exhibition period. The following issues were raised by the representors:

**2.7.1 Road Suitability**

Hyssop Road is 8.75m to 9m wide with existing kerbs, gutters, and street lighting, whereas Incana Road is narrower (8m) and the proposed driveway crossing is only 6.5m wide.

### Response

Council's engineering assessment confirms that Incana Road is constructed to a local residential standard with a width of approximately **8.5 metres between kerbs**, extending approximately 200m from Bundalla Road to the proposed site access.

The supporting TIA demonstrates that the projected traffic generation (169 daily movements for 26 dwellings) is well within the functional capacity of a local residential road of this standard. The Planning Scheme does not require access to be taken from the "widest" road, only that it be safe and functionally appropriate. Accordingly, Incana Road is deemed suitable to accommodate the proposed access.

#### **2.7.2 Pedestrian Conflict**

The driveway dissects a high-use public walkway/cycleway to the Esplanade used by children, elderly residents, and families

### Response

The permit includes conditions requiring:

- A raised concrete threshold across the vehicle crossover to calm traffic;
- Signage and line marking to give pedestrian priority.

These treatments are standard safety measures for residential driveways intersecting footpaths and are considered sufficient to mitigate pedestrian risk. The level of residential traffic generation (17 peak hour movements) is low and dispersed.

The engineering assessment concludes that pedestrian safety can be appropriately managed through the conditioned design.

#### **2.7.3 Geometric Safety Risks**

The Incana Road access is located on the outside of a horizontal bend. Residents provided annotated diagrams arguing that once houses and 1200mm fences are built at 77 and 83 Incana Road, sightlines will be reduced to less than 4 seconds for merging traffic.

### Response

The applicant's TIA includes a sight distance analysis undertaken in accordance with Austroads design guidelines, the recognised standard for road safety assessment.

Council's engineering review confirms:

- Sight distance exceeds 100 metres to the left of the access;
- A minimum of 65 metres is available for right-turning vehicles entering the site;
- These measurements comply with Austroads parameters.

While representors provided annotated diagrams suggesting potential reduced sightlines, Council must rely on measured and standardised engineering analysis rather than hypothetical scenarios. Based on the certified TIA and engineering review, the proposed access provides adequate sight distance and safe operation.

**2.7.4 Traffic Increase:**

The development anticipates 169 daily vehicle trips for the first 26 units. Residents argue the Beach Road roundabout is already at peak capacity and cannot absorb this increase, which they expect will double if Stage 3 proceeds.

Response

Council must assess the application as lodged (26 dwellings), not speculative future development.

The TIA concludes that Incana Road has sufficient capacity to accommodate the projected movements and that the surrounding network can function appropriately. No objection has been raised by Council's engineering staff on capacity grounds.

There is no technical evidence before Council demonstrating that the proposal would cause unacceptable network failure.

**2.7.5 Traffic Impact Assessment Integrity**

The supporting TIA relies on outdated 2013 NSW standards instead of the mandatory August 2020 Tasmanian Department of State Growth TIA Guidelines.

Response

The TIA has been reviewed by Council's engineering staff. Sight distance and access design were assessed against Austroads guidelines, which remain the recognised benchmark for road design in Australia.

**2.7.6 Zone Inconsistency**

The site is currently in a Low Density Residential Zone. Proposed lot sizes of approximately 325m<sup>2</sup> are viewed as an overdevelopment inconsistent with the surrounding character, where recent lots are 1,000m<sup>2</sup> or larger.

Response

While the site area per dwelling is less than 1000m<sup>2</sup>, the planning scheme expressly provides a performance pathway under Clause 12.4.9 P1. Greater density is therefore not prohibited, but assessed against character, natural values, and infrastructure capacity.

The applicant's density analysis demonstrates that the surrounding locality contains numerous properties with site areas per dwelling below 1000m<sup>2</sup>, and that the average density in the defined surrounding Low Density Residential Zone (Area C) is 674m<sup>2</sup>. The proposal at 739m<sup>2</sup> per dwelling is therefore not considered out of character. An assessment against Clause 12.4.9 P1 is provided in section 2.6 of this report.

**2.7.7 Site coverage calculations**

Residents allege the applicant uses the total 1.922 ha site area to artificially dilute density and site coverage figures. They calculate actual site coverage for the developed portion at 41%, exceeding the 25% acceptable limit.

Response

Site coverage under the Planning Scheme is assessed against the total parent lot area, rather than against a selectively defined developed portion of the site. While the proposal includes internal strata boundaries to enable individual unit

ownership, these are internal legal arrangements only and do not alter Council's assessment of the land as a single parent lot for planning purposes. On this basis, the proposal achieves approximately 18% site coverage and 68% pervious area, as identified in the planning report. The 25% acceptable solution therefore applies to the site as a whole. When assessed against the total lot area, the proposal complies with the relevant site coverage provisions.

#### **2.7.8 Distance to commercial centre**

The applicant claims the site is within 1km of the Margate commercial centre. Residents refute this, citing a distance of 1.4km to 2km and stating the closest "local business" zone is actually a cemetery.

##### Response

The Planning Scheme does not impose a strict 1km threshold test. Proximity to services is a contextual consideration rather than a determinative numerical requirement. Regardless of minor variations in measured distance to the commercial centre, the site is located within the established urban settlement of Margate and within the TasWater serviced area. Importantly, the nearest bus stop is approximately 550 metres from the site, providing reasonable access to public transport. The proposed development is therefore appropriately characterised within an existing serviced residential area.

#### **2.7.9 Visual character – two-storey dwellings**

The inclusion of eight two-storey dwellings is considered "out of character" with the single-storey residential pattern in surrounding cul-de-sacs.

##### Response

The proposal includes a mix of single and two-storey dwellings. Two-storey dwellings are not prohibited within the zone and the buildings remain within the permitted 8.5m height envelope.

The overall built form remains low-rise and landscaped. The inclusion of limited two-storey dwellings does not fundamentally alter the residential character.

#### **2.7.10 Urban Heat and Amenity**

Submissions highlight potential Urban Heat Island effects from increased concrete surfaces and the loss of natural airflow across the landscape.

##### Response

The proposal achieves approximately 18% site coverage and 68% pervious area, indicating that a substantial proportion of the site remains landscaped and permeable. The development does not demonstrate excessive hardstand dominance. Notwithstanding this, it is recommended that a detailed landscaping plan prepared by a suitably qualified professional be provided as a condition of permit, including provision for deep soil tree plantings, particularly adjacent to larger hardstand areas, to further reinforce visual amenity and mitigate potential urban heat effects.

#### **2.7.11 Ecological Loss**

Concerns exist regarding the removal and burning of large gum trees in 2025 prior to the application. The site is noted as habitat for the Green Rosella and the rare Forty-spotted Pardalote. Concerns were also expressed in relation to the lack of environmental assessments.

Response

Based on the available information, the vegetation removed from the site prior to the lodgement of the application contained a corpse of smaller native trees and understorey species. There is insufficient evidence to confirm whether any of these trees were of high conservation value, however aerial imagery indicates these trees were smaller than the high conservation value trees remaining on the site.

Apart from the removal of this vegetation, all remaining trees on and adjacent to the site are proposed to be retained. These trees provide potential habitat for the endangered swift parrot. There are no *E. viminalis* trees providing potential habitat for the forty-spotted pardalote on the site.

**2.7.12 Technical Omissions & Information Gaps**

Multiple submissions identified critical reports and data missing from the public application package:

- Noise Impact Assessment: Referenced as commissioned to address the nearby metal fabrication factory, but results were omitted.
- Natural Values & Arborist Reports: Despite mentions in the text, these reports were not provided, leaving the impact on biodiversity and recently cleared vegetation unassessed.
- Landscaping Plan: No comprehensive plan exists beyond isolated photographs of plants.
- Infrastructure Calculations: A lack of specific data on the capacity of water, sewer, and stormwater infrastructure to handle the ultimate build-out of 52+ units.

Response

The concerns raised regarding alleged technical omissions are noted; however, the site is located within the Low Density Residential Zone, where Council's consideration of noise primarily relates to non-residential sources, and therefore a noise assessment is not strictly required for residential development. Notwithstanding this, the applicant has voluntarily provided a Noise Assessment Report, and no concerns have been raised by Council's technical experts.

In relation to natural values, the applicant provided an arborist assessment as a part of this application. A landscaping plan has been provided within the architectural drawing set (Bundle Sheet 08 of 79), clearly identifying planting areas and landscaped zones. The proposal has also been referred to TasWater and TasNetworks as the relevant infrastructure service providers, and no capacity concerns have been raised; accordingly, there is no evidentiary basis to conclude that the development exceeds available servicing capacity.

Where additional reports (e.g., noise, arborist) are required to fully assess impacts, Council may request further information.

The absence of certain documents at exhibition does not invalidate the application if sufficient information exists to assess compliance.

**2.7.13 Legal Status and Land Ownership**

Evidence was provided (including 2018 correspondence) that Kingborough Council and the Crown have previously denied ownership of the "unformed land"

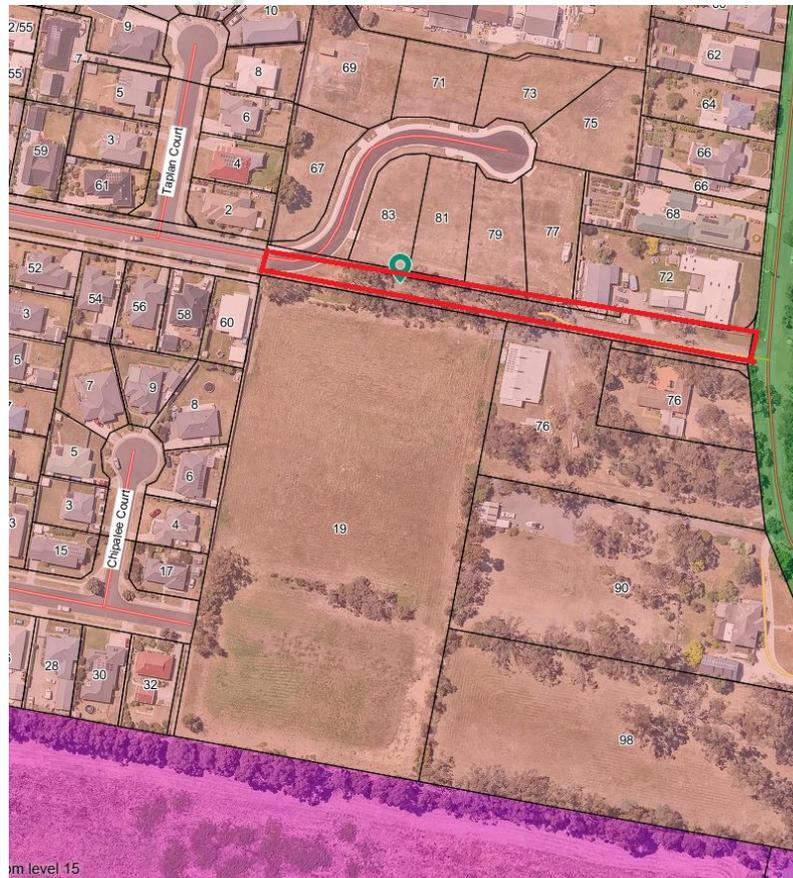
between Incana Road and the site. Therefore, council may not have rights to allow any potential works over that piece of land.

Response

The titles submitted with the application identify the relevant land as road reserve. Council sought independent legal advice regarding the status of the eastern section of Incana Road and how owner consent requirements under the *Land Use Planning and Approvals Act 1993* (LUPAA) should be approached. The legal advice confirms that the eastern section is not Crown land and is most likely a remnant privately owned strip originating from historical subdivision of the Elliot/Lucas estate

Importantly, the advice distinguishes between the formed carriageway currently constructed and maintained by Council and the vegetated portion beyond it. For that portion of the eastern section within the formed carriageway and under the administrative control and maintenance of Council, Section 52(1B) of LUPAA applies. In those circumstances, the application must be signed by and accompanied by written consent from the General Manager. The advice confirms that Ministerial consent is not required, as the land is not Crown land. Beyond the maintained carriageway, the land remains privately owned and not subject to public rights of access.

Accordingly, in relation to the access crossover across the Council-maintained and administered portion of Incana Road, General Manager consent under Section 52(1B) has been properly obtained. Council’s position is consistent with the independent legal advice, which confirms that consent from the Minister is not required and that General Manager consent is sufficient for the portion of land under Council’s administrative control.



**Figure 6: Road Reserve Subject to Submission Concerns (Highlighted in Red)**

### **2.7.14 Previous Assurance**

Some residents state they were explicitly told by Council during their purchase in 2024 that there would be "no way" the 19 Hyssop Road site would have entry/exit onto Incana Road.

#### Response

Alleged previous informal and verbal assurances to individual property owners cannot override the statutory planning process.

## **2.8 Other Matters**

### **2.8.1 Waste collection**

It is acknowledged that the proposal will provide for waste bins stored within the private outdoor area of each dwelling. However, given the nature of the proposal and its layout Council will not provide dwelling based waste collection services and private waste collection will be required to service the proposed development. The requirement for the provision for a private waste service agreement has been included as a recommended condition of any approval.

### **2.8.2 Weed management**

In accordance with Clause 8.11.3 a condition has been included requiring the implementation of best practice hygiene measures.

### **2.8.3 TasNetworks**

TasNetworks were referred the application by Council on 29 October 2025. In their response on 7 November 2025, TasNetworks confirmed the proposed development is not likely to adversely affect Tas Networks operations.

### **2.8.4 TasWater**

Council referred the application to TasWater on 26 November 2025. TasWater responded on 9 December 2025 with its TasWater Submission to Planning Authority Notice which outlines conditions to be applied to any planning permit given by Council, pursuant to the *Water and Sewerage Industry Act 2008 (TAS)* Section 56P(1).

### **2.8.5 Private Open Space over Easements**

It is noted that portions of the private open space areas are located over stormwater and wastewater easements. To protect the integrity of these services and prevent potential damage from deep-rooted vegetation, a permit condition is recommended requiring that only lawn or shallow-rooted shrubs be planted within the easement areas. This will ensure ongoing access for maintenance while maintaining usable landscaped space for residents.

## **3. CONCLUSION**

The proposal involves development which is categorised as permitted use under the planning scheme. The development as proposed relies on the performance criteria to comply with applicable standards and accordingly requires a discretionary assessment.

The proposal is assessed as complying with most use and development standards in the Low Density Zone, as well as the applicable code standards. Where the proposal cannot meet the permitted standard, it can satisfy the corresponding performance criteria. Subsequently the application is recommended for approval subject to conditions.

#### 4. RECOMMENDATION

That the Planning Authority resolves that the development application for 26 multiple dwellings and access works over adjoining road reserves at 19 Hyssop Road and adjoining road reserves, Margate for SJM Property Developments Pty Ltd be approved subject to the following conditions:

1. Except as otherwise required by this Permit, use and development of the land must be substantially in accordance with Development Application No. DA-2025-371 and Council Plan Reference No. P3 submitted on 12 January 2025.

This Permit relates to the use of land or buildings irrespective of the applicant or subsequent occupants, and whoever acts on it must comply with all conditions in this Permit. Any amendment, variation or extension of this Permit requires further planning consent of Council.

2. Prior to the commencement of any on-site works, a detailed Landscaping Plan must be prepared by a qualified landscape architect or suitably experienced landscape professional and approved by the Manager Development Services. Once endorsed, the plan forms part of this permit.

The plan must demonstrate how landscaping will relieve visual impact, contribute to the character of the area, and protect Council and TasWater infrastructure.

The landscaping plan must include all of the following requirements:

- (a) Deep Soil Canopy Trees
  - i. Provision of living, established canopy trees located within deep soil zones with a total area equivalent to not less than 5% of all parking and access areas.
  - ii. Each tree must be capable of achieving a minimum mature canopy diameter of 3–5 metres.
  - iii. Trees must be located along site boundaries, internal accesses, parking areas and common open space.
  - iv. Trees must not be located in private open space, except where located between the primary façade and the access.
- (b) Diversity in plant types and sizes
  - (i) Provision of perennial trees, shrubs, understory and groundcover plants within all areas identified for landscaping on the plan.
  - (ii) Planting must exclude private open space, except where located between the primary façade and the access.
- (c) Infrastructure Protection Requirements
  - (i) Trees located within 2 metres laterally of any Council or TasWater infrastructure must be planted within a closed root-barrier structure (e.g., tub, raised garden bed, or equivalent containment system) designed to prevent root intrusion into pipe trenches.
  - (ii) No trees are permitted within 1 metre laterally of Council or TasWater infrastructure.
  - (iii) Landscaping located directly over Council or TasWater infrastructure must be limited to lawn or shallow-rooted species only.
- (d) Existing Trees & Hard landscaping
  - (i) Identification of all existing trees to be retained.

- (ii) Specification of protection measures during construction consistent with Conditions 7 and 8.
- (iii) Location and details of all proposed fencing.
- (iv) All proposed earth shaping, including retaining wall(s)
- (e) Planting Schedule
  - (i) A planting schedule detailing for each species:
    - botanical name (genus and species)
    - common name
    - quantity
    - expected mature height
    - expected mature canopy size
  - (ii) Artificial plants or astroturf are not permitted.
- (f) Maintenance Schedule
  - (i) A minimum 12-month maintenance schedule, including watering, fertilising, weed control and plant establishment measures

For Advice: A deep soil zone is an area of soil with no impeding building structure or feature above or below, which supports growth of medium to large canopy trees.

It is recommended that the consideration be given to Council's *Landscape Guidelines (Preparing a Landscape Plan)*, which is available on Council's website.

Once endorsed the Landscaping Plan will form part of the permit.

3. Prior to commencement of on-site works associated with the private infrastructure to service the approved dwellings, engineering design drawings must be submitted to Council for approval. The engineering plans and specifications must be prepared and certified by a professional Civil Engineer, in consultation with the Project Arborist. Plans must be to satisfaction of the Director Engineering Services and comply with:
  - Tasmanian Standard Drawings
  - Austroads Standards and Australian Standards
  - Australian Rainfall and Runoff Guidelines
  - The Bushfire Hazard Report and Bushfire Hazard Management Plan (David Lyne, 5 November 2025).

The Plans must include, but are not limited to:

- (a) Detailed internal vehicular and pedestrian access, carparking and manoeuvring areas including:
  - (i) Longitudinal and cross sections of the driveway/access road.
  - (ii) Contours, finish levels and gradients of the driveway/access road.
  - (iii) Provision of a raised concrete threshold across the vehicle access crossover.
  - (iv) Signage and line marking installation across the concrete threshold to give priority to pedestrian movements on Incana Road footpath and the reserve path.

- (v) Wheel stops for open parking bays (as appropriate) and
  - (vi) Marking of residential and visitor parking bays with all visitor parking bays to be included in common property for strata title applications.
  - (vii) Provision of a compliant access and fire hydrants in accordance with sections 4.2 and 4.3 of the Bushfire Hazard Report and Bushfire Hazard Management Plan (David Lyne, 5 November 2025).
- (b) Design (including supporting documentation and hydraulic calculations) of the proposed stormwater infrastructure including:
- (i) Stormwater layout details to service all dwellings.
  - (ii) A stormwater quality treatment system as required in Table E7.1 of the Kingborough Interim Planning Scheme 2015
  - (iii) A stormwater detention system as required in Table E7.1 of the Kingborough Interim Planning Scheme 2015.
  - (iv) Overland flowpaths sized to accommodate the estimated 1% AEP (Annual Exceedance Probability) flow.
  - (v) Soil and water management plan
- (c) A stormwater reticulation main upgrade and extension to service the proposed development connecting to the existing infrastructure in accordance with Council standards and as follows:
- (i) Longitudinal section of the stormwater main extension.
  - (ii) Minimum 750mm class SN8 main from the existing stormwater maintenance pit located adjacent to Incana Road, extension through the subject lot and connection to the existing stormwater headwall in the road reserve.
  - (iii) Provision of 'as-constructed' plans in Autocad format in accordance with Council Survey Requirements for Capital Works Projects Procedure 1.1.5.
- (d) Tree retention and protection measures, including:
- (i) the surveyed location of individual native trees with a diameter >25cm at 1.4m from natural ground level and their associated tree protection zones relative to all civil works on all civil drawings, consistent with the Arboricultural Assessment (Philip Jackson December 2025 version 2) and Addendum (Philip Jackson 17 December 2025 version 2);
  - (ii) demonstration all works are designed, located and will be constructed in accordance with the Arboricultural Assessment (Philip Jackson December 2025 version 2) and Addendum (Philip Jackson 17 December 2025 version 2), including demonstrating that:
    - a. the upgraded path in the Incana road casement will be constructed above the present grade using no-dig methods, including no excavation or compaction, in accordance with Item 6.6 of the TPS;
    - b. fence post footings will be excavated by hand in the tree protection zone (TPZ) and structural root zone (SRZ) and offsetting posts to avoid significant roots (>40mm in diameter) in accordance with sub-item 6.4.1 of the TPS.
    - c. the placement of controlled fill will be limited to the footprint of the proposed units.

- d. remaining fill areas only use friable and non-compacted material equivalent in texture to the existing site topsoil in accordance with sub-item 6.4.2 of the TPS. The fill may be consolidated but must not be compacted to engineering standards.
- e. the new stormwater pipe will be installed above the present grade and along the alignment of the existing swale drain basin, with no excavation and bedding and backfilling the pipe with gap-graded aggregate to avoid compacted fill.
- f. the western end of the stormwater pipe will be connected to the existing main at a culvert without excavation and outside the TPZ of any trees.

Once endorsed the plans will form part of the permit and all works must be undertaken in accordance with the plans.

- 4. A Council fee of 2% of the estimated value of the civil engineering construction works (future Council/public infrastructure only) including GST, provisional items and contingencies, for the development or the current minimum fee, whichever is the greater, must be paid at the time of submission of the engineering design plans for approval. The actual amounts payable shall be based on the rates adopted by Council and prevailing at the time of payment.
- 5. Prior to the commencement of any on-site works, including vegetation removal or modification, demolition, construction, excavations, placement of fill, delivery of building/construction materials and/or temporary buildings, an 'Application for Approval of Planning Start of Works Notice' must be lodged with Council's Planning Department.

This application must be lodged a minimum of 14 days prior to commencement of on-site works and works must not commence until this notice has been approved by the Manager Development Services.

For Advice: This Planning Start of Works Notice is not the same as the Form 39 Building Start Works Notice. As such, lodgment of a Form 39 will not satisfy this condition and a separate 'Application for Approval of Planning Start of Works Notice' must be lodged with Council's Planning Department. A copy of the application form is available on Council's website.

- 6. Approved vegetation removal is limited to the retrospective removal of a small copse of native vegetation within the footprint of the approved development.

No further felling, lopping, ringbarking or otherwise injuring or destroying of native vegetation or individual trees is to take place without the prior written permission of Council.

- 7. All remaining native vegetation identified for retention in Council Plan Reference No. P2 (submitted on 18 December 2025) and P3 (submitted on 12 January 2026) and the Arboricultural Assessment (Philip Jackson December 2025 version 2) and Addendum (Philip Jackson 17 December 2025 version 2) must be appropriately protected during and after construction in accordance with all the recommendations in the arboricultural assessments and AS 4970-2025. This includes but is not limited to implementation of the following measures:

A. Prior to Construction:

Prior to the commencement of any on-site works (including but not limited to excavations, placement of fill, delivery of building/construction materials and/or temporary buildings):

- (a) Appointing a Project Arborist.

- (b) Conducting a site briefing between the Project Arborist and the project manager and site supervisor or equivalent for the development, including provision of the Tree Protection Plan (TPP).
  - (c) Installing tree protection fencing in accordance with the Tree Protection Plan and Section 6.3 of the Tree Protection Schedule (TPS) (Philip Jackson December 2025 version 2) and condition 8.
  - (d) Providing certification by the Project Arborist of satisfactory implementation of these tree protection measures to the Manager Development Services prior to the commencement of any on-site works.
- B. During Construction:
- (a) Maintaining tree protection measures required above for the duration of the construction.
  - (b) Undertaking all works in accordance with the Arboricultural Assessment (Philip Jackson December 2025 version 2) and Addendum (Philip Jackson 17 December 2025 version 2) and endorsed engineering drawings, including:
    - (i) Constructing the upgraded path in the Incana road casement above the present grade using no-dig methods, including no excavation or compaction, in accordance with Item 6.6 of the TPS.
    - (ii) Excavating fence post footings by hand in the tree protection zone (TPZ) and structural root zone (SRZ) and offsetting posts to avoid significant roots (>40mm in diameter) in accordance with sub-item 6.4.1 of the TPS.
    - (iii) Limiting the placement of controlled fill to the footprint of the proposed units.
    - (iv) Ensuring remaining fill areas only use friable and non-compacted material equivalent in texture to the existing site topsoil in accordance with sub-item 6.4.2 of the TPS. The fill may be consolidated but must not be compacted to engineering standards.
    - (v) Installing the new stormwater pipe above the present grade and along the alignment of the existing swale drain basin, with no excavation and bedding and backfilling the pipe with gap-graded aggregate to avoid compacted fill.
    - (vi) Connecting the western end of the stormwater pipe to the existing main at a culvert without excavation and outside the TPZ of any trees.
  - (c) Ensuring the Project Arborist conducts a monthly site audit during construction to check compliance against the tree protection plan and permit conditions.
  - (d) Ensuring the Project Arborist is consulted prior to any works and/or excavation with the SRZ or TPZ of trees and prior to cutting any significant roots within the TPZ.
- C. Post Construction:
- (a) Providing written certification by the Project Arborist of satisfactory implementation of the measures identified in B. above to the Manager Development Services upon completion of construction.
  - (b) Adhering to the following tree management measures post construction for all areas within the tree protection zone but outside the footprint of the approved works:
    - (i) the existing soil level must not be altered around the tree protection zone of the trees (including the disposal of fill, placement of materials or the scalping of the soil);

- (ii) the tree protection zone must be free from the storage of fill, contaminants or other materials;
  - (iii) machinery and vehicles are not permitted to access the tree protection zone; and
  - (iv) development and associated works are not permitted unless otherwise approved by Council in writing or otherwise in accordance with the law.
8. Tree protection fencing required under condition 8 must:
- (a) Be located on the edge of the tree protection zone, unless the outer edge of works as shown on the endorsed plan are closer, in which case this tree/vegetation protection fencing may be reduced to the minimum amount necessary to allow the works to be completed.
  - (b) Exclude the following from the tree protection zones:
    - (i) Machine excavation including trenching.
    - (ii) Machinery movement.
    - (iii) Excavation of silt fencing.
    - (iv) Cultivation.
    - (v) Storage.
    - (vi) Preparation of chemicals, including preparation of cement products.
    - (vii) Parking of vehicles and plant.
    - (viii) Refuelling.
    - (ix) Dumping waste.
    - (x) Placement of fill.
    - (xi) Lighting of fires.
    - (xii) Soil level changes.
    - (xiii) Temporary or permanent installation of utilities and signs.
    - (xiv) Physical damage to the trees.
  - (c) Be constructed in accordance with the following requirements:
    - (i) Utilise barrier mesh and star pickets fencing unless otherwise approved in writing.
    - (ii) Form a visual and physical barrier.
    - (iii) Be a minimum height of 1.5 metres above ground level.
    - (iv) Include signage clearly marked "Tree Protection Zone - No Entry" on all sides.
9. Bushfire hazard management measures for the use and development must be implemented consistent with Council Plan Reference No. P3 (submitted on 12 January 2026) and the Bushfire Hazard Report and Bushfire Hazard Management Plan (David Lyne, 5 November 2025), including the construction standard of the units, the extent and location of vegetation retention and removal and/or modification to establish the hazard management area, and water supply and access arrangements.
- Once implemented, all bushfire hazard management measures must be maintained for the life of the approved use and development in accordance with Council Plan Reference

No. P3 (submitted on 12 January 2026) and the Bushfire Hazard Report and Bushfire Hazard Management Plan (David Lyne, 5 November 2025).

For Advice: Documentation submitted for building approval must demonstrate compliance with these measures.

10. To reduce the spread of weeds or pathogens, all machinery must take appropriate hygiene measures prior to entering and leaving the site as per the Weed and Disease Planning and Hygiene Guidelines 2015 produced by the Department of Primary Industries, Parks, Water and Environment.

Any imported materials must be from a weed and pathogen free source to prevent introduction of new weeds and pathogens to the area.

11. All waste material generated by the development or from other sources must be contained in appropriate building waste containers for periodic removal to a licensed disposal site. The receptacle must be of a size to adequately contain the amount of waste generated and must be appropriately located on the subject site and must not impede residential traffic or parking at any time.
12. The construction works must be undertaken in accordance with the approved drawings. All works relating to Council's assets must be to the Council's standards and to the satisfaction and approval of the Director Engineering Services. These works must be inspected by Council prior to backfill. The supervising Engineer must request a joint onsite inspection with the Council's authorised representative.

On completion of works relating to Council's assets, a written certification from the developer's supervising professional engineer must be submitted to and approved by the Director Engineering Services that states:

- (a) works have been completed in accordance with the Council approved engineering design drawings, specifications and Council standards; and
- (b) the appropriate levels of quality and workmanship have been achieved

At the end of the project, acceptable "As Constructed" drawings certified as correct by a registered land surveyor in accordance with Council's Survey requirements for subdivisions and developments must be submitted to Council along with CCTV evidence of the Council's stormwater infrastructure to confirm any damage to the stormwater pipe. If damage has occurred, the applicant/developer will be responsible to carry out repair or replacement works as per Council's standards and requirements and bear its cost.

13. The applicant/developer must lodge a maintenance bond or bank guarantee of 10% of the total cost of the civil engineering construction works (future Council/public infrastructure only) including GST to cover the satisfactory rectification of all defects and defective works during the statutory 52 weeks maintenance period prior to the issue of Certificate of Practical Completion for the civil works (future Council/public infrastructure only).
14. The satisfactory completion of all public infrastructure works will be considered achieved when:
  - (a) a Certificate of Practical Completion has been issued
  - (b) all defects and any defective works have been satisfactorily rectified at the completion of the 52 weeks maintenance period
  - (c) Council will be entitled to call upon the bond and bank guarantee funds to complete or rectify any outstanding defective works after the expiry of the maintenance period
  - (d) at the end of the statutory 52 weeks maintenance period, the supervising Engineer must request a joint on-site inspection with the Council's authorised representative to confirm that all outstanding defects and defective works have been satisfactorily completed

- (e) upon satisfactory completion of all outstanding defects and defective works Council will issue a notice of satisfactory 'Final Inspection' and the public assets will be taken over by Council; and
  - (f) after takeover of the works, it will be the responsibility of the supervising engineer or applicant/developer to request Council to release all Bond and Bank Guarantee monies.
15. At least six (6) visitor parking spaces must be provided for the proposed development. The visitor parking spaces must be appropriately signposted and kept available for visitor parking at all times. Any future application for strata title in respect of the property must ensure that the visitor parking space is included within the common property on the strata plan and be accessible through the common property from all units.
  16. The applicant must make provision for a private waste service agreement to enable the development to be serviced with waste and recycling collection. This agreement is to ensure these services are provided to all individual units as indicated on the submitted plans. Council waste services will not be provided to service the development.
  17. Erosion and sedimentation control measures are to be implemented during the construction phase of the development.

Prior to the commencement of site works an erosion and sedimentation control plan shall be provided to Council in accordance with the following:

- Hobart regional soil and water management code of practice
  - Kingborough Planning Scheme 2000
  - The satisfaction of the Executive Manager - Engineering Services
  - The satisfaction of the Director Community Planning and Development.
18. Prior to the occupation of any dwelling, landscaping must be provided in accordance with the landscaping plan endorsed under condition 2 and evidence of completion must be provided to the Manager Development Services.  
  
The landscaping areas shown on the endorsed plans must be used for landscaping and no other purpose and any landscaping must be maintained to the satisfaction of the Manager Development Services, including that any dead, diseased or damaged plants are to be replaced.
  19. At least three (3) visitor parking spaces must be provided for the proposed development. The visitor parking spaces must be appropriately signposted and kept available for visitor parking at all times. Any future application for strata title in respect of the property must ensure that the visitor parking space is included within the common property on the strata plan and be accessible through the common property from all units.
  20. The area between the dwelling and the access, excluding the approved driveway and pedestrian path, must not be sealed or finished with hard landscaping materials such as pebbles or artificial turf, unless otherwise approved by the Manager Development Services. The predominant ground treatment within this area must consist of vegetation such as lawn or garden.
  21. The conditions as determined by TasWater, and set out in the attached Appendix A, form part of this permit.

**ADVICE**

- A. In accordance with section 53(5) of the *Land Use Planning and Approvals Act 1993* this permit lapses after a period of two years from the date on which it is granted if the use or development in respect of which it is granted is not substantially commenced within that period.
- B. The approval in this permit is under the *Land Use Planning and Approvals Act 1993* and does not provide any approvals under other Acts including, but not limited to *Building Act 2016*, *Urban Drainage Act 2013*, *Food Act 2003* or Council by-laws.

If your development involves demolition, new buildings or alterations to buildings (including plumbing works or onsite wastewater treatment) it is likely that you will be required to get approvals under the *Building Act 2016*. Change of use, including visitor accommodation, may also require approval under the *Building Act 2016*. Advice should be sought from Council's Building Department or an independent building surveyor to establish any requirements.

- C. An application for Notifiable Plumbing Work must be lodged with Council before commencing any work.
- D. A drainage design plan at a scale of 1:200, designed by a qualified Hydraulic Designer, showing the location of the proposed sewer and stormwater house connection drains; including the pipe sizes, pits and driveway drainage, must be submitted with the application for Plumbing Permit.
- E. The Developer should not allocate any property address numbers for the proposed units. Details of new property addresses will be provided prior to any Strata Plans lodged for sealing.

**ATTACHMENTS**

1. **Assessment Checklist**
2. **Proposed development plans**
3. **TasWater submission**

**Assessment checklist.**

**Use Status**

|                   |  |
|-------------------|--|
| <b>Use Class</b>  | <b>Residential – 26 multiple dwellings</b> |
| <b>Use Status</b> | <b>Permitted</b>                           |

**Low Density Residential Zone Provisions (multiple dwellings)**

*Checklist is based on KIPS2015 and provisions of PD8 (which commenced 22 Feb 2022)*

| <b>Clause</b>   | <b>Compliance/Comments</b>   |
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| <p><b>Clause 12.4.2 - Setbacks and building envelope</b></p> <p><b>A1</b> - Unless within a building area, a dwelling, excluding protrusions (such as eaves, steps, porches, and awnings) that extend not more than 0.6 m into the frontage setback, must have a setback from a frontage that is:</p> <p>(a) if the frontage is a primary frontage, at least 4.5 m, or, if the setback from the primary frontage is less than 4.5 m, not less than the setback, from the primary frontage, of any existing dwelling on the site; or</p> <p>(b) if the frontage is not a primary frontage, at least 3 m, or, if the setback from the frontage is less than 3 m, not less than the setback, from a frontage that is not a primary frontage, of any existing dwelling on the site; or</p> <p>(c) if for a vacant site with existing dwellings on adjoining sites on the same street, not more than the greater, or less than the lesser, setback for the equivalent frontage of the dwellings on the adjoining sites on the same street.</p> <p><b>A2</b> - A garage or carport must have a setback from a frontage of at least:</p> <p>(a) 5.5 m, or alternatively 1m behind the façade of the dwelling; or</p> | <p><b>A1 – Complies</b></p> <p>The building primary frontage is Hyssop Road. No buildings are within the 4.5m setback No building are within 3m setback from the secondary frontage to Incana Road.</p> <p><b>A2 (a) to (c) – Complies</b></p> <p>The proposal does not contain any garage within 5.5m of the setback.</p> |

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| <p>(b) the same as the dwelling façade, if a portion of the dwelling gross floor area is located above the garage or carport; or</p> <p>(c) 1m, if the natural ground level slopes up or down at a gradient steeper than 1 in 5 for a distance of 10 m from the frontage.</p> <p><b>A3</b> - A dwelling, excluding outbuildings with a building height of not more than 2.4m and protrusions (such as eaves, steps, porches, and awnings) that extend not more than 0.6m horizontally beyond the building envelope, must:</p> <p>(a) be contained within a building envelope (refer to diagrams 12.4.2A, 12.4.2B, 12.4.2C and 12.4.2D) determined by:</p> <p>(i) a distance equal to the frontage setback or, for an internal lot, a distance of 4.5m from the rear boundary of a lot with an adjoining frontage; and</p> <p>(ii) projecting a line at an angle of 45 degrees from the horizontal at a height of 3m above natural ground level at the side boundaries and a distance of 4m from the rear boundary to a building height of not more than 8.5m above natural ground level; and</p> <p>(b) only have a setback within 1.5m of a side boundary if the dwelling:</p> <p>(i) does not extend beyond an existing building built on or within 0.2m of the boundary of the adjoining lot; or</p> <p>(ii) does not exceed a total length of 9m or one-third the length of the side boundary (whichever is the lesser).</p> | <p><b>A3(a)</b> – Complies</p> <p>The architectural plans indicate that all the buildings are within the permitted building envelope.</p> <p><b>A3(b)</b> – Complies</p> <p>No dwelling within 1.5m</p>  |
| <p><b>A4</b> - No trees of high conservation value will be impacted.</p>   | <p><b>A4 – Does not comply</b></p> <p>The arboricultural assessments (Philip Jackson November 2025 and 17 December 2025) confirm that the proposed development has:</p> <ul style="list-style-type: none"> <li>an acceptable impact on two (2) very high and one (1) high conservation value trees;</li> </ul> |

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|  | <ul style="list-style-type: none"> <li>• a moderate and tolerable impact on one (1) very high and one (1) high conservation value trees; and</li> <li>• an unacceptable impact on one (1) very high and three (3) high conservation value trees;</li> </ul> <p>As the development impacts on high and very high conservation value trees, it requires assessment against the performance criteria.</p>   |
| <p><b>Clause 12.4.3 - Site coverage and private open space</b></p> <p><b>A1</b> – Dwellings must have:</p> <p>(a) a site coverage of not more than 25% (excluding eaves up to 0.6m); and</p> <p>(b) a site area of which at least 25% of the site area is free from impervious surfaces;</p> <p>(c) for multiple dwellings, a total area of private open space of not less than 60 m<sup>2</sup> associated with each dwelling.</p> <p><b>A2</b> - A dwelling must have an area of private open space that:</p> <p>(a) is in one location and is at least:</p> <p style="margin-left: 20px;">(i) 24 m<sup>2</sup>; or</p> <p style="margin-left: 20px;">(ii) 12 m<sup>2</sup>, if the dwelling has a finished floor level that is entirely more than 1.8 m above the finished ground level (excluding a garage, carport or entry foyer); and</p> <p>(b) has a minimum horizontal dimension of:</p> <p style="margin-left: 20px;">(i) 4 m; or</p> <p style="margin-left: 20px;">(ii) 2 m, if the dwelling has a finished floor level that is entirely more than 1.8 m above the finished ground level (excluding a garage, carport or entry foyer); and</p> | <p><b>A1(a)</b> – Complies</p> <p>The proposed building coverage is 18%</p> <p><b>A1(b)</b> – Complies</p> <p>68% of the site area is free from impervious surface</p> <p><b>A1(c)</b> – Complies</p> <p>The proposed landscape plan only indicates a minimum of 128.62 m<sup>2</sup> POS for each lot, therefore it must be assessed under relevant performance criteria</p> <p><b>A2(a) – (c)</b> – Complies</p> <p>Each dwelling has a POS with dimension 6mx4m and directly accessible from a habitable room.</p> <p><b>A2(d) – Does not Comply</b></p> <p>The private open space (POS) areas for Units 13–15 are located to the south or southeast of the site. The submitted shadow analysis demonstrates that the nominated 24m<sup>2</sup> POS for each unit receives at least 3 hours of sunlight to a minimum of 50% of the area between 9.00am and 3.00pm on 21 June, thereby satisfying the relevant requirement. While additional open space areas are provided around each dwelling beyond the designated 24m<sup>2</sup> POS, these areas do not individually meet the specified solar access standard.</p> |

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| <p>(c) is directly accessible from, and adjacent to, a habitable room (other than a bedroom); and</p> <p>(d) is not located to the south, south-east or south-west of the dwelling, unless the area receives at least 3 hours of sunlight to 50% of the area between 9.00am and 3.00pm on the 21st June; and</p> <p>(e) is located between the dwelling and the frontage only if the frontage is orientated between 30 degrees west of north and 30 degrees east of north, excluding any dwelling located behind another on the same site; and</p> <p>(f) has a gradient not steeper than 1 in 10; and</p> <p>(g) is not used for vehicle access or parking.</p>   | <p><b>A2(e)</b> – Complies</p> <p>Only the private open space (POS) areas for Units 26 and 27 are located to the front of the dwellings; however, both POS areas are oriented toward a favourable northerly aspect.</p> <p><b>A2(f)</b> – Complies</p> <p>The site is flat throughout; no slope is greater than 1:10</p> <p><b>A2(g)</b> – Complies</p> <p>All the POS areas are not for access for parking</p> |
| <p><b>Clause 12.4.4 – Sunlight and overshadowing</b></p> <p><b>A1</b> – A dwelling must have at least one habitable room (other than a bedroom) window that faces between 30 degrees west of north and 30 degrees east of north (see diagram 12.4.4A).</p>   | <p><b>A1</b> – Complies</p> <p>All the dwellings have at least one habitable room (other than a bedroom) window that faces between 30 degrees west of north and 30 degrees east of north</p>  |
| <p><b>A2</b> - A multiple dwelling that is to the north of a window of a habitable room (other than a bedroom) of another dwelling on the same site, which window faces between 30 degrees west of north and 30 degrees east of north (see diagram 12.4.4A), must be in accordance with (a) or (b), unless excluded by (c):</p> <p>(a) The multiple dwelling is contained within a line projecting (see diagram 12.4.4B):</p> <p>(i) at a distance of 3 m from the window; and</p> <p>(ii) vertically to a height of 3 m above natural ground level and then at an angle of 45 degrees from the horizontal.</p> <p>(b) The multiple dwelling does not cause the habitable room to receive less than 3 hours of sunlight between 9.00 am and 3.00 pm on 21st June.</p> <p>(c) That part, of a multiple dwelling, consisting of:</p> | <p><b>A2</b> – Not applicable</p> <p>Units 3 and 4 have windows that are within 3m although these do not face 30 degrees west of north or 30 degrees east of north.</p> <p>For units 7 and 8, which the distance is 2.52m, but there is no window of habitable room facing north for unit 7.</p> <p>For units 13, no window of a habitable room is within 3m of unit 12.</p>                                    |

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| <ul style="list-style-type: none"> <li>(i) an outbuilding with a building height no more than 2.4 m; or</li> <li>(ii) protrusions (such as eaves, steps, and awnings) that extend no more than 0.6 m horizontally from the multiple dwelling.</li> </ul>   |   |
| <p><b>A3</b> - A multiple dwelling, that is to the north of the private open space, of another dwelling on the same site, required in accordance with A2 or P2 of 12.4.3, must be in accordance with (a) or (b), unless excluded by (c):</p> <ul style="list-style-type: none"> <li>(a) The multiple dwelling is contained within a line projecting (see diagram 12.4.4C):             <ul style="list-style-type: none"> <li>(i) at a distance of 3 m from the northern edge of the private open space; and</li> <li>(ii) vertically to a height of 3 m above natural ground level and then at an angle of 45 degrees from the horizontal.</li> </ul> </li> <li>(b) The multiple dwelling does not cause 50% of the private open space to receive less than 3 hours of sunlight between 9.00 am and 3.00 pm on 21st June.</li> <li>(c) That part, of a multiple dwelling, consisting of:             <ul style="list-style-type: none"> <li>(i) an outbuilding with a building height no more than 2.4 m; or</li> <li>(ii) protrusions (such as eaves, steps, and awnings) that extend no more than 0.6 m horizontally from the multiple dwelling.</li> </ul> </li> </ul> | <p><b>A3</b> – Complies</p> <p>For units 16-25, each dwelling is a minimum distance of 3m from the north edge of the private open space, and meets the 3m+45° requirements.</p> |
| <p><b>Clause 12.4.5 - Width of openings for garages and carports</b></p> <p><b>A1</b> – A garage or carport within 12 m of a primary frontage (whether the garage or carport is free-standing or part of the dwelling) must have a total width of openings facing the primary frontage not exceeding 6m or half the width of the frontage (whichever is the lesser).</p>   | <p><b>A1</b> – Not applicable</p> <p>No garage or carport is within 12m of primary frontage</p>   |

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| <p><b>Clause 12.4.6 - Privacy</b></p> <p><b>A1</b> - A balcony, deck, roof terrace, parking space, or carport (whether freestanding or part of the dwelling) that has a finished surface or floor level more than 1 m above natural ground level must have a permanently fixed screen to a height of at least 1.7 m above the finished surface or floor level, with a uniform transparency of no more than 25%, along the sides facing a:</p> <ul style="list-style-type: none"> <li>(a) side boundary, unless the balcony, deck, roof terrace, parking space, or carport has a setback of at least 3 m from the side boundary;</li> <li>(b) rear boundary, unless the balcony, deck, roof terrace, parking space or carport has a setback of at least 4m from the rear boundary;</li> <li>(c) dwelling on the same site, unless the balcony, deck, roof terrace, parking space, or carport is at least 6 m:             <ul style="list-style-type: none"> <li>(i) from a window or glazed door, to a habitable room of the other dwelling on the same site; or</li> <li>(ii) from a balcony, deck, roof terrace or the private open space, or the other dwelling on the same site.</li> </ul> </li> </ul> <p><b>A2</b> - A window or glazed door, to a habitable room, of a dwelling, that has a floor level more than 1 m above the natural ground level, must be in accordance with (a), unless it is in accordance with (b):</p> <ul style="list-style-type: none"> <li>(a) The window or glazed door:             <ul style="list-style-type: none"> <li>(i) is to have a setback of at least 3 m from a side boundary;</li> <li>(ii) is to have a setback of at least 4 m from a rear boundary;</li> <li>(iii) if the dwelling is a multiple dwelling, is to be at least 6 m from a window or glazed door, to a habitable room, of another dwelling on the same site;</li> <li>(iv) if the dwelling is a multiple dwelling, is to be at least 6 m from the private open space of another dwelling on the same site.</li> </ul> </li> </ul> | <p><b>A1(a)</b> – Complies</p> <p>Any balcony or deck with an FFL of more than 1m above NGL are sited at least 3m from any side boundary.</p> <p><b>A1(b)</b> – Complies</p> <p>Any balcony or deck with an FFL of more than 1m above NGL are sited at least 4m from any rear boundary.</p> <p><b>A1(c)</b> – Complies</p> <p>Any balcony or deck with an FFL of more than 1m above NGL are sited at least 6m from any habitable room or balcony / deck of the other dwelling on the same site.</p> <p><b>A2- (a) and (b)</b> – complies</p> <p>Where any proposed window or glazed door, to a habitable room, of a dwelling, has a floor level more than 1 m above the natural ground level, and does not meet the minim setbacks, the subject windows are:</p> <ul style="list-style-type: none"> <li>- offset by 1.5m; or</li> <li>- Have a minimum sill height of 1.7m</li> </ul> |
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| <p>(b) The window or glazed door:</p> <ul style="list-style-type: none"> <li>(i) is to be offset, in the horizontal plane, at least 1.5 m from the edge of a window or glazed door, to a habitable room of another dwelling; or</li> <li>(ii) is to have a sill height of at least 1.7 m above the floor level or has fixed obscure glazing extending to a height of at least 1.7 m above the floor level; or</li> <li>(iii) is to have a permanently fixed external screen for the full length of the window or glazed door, to a height of at least 1.7 m above floor level, with a uniform transparency of not more than 25%.</li> </ul> <p><b>A3</b> - A shared driveway or parking space (excluding a parking space allocated to that dwelling) must be separated from a window, or glazed door, to a habitable room of a multiple dwelling by a horizontal distance of at least:</p> <ul style="list-style-type: none"> <li>(a) 2.5m; or</li> <li>(b) 1m if: <ul style="list-style-type: none"> <li>(i) it is separated by a screen of at least 1.7 m in height; or</li> <li>(ii) the window, or glazed door, to a habitable room has a sill height of at least 1.7 m above the shared driveway or parking space, or has fixed obscure glazing extending to a height of at least 1.7 m above the floor level.</li> </ul> </li> </ul> | <p><b>A3</b> – Complies</p> <p>All parking spaces and shared driveways are located at least a 2.5 m horizontal distance from any window, or glazed door, to a habitable room of a multiple dwelling on the site, and therefore the proposal satisfies the relevant requirement of the clause.</p> |
| <p><b>Clause 12.4.7 - Frontage fences</b></p> <p><b>A1</b> - A fence (including a free-standing wall) within 4.5 m of a frontage must have a height above natural ground level of not more than:</p> <ul style="list-style-type: none"> <li>(a) 1.2 m if the fence is solid; or</li> <li>(b) 1.5 m, if any part of the fence that is within 4.5 m of a primary frontage has openings above a height of 1.2 m which provide a uniform transparency of not less than 30% (excluding any posts or uprights).</li> </ul>   | <p><b>A1</b> – Complies</p> <p>The proposal includes a 1.2m high solid fence within 4.5m of any frontage.</p>   |

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| <p><b>Clause 12.4.8 – Waste storage for multiple dwellings</b></p> <p><b>A1</b> - A multiple dwelling must have a storage area, for waste and recycling bins, that is an area of at least 1.5 m<sup>2</sup> per dwelling and is within one of the following locations:</p> <p>(a) in an area for the exclusive use of each dwelling, excluding the area between the dwelling and the frontage; or</p> <p>(b) in a communal storage area with an impervious surface that:</p> <p>(i) has a setback of at least 4.5 m from a frontage; and</p> <p>(ii) is at least 5.5 m from any dwelling; and</p> <p>(iii) is screened from the frontage and any dwelling by a wall to a height of at least 1.2 m above the finished surface level of the storage area.</p> | <p><b>A1 – Complies</b></p> <p>Each dwelling has a 1.5 m<sup>2</sup> storage area for bins. No storage area is proposed forward to the building line of a dwelling.</p>  |
| <p><b>Clause 12.4.9 – Residential density for multiple dwellings</b></p> <p><b>A1</b> – Multiple dwellings must have a site area per dwelling of not less than 1000m<sup>2</sup>.</p>   | <p><b>A1 – Does not Comply</b></p> <p>The proposed onsite density is 739m<sup>2</sup> per dwelling, a shortfall of 261m<sup>2</sup>. Therefore, the proposal must be assessed under the relevant performance criteria.</p> |

**Code Provisions**

| Clause   | Compliance/Comments |
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| <p><b>E1.0 Bushfire-Prone Areas Code</b></p>   |                     |
| <p>While the proposed development is located within a Bushfire Prone Area, the Bushfire Prone Areas Code (Clause E1.2) does not apply to the Residential use class and no hazardous or vulnerable use is proposed. Notwithstanding, a Bushfire Hazard Report and accompanying BHMP (David Lyne, 5 November 2025) was submitted with the application to demonstrate that the hazard management area is capable of being contained within the lot and does not rely on the additional management of any vegetation within or outside the site, excluding grassland within the site, providing the units are constructed to the following standard:</p> <ul style="list-style-type: none"> <li>• BAL-Low Units 1-4 and 25-26</li> <li>• BAL-12.5 Units 5-6 and 16-24</li> <li>• BAL-19 Unit 7</li> <li>• BAL-29 Units 8-15</li> </ul> |                     |

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| <p>A condition is recommended for inclusion in any permit issued requiring implementation of the bushfire measures.</p>   |  |
| <p><b>E5.0 Road and Railway Assets Code</b></p>   |  |
| <p><b>Clause E5.5.1 – Existing road accesses and junctions</b></p> <p><b>A1</b> – The annual average daily traffic (AADT) of vehicle movements, to and from a site, onto a category 1 or category 2 road, in an area subject to a speed limit of more than 60km/h , must not increase by more than 10% or 10 vehicle movements per day, whichever is the greater.</p>   | <p><b>A1 – Not applicable.</b> As the site is not a Category 1 or 2 road.</p>  |
| <p><b>A2</b> – The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of more than 60km/h, must not increase by more than 10% or 10 vehicle movements per day, whichever is the greater.</p>   | <p><b>A2 – Not applicable.</b> As the area is not subject to a speed limit more than 60km/h.</p>   |
| <p><b>A3</b> – The annual average daily traffic (AADT) of vehicle movements, to and from a site, using an existing access or junction, in an area subject to a speed limit of 60km/h or less, must not increase by more than 20% or 40 vehicle movements per day, whichever is the greater.</p>   | <p><b>A3 – Does not Comply</b></p> <p>The proposal is in an area subject to a speed limit 60km/h or less and the number of vehicle movements will increase by more than 20% or 40 movements per day.</p> |
| <p><b>Clause E5.6.1 - Development adjacent to roads and railways</b></p> <p><b>A1.1</b> – Except as provided in A1.2, the following development must be located at least 50m from the rail network, or a category 1 road or category 2 road, in an area subject to a speed limit of more than 60km/h:</p> <ul style="list-style-type: none"> <li>(a) new buildings;</li> <li>(b) other road or earth works; and</li> <li>(c) building envelopes on new lots.</li> </ul> | <p><b>A1.1 – Not applicable.</b></p>   |
| <p><b>A1.2</b> – Buildings, may be:</p> <ul style="list-style-type: none"> <li>(a) located within a row of existing buildings and setback no closer than the immediately adjacent building; or</li> <li>(b) an extension which extends no closer than:                             <ul style="list-style-type: none"> <li>(i) the existing building; or</li> <li>(ii) an immediately adjacent building.</li> </ul> </li> </ul>  | <p><b>A1.2 – Not applicable.</b> The development is not within 50m of a Category 1 or 2 road, or rail network.</p>   |
| <p><b>Clause E5.6.2 - Road access and junctions</b></p> <p><b>A1</b> – No new access or junction to roads in an area subject to a speed limit of more than 60km/h.</p>  | <p><b>A1 – Not applicable.</b> The speed limit in area being accessed is less than 60km/h.</p>   |

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| <p><b>A2</b> – No more than one access providing both entry and exit, or two accesses providing separate entry and exit, to roads in an area subject to a speed limit of 60km/h or less.</p>  | <p><b>A2 – Complies.</b> The speed limit is 60km/h or less and the access arrangement is one access providing both entry and exit.</p>  |
| <p><b>Clause E5.6.4 - Sight distance at accesses, junctions and level crossings</b></p> <p><b>A1</b> – Sight distances at:</p> <p>(a) an access or junction must comply with the Safe Intersection Sight Distance shown in Table E5.1; and</p> <p>(b) rail level crossings must comply with AS1742.7 Manual of uniform traffic control devices - Railway crossings, Standards Association of Australia.</p>   | <p><b>A1 – Does not Comply</b></p> <p>Table E5.1 does not include sight distances for vehicle speeds less than 50 km/h. Vehicles approaching the site access from the left or cul-de-sac side will approach at estimated speed of 40km/h due to the tight curves from the cul-de-sac side. Therefore, the proposal must be assessed against the Performance Criteria.</p> |
| <p><b>E6.0 Parking and Access Code</b></p>  |   |
| <p><b>Clause E6.6.1 - Number of car parking spaces</b></p> <p><b>A1</b> - The number of on-site car parking spaces must be:</p> <p>(a) no less than the number specified in Table E6.1;</p> <p>except if:</p> <p>(i) the site is subject to a parking plan for the area adopted by Council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;</p>  | <p><b>A1 – Complies</b></p> <p>There are 58 spaces provided onsite. Each unit has at least 2 parking spaces and 6 visitor spaces are provided. The parking provided is in accordance with the planning scheme standards..</p>   |
| <p><b>Clause E6.6.3 – Number of Motorcycle Parking Spaces</b></p> <p><b>A1</b> - The number of on-site motorcycle parking spaces provided must be at a rate of 1 space to each 20 car parking spaces after the first 19 car parking spaces except if bulky goods sales, (rounded to the nearest whole number). Where an existing use or development is extended or intensified, the additional number of motorcycle parking spaces provided must be calculated on the amount of extension or intensification, provided the existing number of motorcycle parking spaces is not reduced.</p> | <p><b>A1 – Not applicable.</b></p>  |
| <p><b>Clause E6.7.1 - Number of vehicular accesses</b></p> <p><b>A1</b> – The number of vehicle access points provided for each road frontage must be no more than 1 or the existing number of vehicle access points, whichever is the greater.</p>   | <p><b>A1 – Complies.</b> There is one access point provided for the proposed application from Incana Road.</p>  |
| <p><b>Clause E6.7.2 - Design of vehicular accesses</b></p>  | <p><b>A1 – Complies.</b> The vehicle access point complies with the Australian Standard.</p>  |

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| <p><b>A1</b> – Design of vehicle access points must comply with all of the following:</p> <ul style="list-style-type: none"> <li>(a) in the case of non-commercial vehicle access; the location, sight distance, width and gradient of an access must be designed and constructed to comply with section 3 – “Access Facilities to Off-street Parking Areas and Queuing Areas” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;</li> <li>(b) in the case of commercial vehicle access; the location, sight distance, geometry and gradient of an access must be designed and constructed to comply with all access driveway provisions in section 3 “Access Driveways and Circulation Roadways” of AS2890.2 - 2002 Parking facilities Part 2: Off-street commercial vehicle facilities.</li> </ul> |   |
| <p><b>Clause E6.7.3 - Vehicular passing areas along an access</b></p> <p><b>A1</b> – Vehicular passing areas must:</p> <ul style="list-style-type: none"> <li>(a) be provided if any of the following applies to an access: <ul style="list-style-type: none"> <li>(i) it serves more than 5 car parking spaces;</li> <li>(ii) is more than 30 m long;</li> <li>(iii) it meets a road serving more than 6000 vehicles per day</li> </ul> </li> <li>(b) be 6 m long, 5.5 m wide, and taper to the width of the driveway;</li> <li>(c) it meets a road serving more than 6000 vehicles per day;</li> <li>(d) have the first passing area constructed at the kerb;</li> <li>(e) be at intervals of no more than 30 m along the access.</li> </ul>   | <p><b>A1 – Complies.</b> The internal road of the proposed development is 6m wide. This allows for two-way traffic and passing for the full length of the road.</p> |
| <p><b>Clause E6.7.4 - On-site turning</b></p> <p><b>A1</b> – On-site turning must be provided to enable vehicles to exit a site in a forward direction, except where the access complies with any of the following:</p> <ul style="list-style-type: none"> <li>(a) it serves no more than two dwelling units;</li> </ul>   | <p><b>A1 – Complies.</b> All vehicles and leave the site in a forward direction</p>   |
| <p><b>Clause E6.7.5 - Layout of parking areas</b></p> <p><b>A1</b> – The layout of car parking spaces, access aisles, circulation roadways and ramps must be designed and constructed to comply with section 2 “Design of Parking Modules, Circulation Roadways and Ramps” of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking and must</p>  | <p><b>A1 – Complies.</b> The vehicle access and parking layout complies with the Australian Standard.</p>   |

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| <p>have sufficient headroom to comply with clause 5.3 "Headroom" of the same Standard.</p>  |  |
| <p><b>Clause E6.7.6 - Surface treatment of parking areas</b></p> <p><b>A1</b> – Parking spaces and vehicle circulation roadways must be in accordance with all of the following;</p> <p>(a) paved or treated with a durable all-weather pavement where within 75m of a property boundary or a sealed roadway;</p> <p>(b) drained to an approved stormwater system, unless the road from which access is provided to the property is unsealed.</p> | <p><b>A1 – Complies.</b> Because the internal road and parking areas are proposed to be concrete sealed.</p>   |
| <p><b>Clause E6.7.7 - Lighting of parking areas</b></p> <p><b>A1</b> – Parking and vehicle circulation roadways and pedestrian paths serving 5 or more car parking spaces, used outside daylight hours, must be provided with lighting in accordance with clause 3.1 "Basis of Design" and clause 3.6 "Car Parks" in AS/NZS 1158.3.1:2005 Lighting for roads and public spaces Part 3.1: Pedestrian area (Category P) lighting.</p>               | <p><b>A1 – Complies.</b> The proposal meets the relevant standards.</p>  |
| <p><b>Clause E6.7.8 - Landscaping of parking areas</b></p> <p><b>A1</b> – Landscaping of parking and circulation areas must be provided where more than 5 car parking spaces are proposed. This landscaping must be no less than 5 percent of the area of the car park, except in the Central Business Zone where no landscaping is required.</p>   | <p><b>A1 – Complies.</b> A landscaping plan has been provided with the application showing that there is more than 5% of the area incorporating landscaping.</p> |
| <p><b>Clause E6.7.9 – Design of motorcycle parking areas</b></p> <p><b>A1</b> - The design of motorcycle parking areas must comply with all of the following:</p> <p>(a) be located, designed and constructed to comply with section 2.4.7 "Provision for Motorcycles" of AS/NZS 2890.1:2004 Parking Facilities Part 1: Off-street car parking;</p> <p>(b) be located within 30 m of the main entrance to the building.</p>                       | <p><b>A1 – Not applicable.</b></p>   |
| <p><b>Clause E6.7.12 – Siting of car parking</b></p> <p><b>A1</b> - Parking spaces and vehicle turning areas, including garages or covered parking areas in the Inner Residential Zone, Urban Mixed Use Zone, Village Zone, Local Business Zone and General Business Zone must be located behind the building line of buildings located or proposed on a site except</p>  | <p><b>A1 – Not applicable.</b> The proposal is in the low density zone.</p>  |

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| <p>if a parking area is already provided in front of the building line of a shopping centre.</p>  |   |
| <p><b>Clause E6.7.13 – Facilities for commercial vehicles</b></p> <p><b>A1</b> - Commercial vehicle facilities for loading, unloading or manoeuvring must be provided on-site in accordance with Australian Standard for Off-street Parking, Part 2 : Commercial. Vehicle Facilities AS 2890.2:2002, unless:</p> <p>(a) the delivery of all inward bound goods is by a single person from a vehicle parked in a dedicated loading zone within 50 m of the site;</p> <p>(b) the use is not primarily dependent on outward delivery of goods from the site.</p> | <p><b>A1 – Complies.</b> The internal road of the proposed development is 6m wide. This allows for 8.8m service vehicles and emergency vehicles to enter and passing for the full length of the road. The proposed development will require a private waste service to be provided and this is included as a condition of approval.</p> |
| <p><b>Clause E6.7.14 - Access to a road</b></p> <p><b>A1</b> – Access to a road must be in accordance with the requirements of the road authority.</p>  | <p><b>A1 – Complies.</b> The access is in accordance with Council's requirements.</p>   |
| <p><b>E7.0 Stormwater Management Code</b></p>   |   |
| <p><b>Clause E7.7.1 - Stormwater drainage and disposal</b></p> <p><b>A1</b> – Stormwater from new impervious surfaces must be disposed of by gravity to public stormwater infrastructure.</p>   | <p><b>A1 – Complies.</b> The proposal will be connected to the available reticulated public stormwater by gravity.</p>  |
| <p><b>A2</b> – A stormwater system for a new development must incorporate water sensitive urban design principles R1 for the treatment and disposal of stormwater if any of the following apply:</p> <p>(a) the size of new impervious area is more than 600 m<sup>2</sup>;</p> <p>(b) new car parking is provided for more than 6 cars;</p> <p>(c) a subdivision is for more than 5 lots.</p>  | <p><b>A2 – Complies.</b> The proposal provides a compliant water sensitive urban design detention, treatment and disposal of stormwater.</p>  |
| <p><b>A3</b> – A minor stormwater drainage system must be designed to comply with all of the following:</p> <p>(a) be able to accommodate a storm with an ARI of 20 years in the case of non-industrial zoned land and an ARI of 50 years in the case of industrial zoned land, when the land serviced by the system is fully developed;</p> <p>(b) stormwater runoff will be no greater than pre-existing runoff or any increase can be accommodated within existing or upgraded public stormwater infrastructure.</p>                                       | <p><b>A3 – Complies.</b> The stormwater design meets the ARI 20 years and the increase can be accommodated in the existing system.</p>  |

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| <p><b>A4</b> – A major stormwater drainage system must be designed to accommodate a storm with an ARI of 100 years.</p>  | <p><b>A4 – Complies.</b> The stormwater design has been designed to accommodate a storm with an ARI of 100 years.</p>   |
| <p><b>E10.0 Biodiversity Code</b></p>  |   |
| <p><b>Clause E10.7.1 - Buildings and works</b><br/> <b>A1</b> – Clearance and conversion or disturbance must be within a Building Area on a plan of subdivision approved under this planning scheme.</p>   | <p><b>A1 – Does not comply.</b> The proposal relies upon retrospective removal of a small copse of native vegetation and disturbance of native trees located within a Biodiversity Protection Area.<br/><br/>                 The proposal is unable to meet A1 as there is no building area on the title. Therefore, the proposal must be assessed against the Performance Criteria.</p> |
| <p><b>E15.0 Inundation Prone Areas Code</b></p>  |   |
| <p><b>Clause E15.6 - Use standards</b><br/> <b>A1</b> – Change of use of a non-habitable building to a habitable building or a use involving habitable rooms must comply with all of the following:<br/>                 (a) floor level of habitable rooms is no less than the AHD level for the Coastal Inundation Low Hazard Area in Table E15.1;<br/>                 (b) floor level of habitable rooms is no less than the AHD level for the 1% AEP plus 300mm if in an area subject to riverine flooding.</p> | <p><b>A1 – Not applicable.</b> While located partially in a Riverine Inundation Area (overland flow), the proposal does not involve a change of use of a non-habitable building to a habitable building or a change of use from a non-habitable building to a use involving habitable rooms.</p>  |
| <p><b>Clause E15.7.1 - Coastal Inundation High Hazard Areas</b><br/> <b>A1</b> – For a habitable building, including extensions to existing habitable buildings, there is no Acceptable Solution.</p>  | <p><b>A1 – Not applicable.</b> The proposal does not include habitable buildings within a Coastal Inundation High Hazard Area.</p>  |
| <p><b>A2</b> – For a non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, there is no Acceptable Solution.</p>   | <p><b>A2 – Not applicable.</b> There are no non-habitable buildings or Class 10b<sup>1</sup> buildings proposed within a Coastal Inundation High Hazard Area.</p>   |
| <p><b>Clause E15.7.2 - Coastal Inundation Medium Hazard Areas</b><br/> <b>A1</b> – For a new habitable building there is no Acceptable Solution.</p>   | <p><b>A1 – Not applicable.</b> The proposal does not include a new habitable building within a Coastal Inundation Medium Hazard Area.</p>   |
| <p><b>A2</b> – Except for new rooms associated with habitable buildings other than dwellings, for which there is no acceptable solution, an extension to an existing habitable building must comply with one of the following:<br/>                 (a) new habitable rooms must comply with both of the following:</p>  | <p><b>A2 – Not applicable.</b> The proposal does not involve new rooms associated with a habitable building or an extension to an existing habitable building within a Coastal Inundation Medium Hazard Area.</p>   |

<sup>1</sup> Class 10b is a structure being a fence, mast, antenna, retaining wall, swimming pool, or the like

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| <ul style="list-style-type: none"> <li>(i) floor level no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1,</li> <li>(ii) floor area of the extension no more than 40 m<sup>2</sup> from the date of commencement of this planning scheme;</li> </ul> <p>(b) new habitable rooms must be above ground floor.</p>   |   |
| <p><b>A3</b> – A non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, must have a floor area no more than 40 m<sup>2</sup>.</p>  | <p><b>A3 – Not applicable.</b> The proposal does not include a non-habitable building, outbuilding or class 10b building a Coastal Inundation Medium Hazard Area.</p>   |
| <p><b>Clause E15.7.3 - Coastal Inundation Low Hazard Areas</b><br/> <b>A1</b> – A new habitable building must comply with the following:</p> <ul style="list-style-type: none"> <li>(a) floor level no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1;</li> </ul>  | <p><b>A1 – Not applicable.</b> The proposal does not include a new habitable building within a Coastal Inundation Low Hazard Area.</p>  |
| <p><b>A2</b> – An extension to a habitable building must comply with either of the following:</p> <ul style="list-style-type: none"> <li>(a) floor level of habitable rooms is no lower than the Minimum Level for the Coastal Inundation Low Hazard Area in Table E15.1;</li> <li>(b) floor area is no more than 60 m<sup>2</sup>.</li> </ul>   | <p><b>A2 – Not applicable.</b> The proposal does not include an extension to a habitable building within the Coastal Inundation Low Hazard Area.</p>  |
| <p><b>A3</b> – A non-habitable building, an outbuilding or a Class 10b building under the Building Code of Australia, must have a floor area no more than 60 m<sup>2</sup>.</p>  | <p><b>A3 – Not applicable.</b> The proposal does not include a non-habitable building, outbuilding or class 10b building within the Coastal Inundation Low Hazard Area.<br/><br/>the Performance Criteria.</p>            |
| <p><b>Clause E15.7.4 - Riverine inundation hazard areas</b><br/> <b>A1</b> - A new habitable building must have a floor level no lower than the 1% AEP (100 yr ARI) storm event plus 300 mm.</p>   | <p><b>A1 – Complies.</b> The existing open channel across the site are proposed to be piped with a drainage system designed for the 1 in 20 year flow. All buildings are at least 300mm above the 100 year flow event</p> |
| <p><b>A2</b> – An extension to an existing habitable building must comply with one of the following:</p> <ul style="list-style-type: none"> <li>(a) floor level of habitable rooms is no lower than the 1% AEP (100 yr ARI) storm event plus 300 mm;</li> <li>(b) floor area of the extension no more than 60 m<sup>2</sup> as at the date of commencement of this planning scheme.</li> </ul> | <p><b>A2 – Not applicable.</b> Because the proposal is not an extension to an existing habitable building.</p>  |
| <p><b>A3</b> – The total floor area of all non-habitable buildings, outbuildings and Class 10b buildings</p>   | <p><b>A3 – Does not apply.</b> There are not non-habitable buildings, outbuildings</p>  |

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| under the Building Code of Australia, on a site must be no more than 60 m <sup>2</sup> .   | or class 10b structures as part of the proposal.  |
| <p><b>Clause E15.7.5 - Riverine, coastal investigation area, low, medium high inundation hazard area</b></p> <p><b>A1</b> – For landfill, or solid walls greater than 5 m in length and 0.5 m in height, there is no acceptable solution (requires assessment against performance criteria).</p> | <p><b>A1 – Not applicable.</b> There is no landfill or solid walls greater than 5m length and 0.5m high in the area affected by the code.</p>   |
| <p><b>Clause E15.7.5 - Riverine, coastal investigation area, low, medium high inundation hazard area</b></p> <p><b>A1</b> – For landfill, or solid walls greater than 5 m in length and 0.5 m in height, there is no acceptable solution.</p>  | <p><b>A1 – Not applicable.</b> While the proposal involves placement of fill within the footprint private open space areas than 5m length and 0.5m high in the area affected by overland flows, as the private road networks within the site are designed to capture and divert overland flows away from solid walls.</p> |
| <p><b>A2</b> – No acceptable solution.</p>   | <p><b>A2 – Not Applicable.</b> The proposal does not involve or rely upon any mitigation measures.</p>  |
| <p><b>Clause E15.7.6 - Development Dependent on a Coastal Location</b></p> <p><b>A1</b> - An extension to an existing boat ramp, car park, jetty, marina, marine farming shore facility or slipway must be no more than 20% of the size of the facility existing at the effective date.</p>      | <p><b>A1 – Not applicable.</b> The proposal does not include development dependent on a coastal location.</p>   |
| <p><b>A2</b> - No acceptable solution.</p>   | <p><b>A2 – Not Applicable.</b> The proposal does not involve any dredging or reclamation.</p>   |
| <p><b>A3</b> - No Acceptable Solution for coastal protection works initiated by the private sector.</p>  | <p><b>A3 – Not Applicable.</b> The proposal does not involve any coastal protection works.</p>  |

Note: Codes not listed in this Checklist have been assessed as not being relevant to the assessment of this application.



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**PLANNING SCHEME OVERLAYS:**  
 Bushfire Prone Area

Survey by Leary, Cox & Cropp  
 Natural Values Report by ECOTAS  
 Preliminary BAL report by GES PTY LTD  
 Civil Drawings by GE Consulting Engineers  
 C.T. 1888765/1  
 PID 9016395

**Kingborough Council**  
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 Date placed on Public Exhibition: 17/1/2026

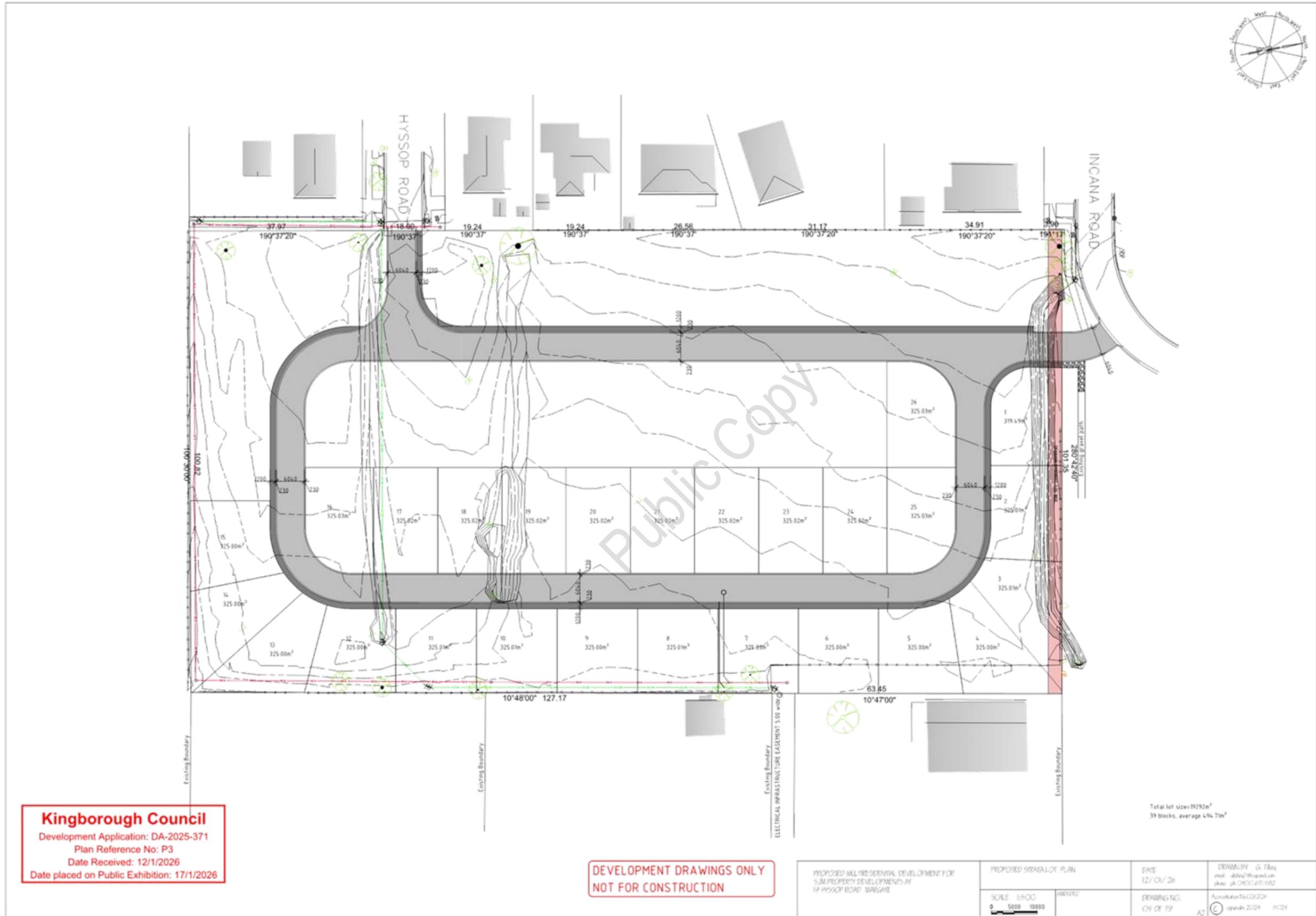
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| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SUB PROPERTY DEVELOPMENTS AT<br>16 HYSOOP ROAD, MARGATE |        | OVERLAY                 | DATE<br>12/01/26  | DRAWN BY<br>G. Tiley<br>mod: jk@geconsult.com<br>skw: jk@geconsult.com |
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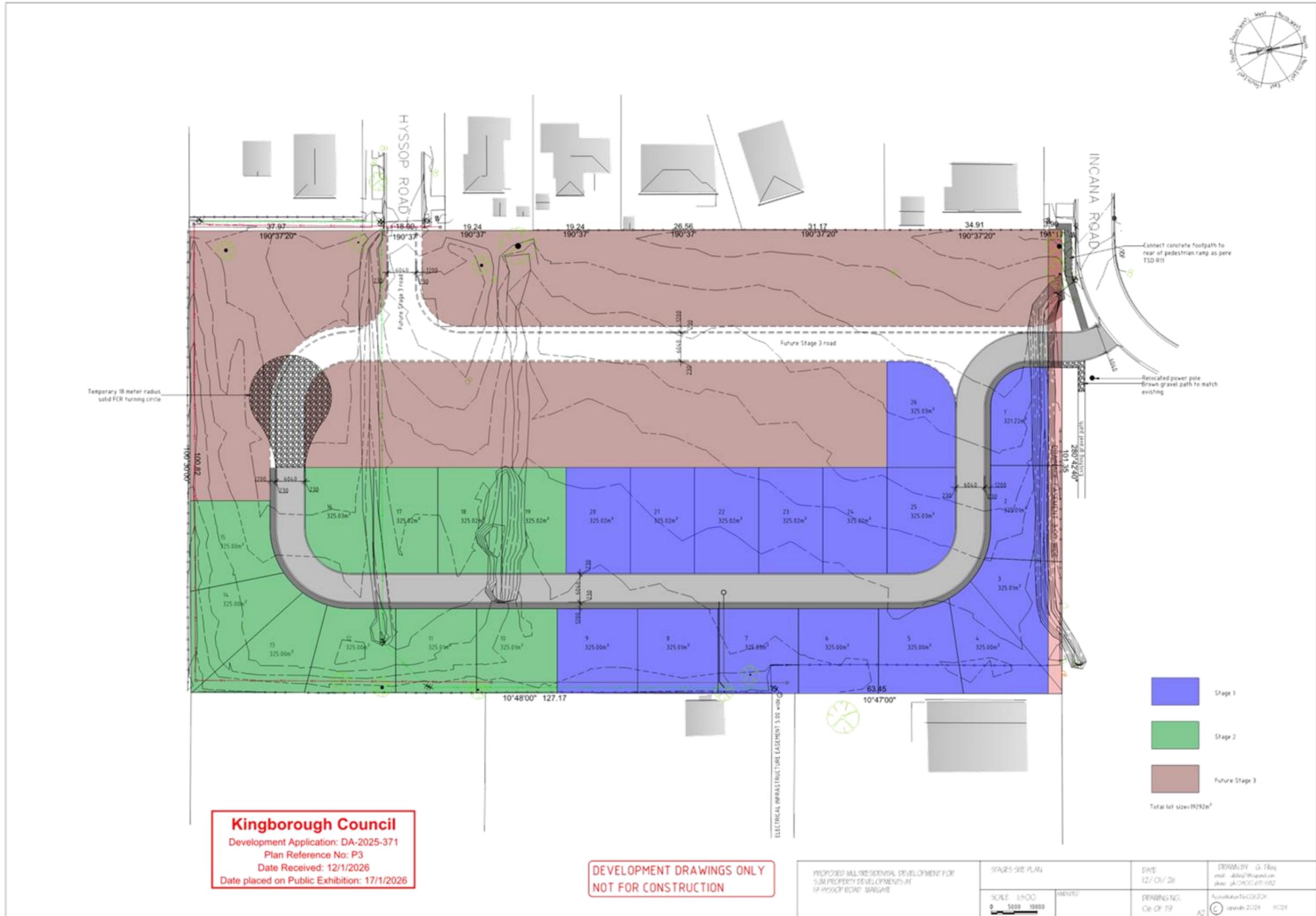








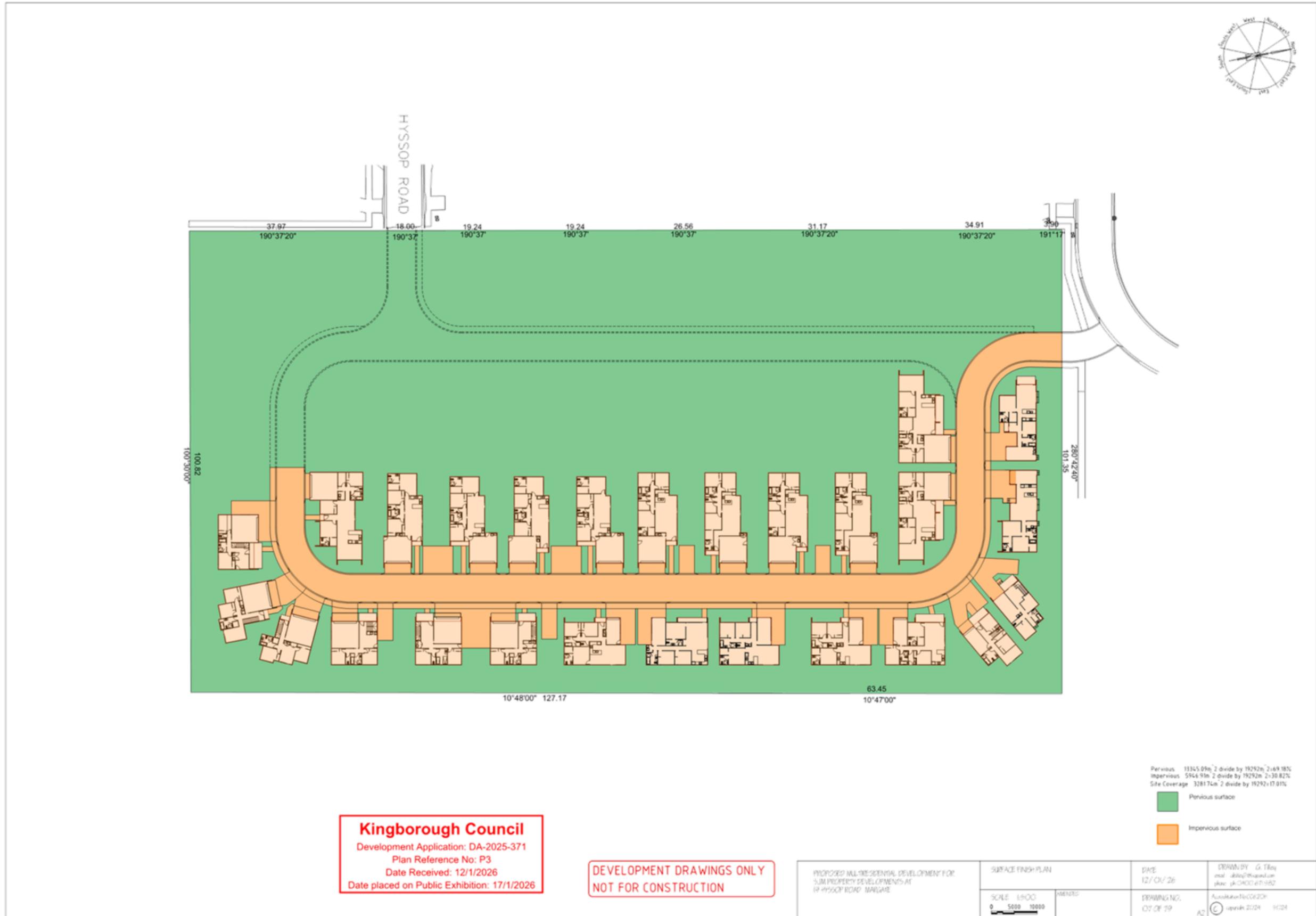
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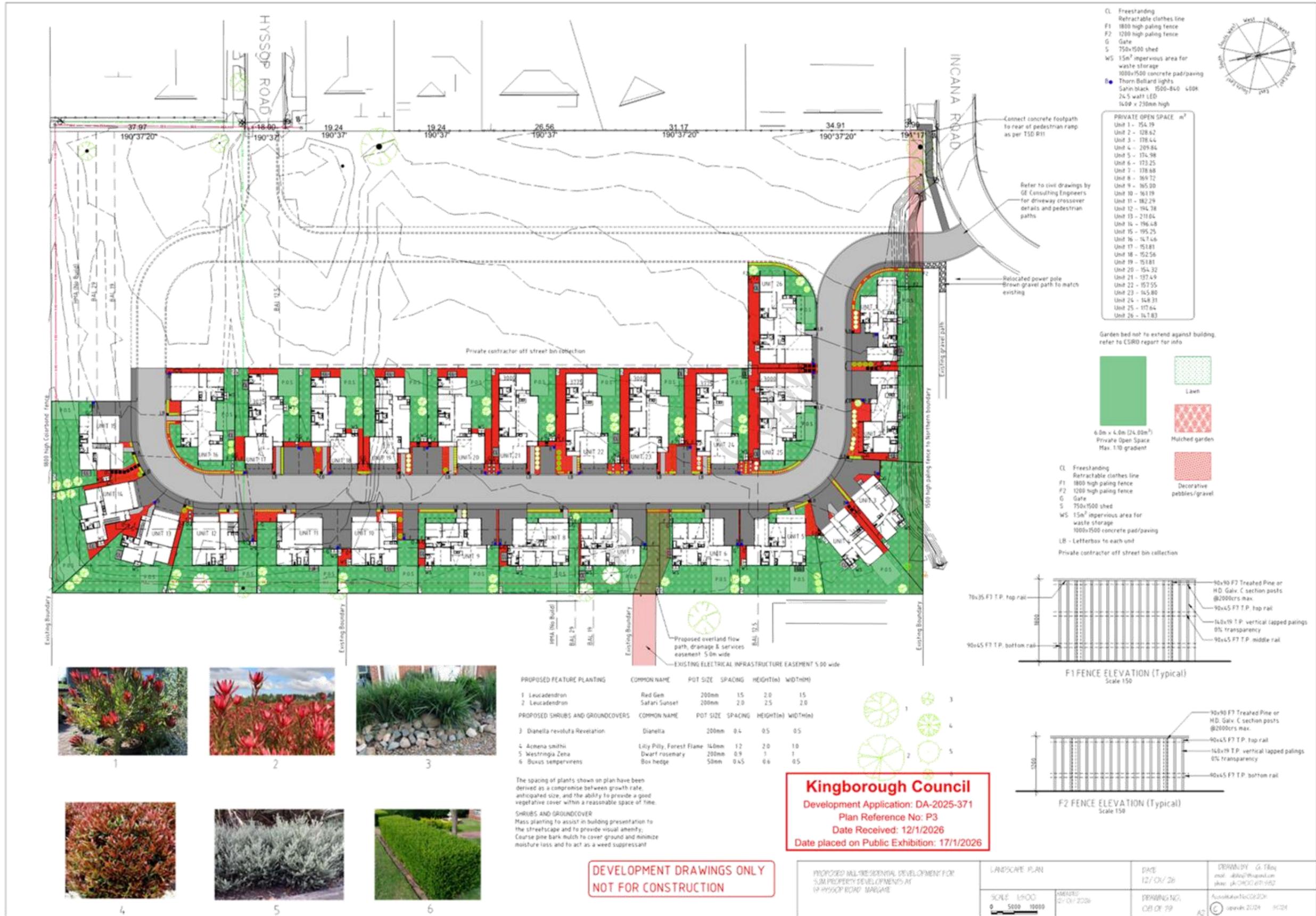


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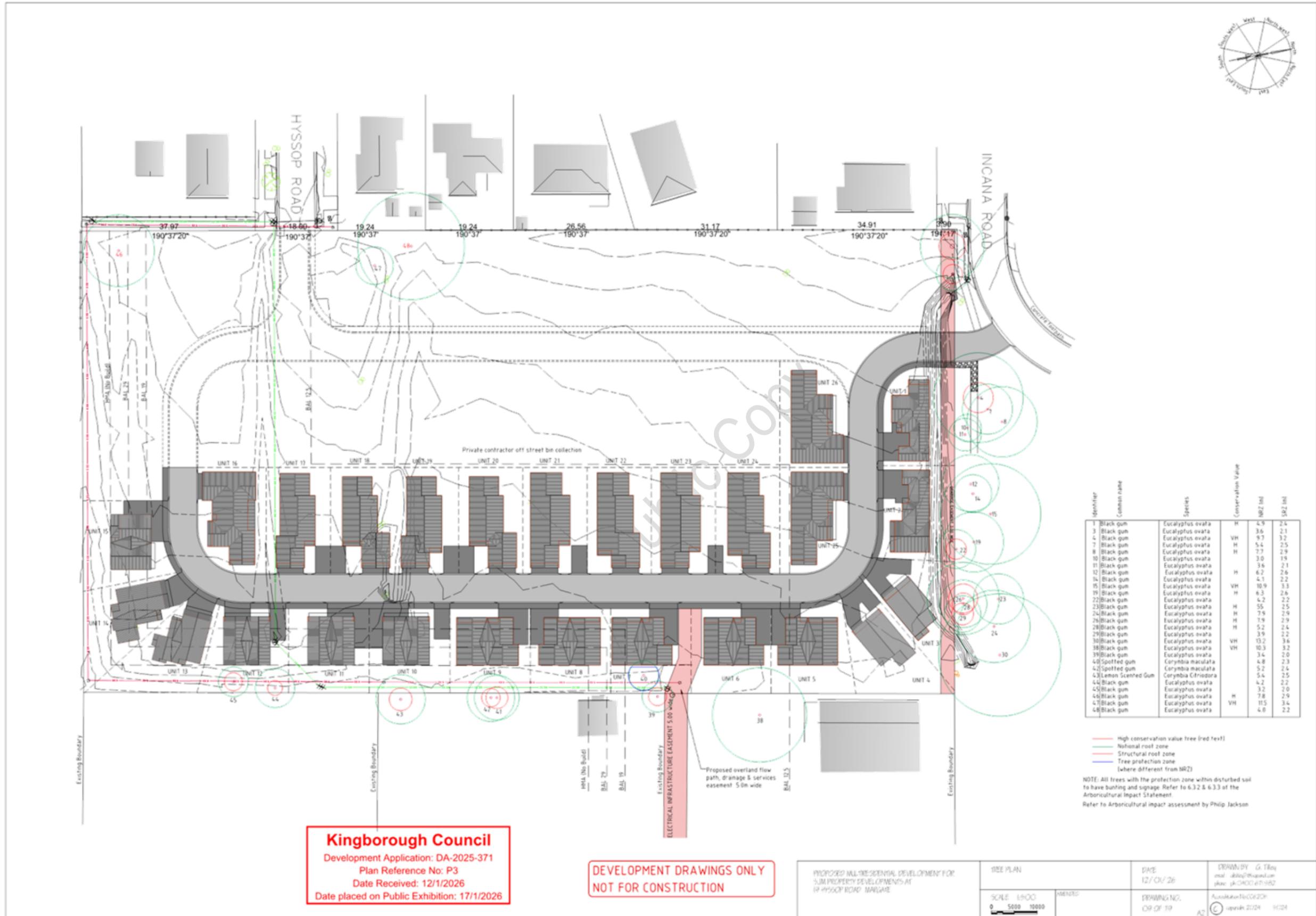
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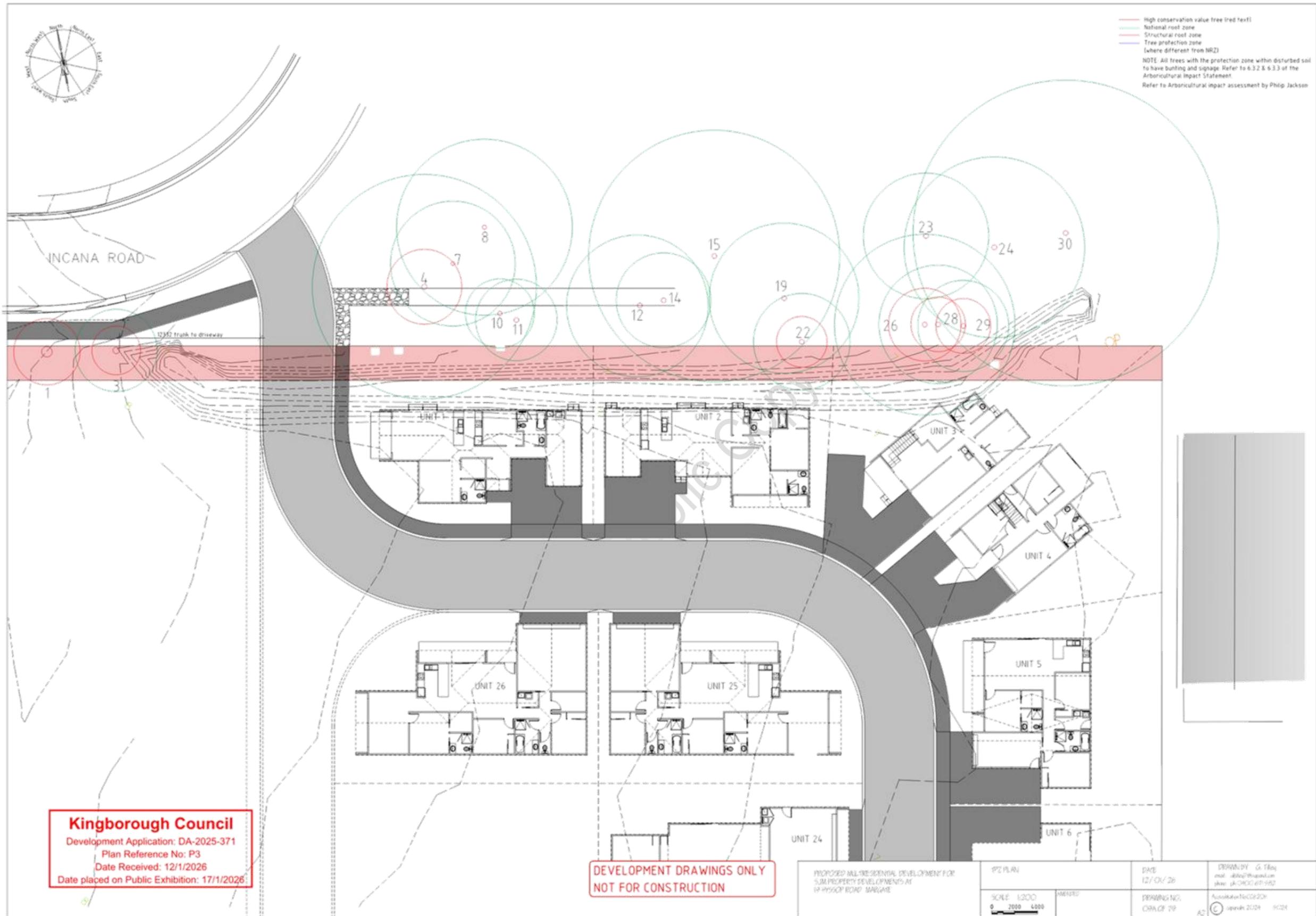
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|  | SCALE<br>1:500<br>0 5000 10000 | METERS | DRAWING NO.<br>C6 OF 19 | APPROVED BY<br>G. Tiley<br>mob: 484978494<br>skw: jk@kdc.com.au |





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SIGHT LINE DISTANCE  
as per AS2890.1 Figure 3.2  
Scale 1:1000

| Frontage road speed (note 4) km/h | Distance (Y) along frontage road in           |             |                                   |
|-----------------------------------|---|-------------|-----------------------------------|
|                                   | Access driveways other than domestic (Note 5) |             | Domestic Property Access (note 6) |
|                                   | Desirable 5s gap                              | Minimum 5SD |                                   |
| 40                                | 55  | 35          | 30                                |
| 50                                | 69  | 45          | 40                                |
| 60                                | 83  | 65          | 55                                |
| 70                                | 97  | 85          | 70                                |
| 80                                | 111   | 105         | 95                                |
| 90                                | 125   | 130         |                                   |
| 100                               | 139   | 160         | Use Values from 2nd & 3rd columns |
| 110                               | 153   | 190         |                                   |

- NOTES:
- Centre-line or centre of road (undivided road), or right hand edge of right hand through lane (divided road).
  - A check to the left is not required at a divided road where the median is wide enough to shelter a vehicle leaving the driveway.
  - Parking on this side of the frontage road may need to be restricted on either side of the driveway so that the sight distance required by the above table to an approaching vehicle is not obstructed.
  - This is the posted or general speed limit unless the 85th percentile speed is more than 5km/h above the limit in which case the tabulated speed nearest the 85th percentile shall be adopted.
  - The values in the table apply only to left turn and right turn manoeuvres into two-way roads up to four lanes wide and one-way streets regardless of width, either for a 5s gap, desirable or lower frontage road speeds, or minimum stopping sight distance based on 2s reaction time. Crossing manoeuvres (e.g. from an access opposite the stream at a T-junction) over four lanes or more, and turning manoeuvres into a six lane two-way road would require longer gaps unless there was a median wide enough to store a vehicle and allow a two stage manoeuvre.
  - These distances are based on stopping sight distances with reaction time of 1.5s for traffic approaching along the frontage road and are applicable to a frontage road speed of up to 80km/h only. Wherever practicable sight distance provided at domestic property accesses should meet the values given in the second or third columns of table.
  - When checking sight sight distance the driver's eye height and the height of the object (approaching vehicle) are to be taken as 1.5m above the road surface.

FIGURE 3.2 SIGHT DISTANCE REQUIREMENTS AT ACCESS DRIVEWAY

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| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SUB PROPERTY DEVELOPMENTS AT<br>16 HYSSOP ROAD, MARGARET | VEHICLE SIGHT LINE DISTANCES |        | DATE<br>12/01/26        | DRAWN BY<br>G. Tiley<br>mat. gtiley@kingborough.nsw.gov.au<br>pkc4000/4711652 |
|   | SCALE<br>0 5000 10000        | ASPECT | DRAWING NO.<br>11 OF 39 | APPROVED BY<br>AC<br>17/01/2026 10:24   |



SIGHT LINE DISTANCE  
as per AS2890.1 Figure 3.2  
Scale 1:1000

| Frontage road speed (note 4) km/h | Distance (Y) along frontage road in           |             |                                   |
|-----------------------------------|---|-------------|-----------------------------------|
|                                   | Access driveways other than domestic (Note 5) |             | Domestic Property Access (note 4) |
|                                   | Desirable 5s gap                              | Minimum 5SD |                                   |
| 40                                | 55  | 35          | 30                                |
| 50                                | 69  | 45          | 40                                |
| 60                                | 83  | 65          | 55                                |
| 70                                | 97  | 85          | 70                                |
| 80                                | 111   | 105         | 95                                |
| 90                                | 125   | 130         |                                   |
| 100                               | 139   | 160         | Use Values from 2nd & 3rd columns |
| 110                               | 153   | 190         |                                   |

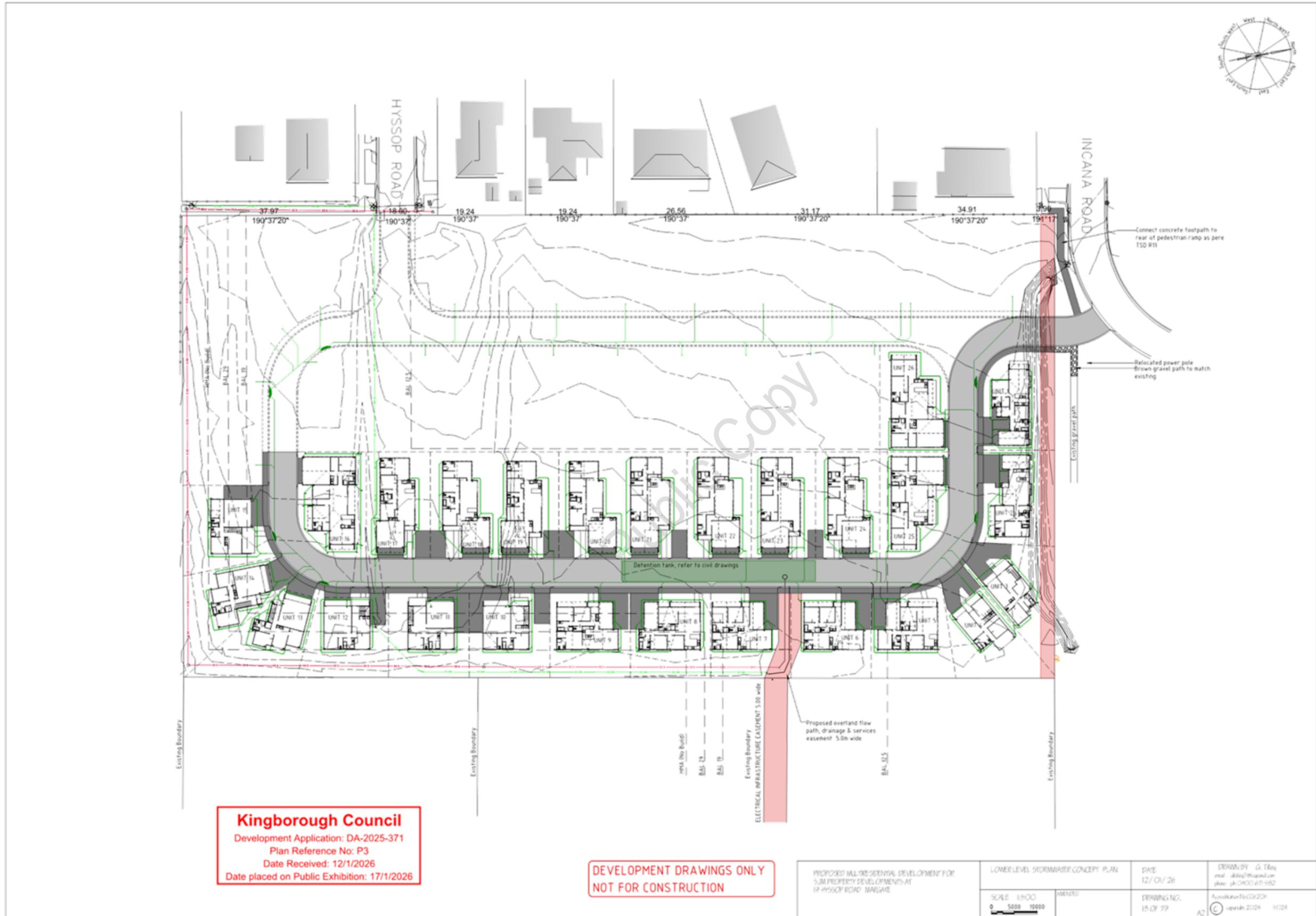
- NOTES:
- Centre-line or centre of road (undivided road), or right hand edge of right hand through lane (divided road).
  - A check to the left is not required at a divided road where the median is wide enough to shelter a vehicle leaving the driveway.
  - Parking on this side of the frontage road may need to be restricted on either side of the driveway so that the sight distance required by the above table to an approaching vehicle is not obstructed.
  - This is the posted or general speed limit unless the 85th percentile speed is more than 5km/h above the limit in which case the tabulated speed nearest the 85th percentile shall be adopted.
  - The values in the table apply only to left turn and right turn manoeuvres into two-way roads up to four lanes wide and one-way street regardless of width, either for a 5s gap, desirable or lower frontage road speeds, or minimum stopping sight distance based on 2s reaction time. Crossing manoeuvres (e.g. from an access opposite the stream at a T-junction) over four lanes or more, and turning manoeuvres into a six lane two-way road would require longer gaps unless there was a median wide enough to store a vehicle and allow a two stage manoeuvre.
  - These distances are based on stopping sight distances with reaction time of 1.5s for traffic approaching along the frontage road and are applicable to a frontage road speed of up to 80km/h only. Wherever practicable sight distance provided at domestic property accesses should meet the values given in the second or third columns of table.
  - When checking sight sight distance the driver's eye height and the height of the object (approaching vehicle) are to be taken as 1.5m above the road surface.

FIGURE 3.2 SIGHT DISTANCE REQUIREMENTS AT ACCESS DRIVEWAY

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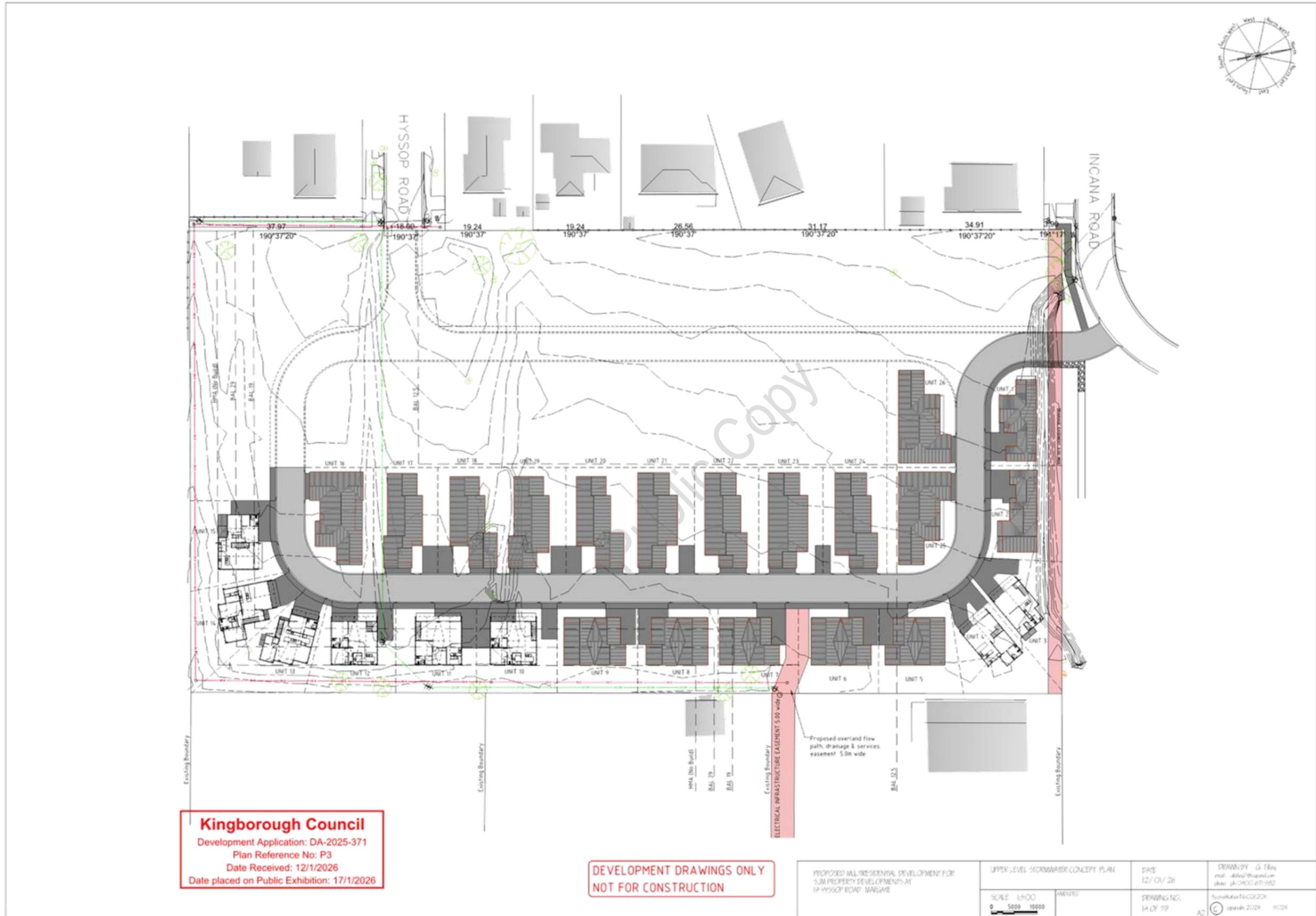
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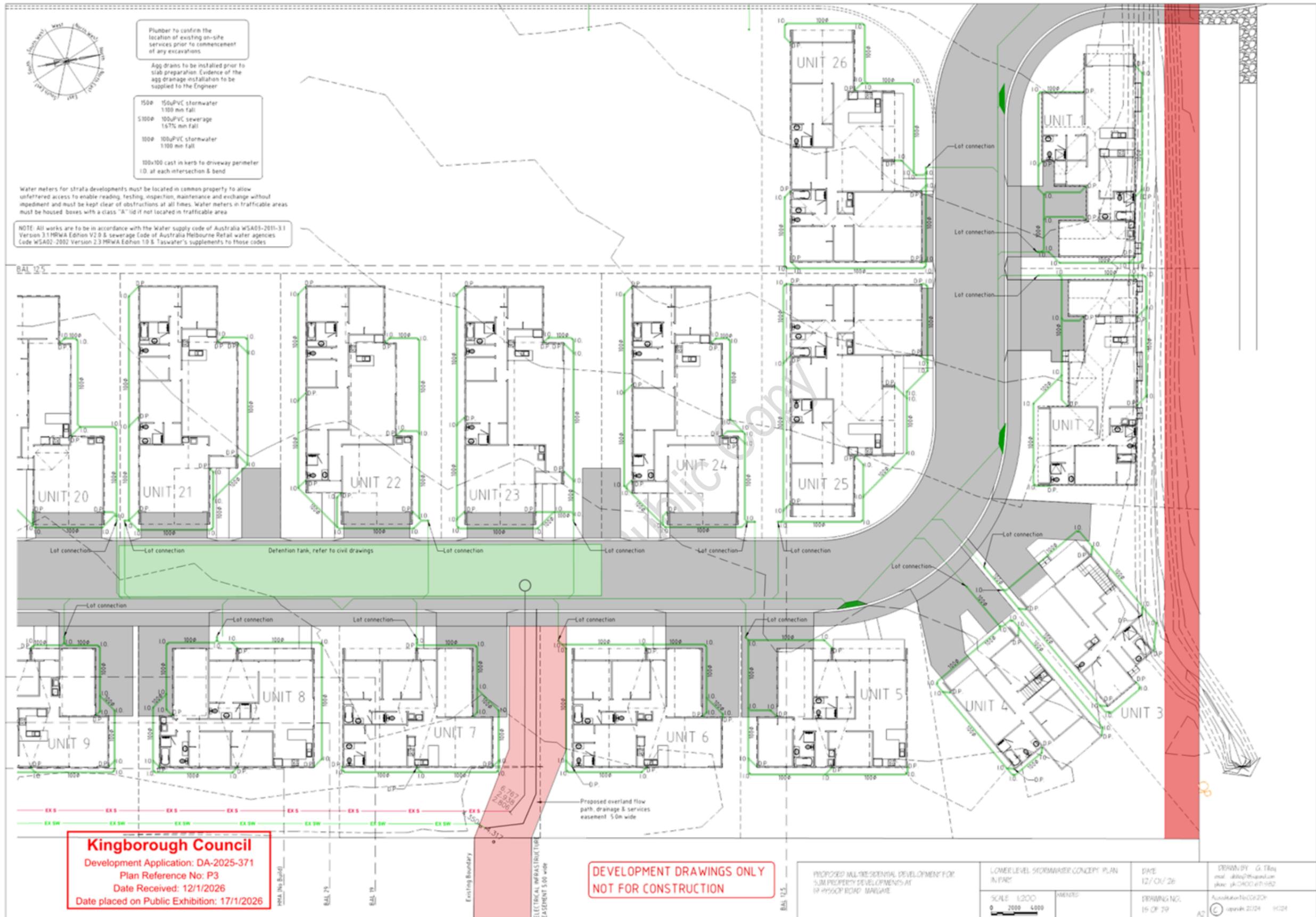


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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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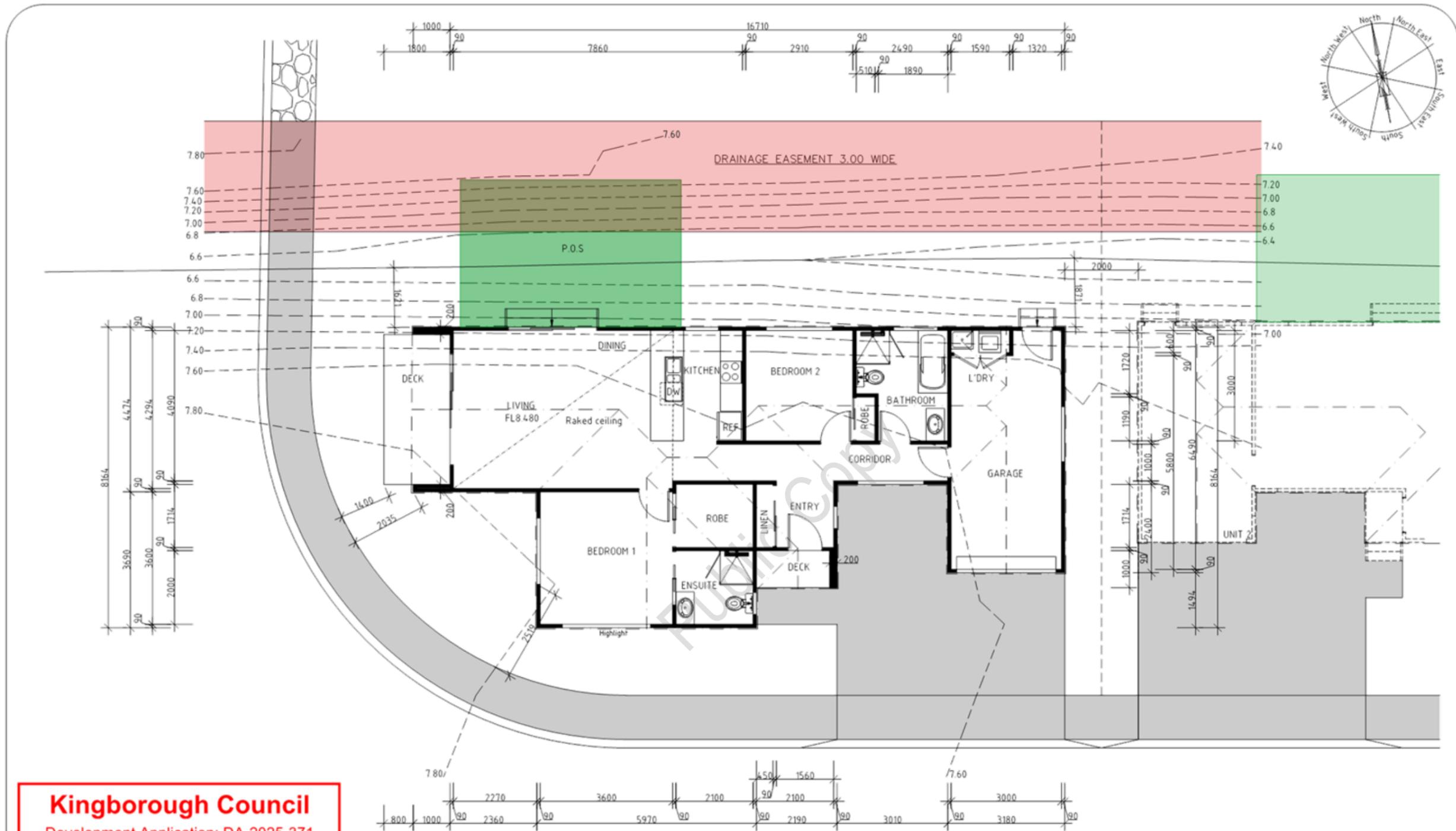
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|---|---------|-------------------------------------|-------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR 26 UNIT PROPERTY DEVELOPMENTS AT 16 HYSSOP ROAD, MARGAREE |         | LOWER LEVEL STORMWATER CONCEPT PLAN | DATE<br>12/01/26  | DRAWN BY<br>G. Tiley<br>mail: gtiley@kingborough.nsw.gov.au<br>phone: 08 9400 4171 |
| SCALE<br>1:500<br>0 5000 10000  | AMENDS: | DRAWING NO.<br>15 OF 79             | APPROVED BY<br>AC |  |





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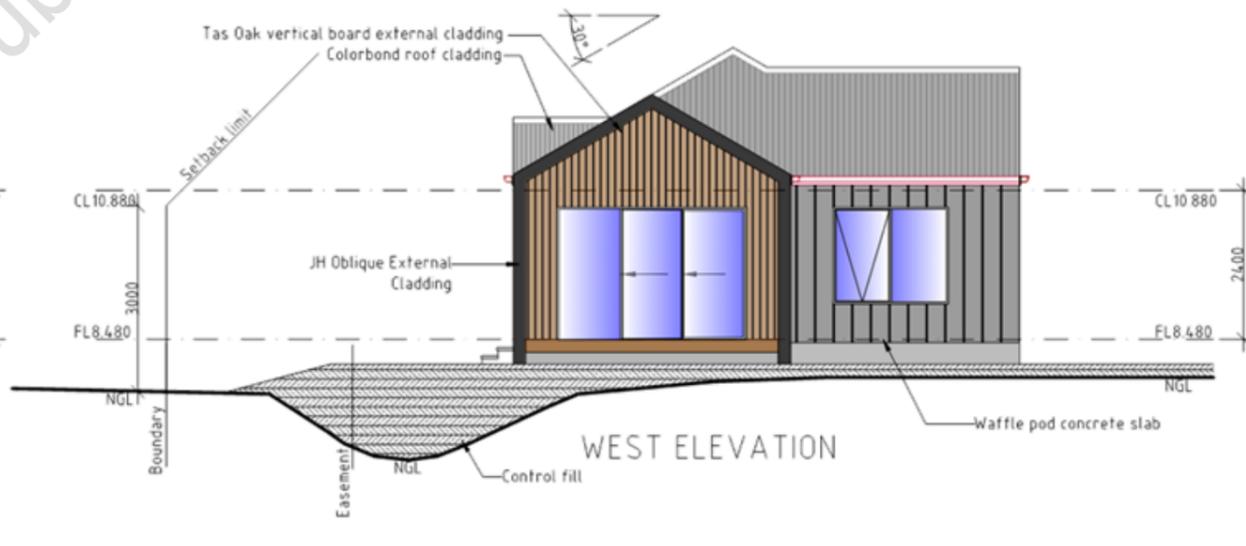
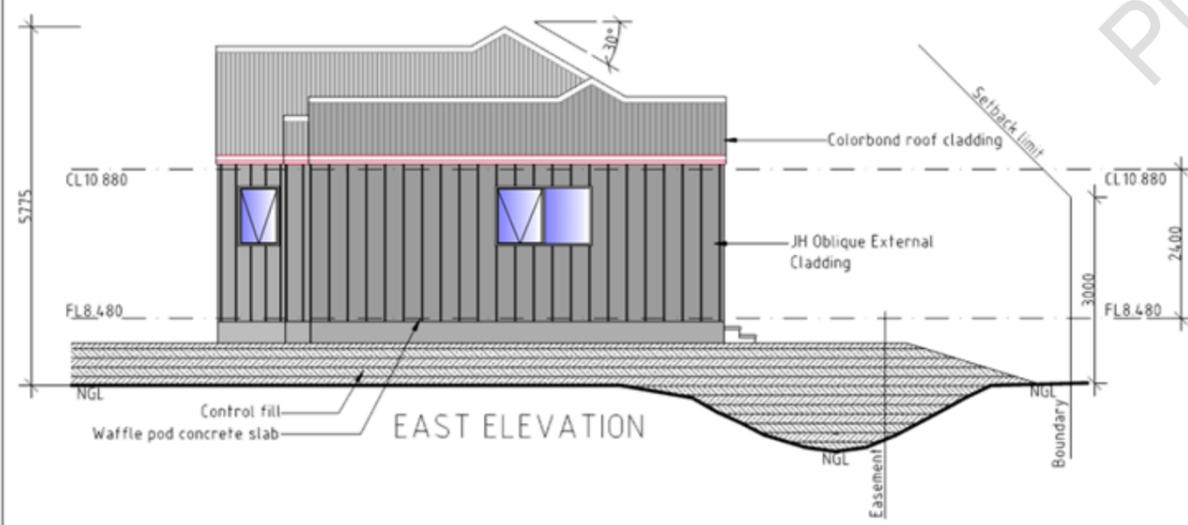
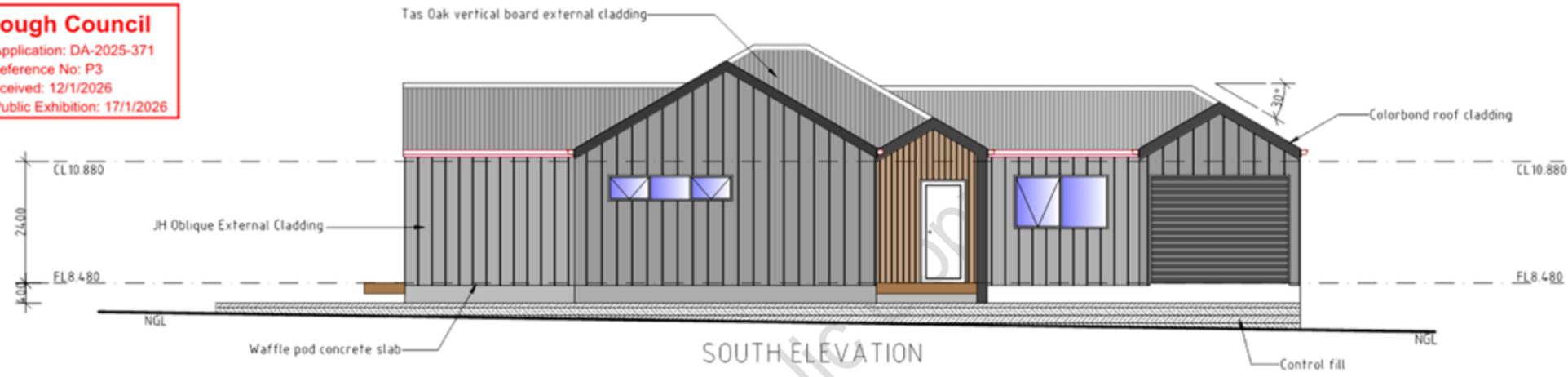
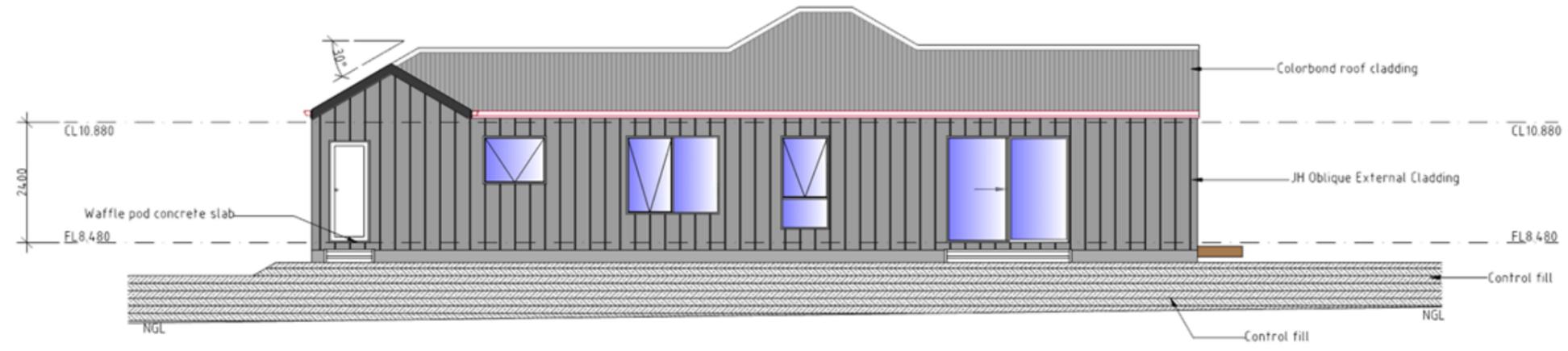
AREAS Unit 1  
 Unit 1 106.71m<sup>2</sup>  
 Decks 9.32m<sup>2</sup>

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**UNIT 1**

|  |                            |                  |   |
|--|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 1 PLAN                | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|  | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>17 OF 19   |

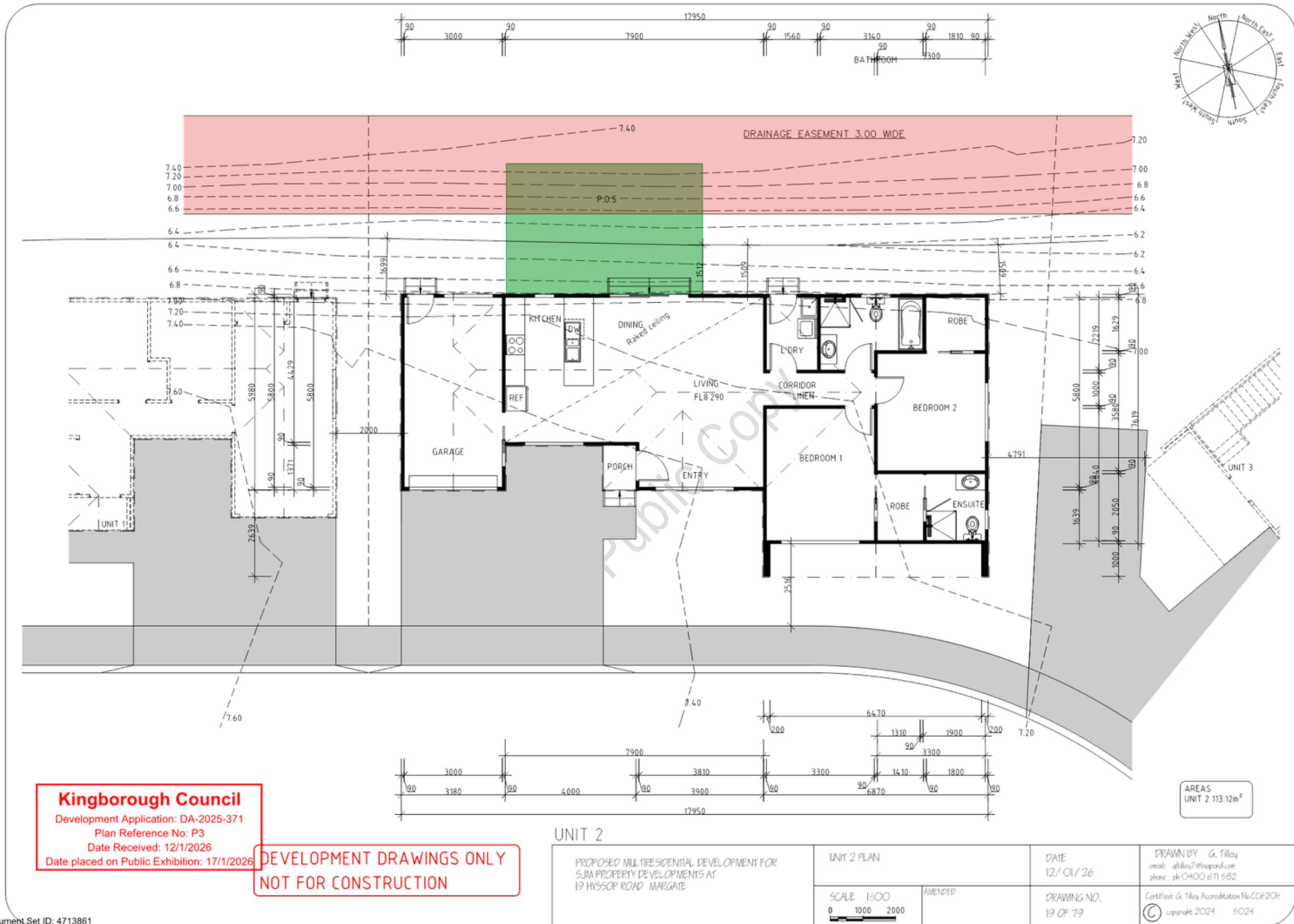
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026



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**UNIT 1**

|  |                            |                  |   |
|--|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 1 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|  | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>18 OF 19   |





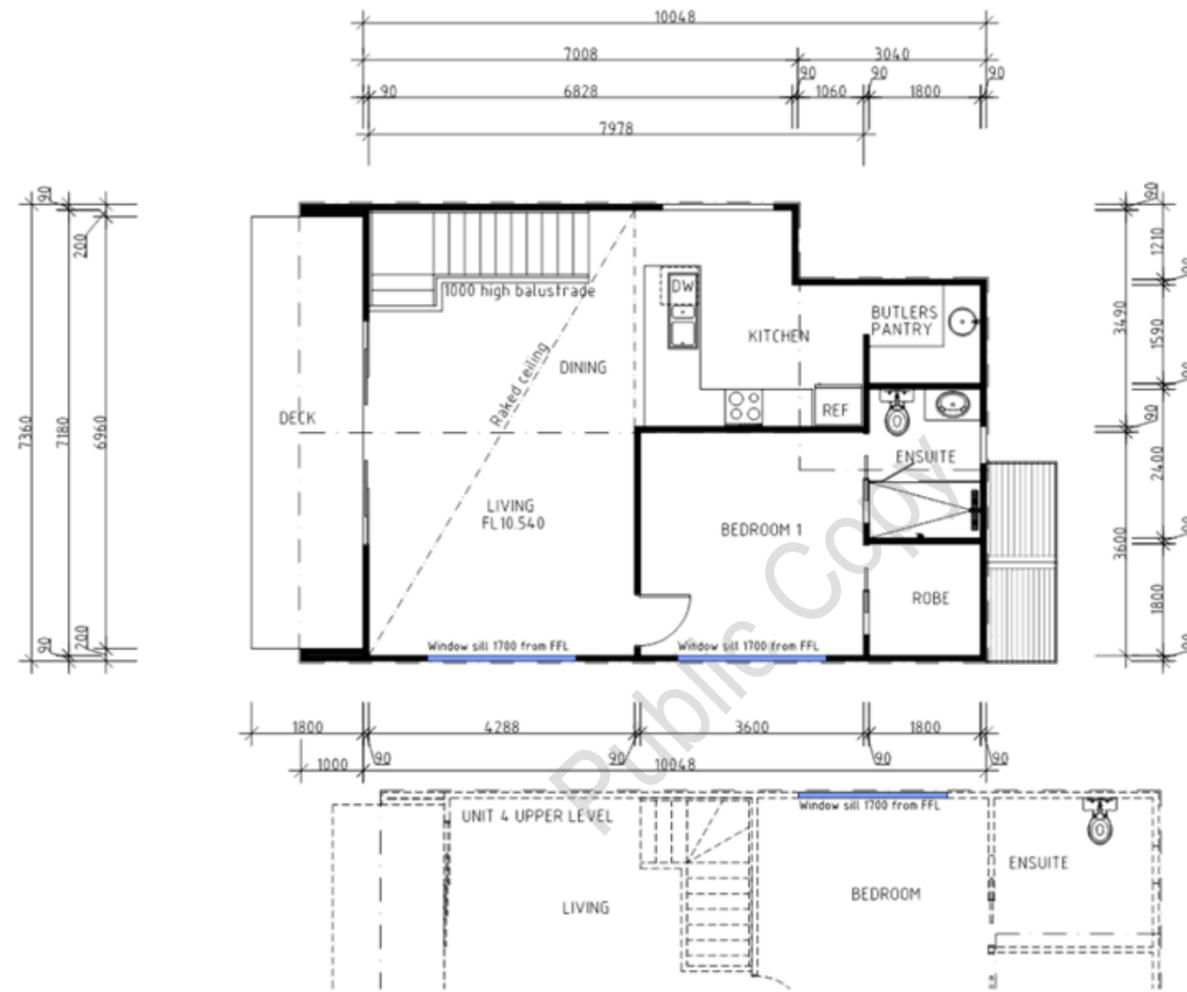
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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**UNIT 2**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                               |         |                         |   |
|-------------------------------|---------|-------------------------|---|
| UNIT 2 ELEVATIONS             |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE<br>1:100<br>0 1000 2000 | REVISED | DRAWING NO.<br>20 OF 79 | Certified G. Tilley Accreditation No:CC620H<br>© copyright 2024 6:024         |





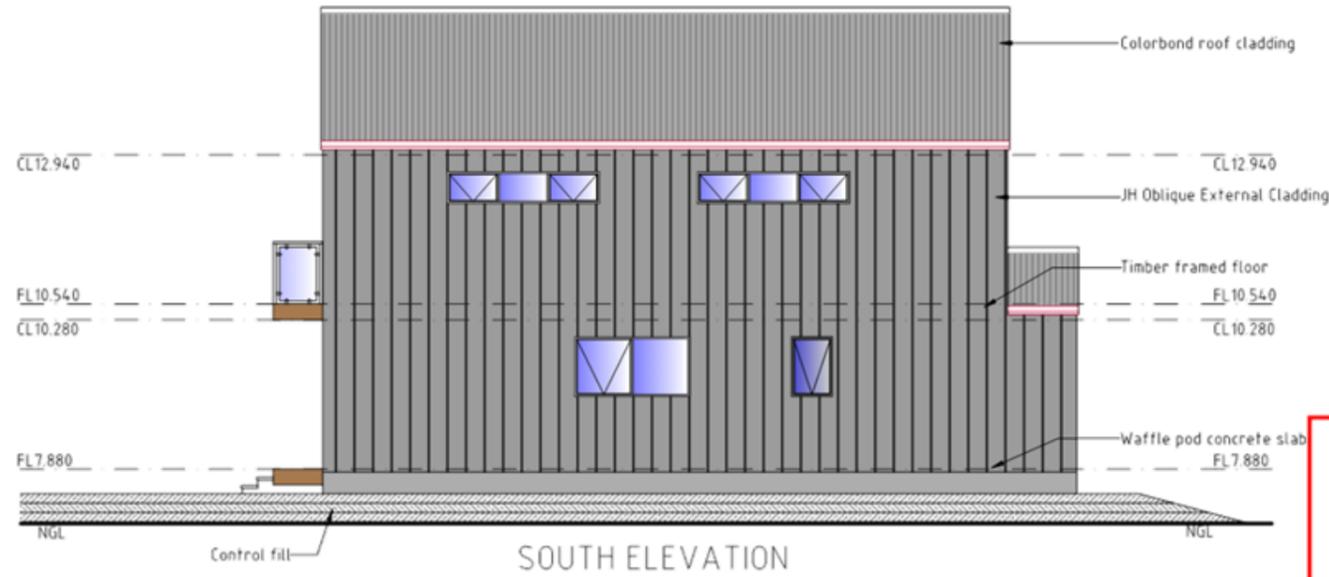
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|                                 |
|---------------------------------|
| AREAS Unit 3                    |
| Lower Level 73.80m <sup>2</sup> |
| Upper Level 70.27m <sup>2</sup> |
| Total 144.07m <sup>2</sup>      |
| Decks 19.36m <sup>2</sup>       |

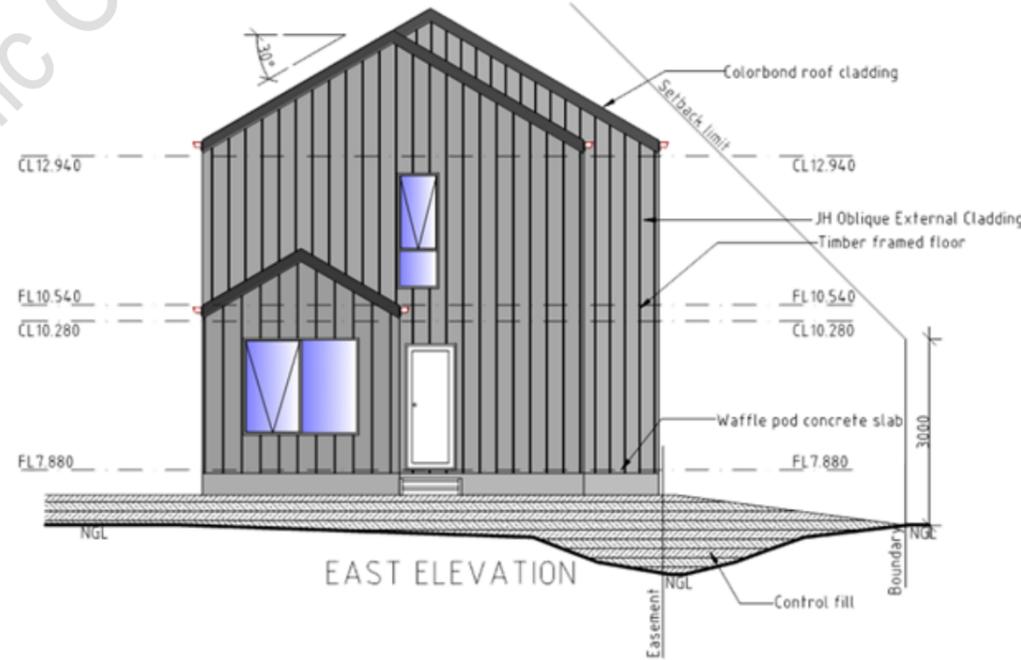
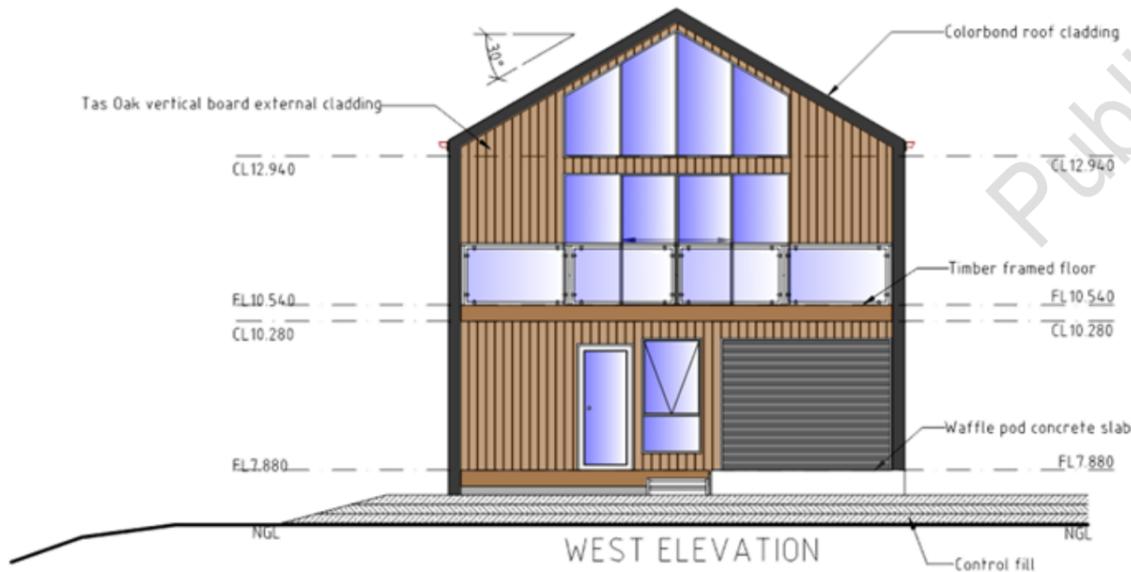
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UNIT 3 UPPER LEVEL

|   |                         |                  |   |
|---|-------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 3 UPPER LEVEL PLAN | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>         | REVISED          | DRAWING NO.<br>22 OF 79   |



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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

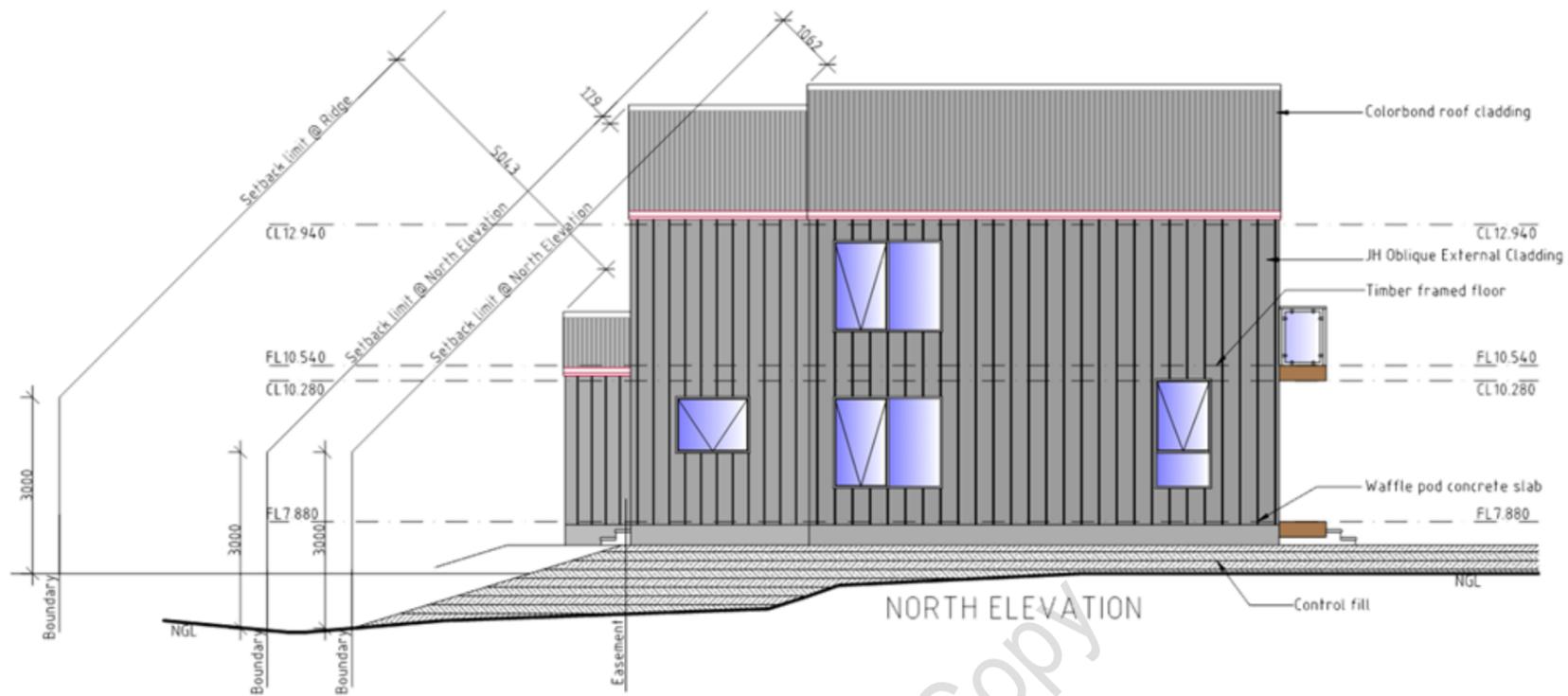


UNIT 3

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PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                                  |         |                         |   |
|----------------------------------|---------|-------------------------|---|
| UNIT 3 ELEVATIONS                |         | DATE<br>12/01/26        | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE 1:100, 1:20<br>0 1000 2000 | AMENDED | DRAWING NO.<br>25 OF 79 | Certified G. Tilley Accreditation No:CC620H<br>© copyright 2024 5:024       |



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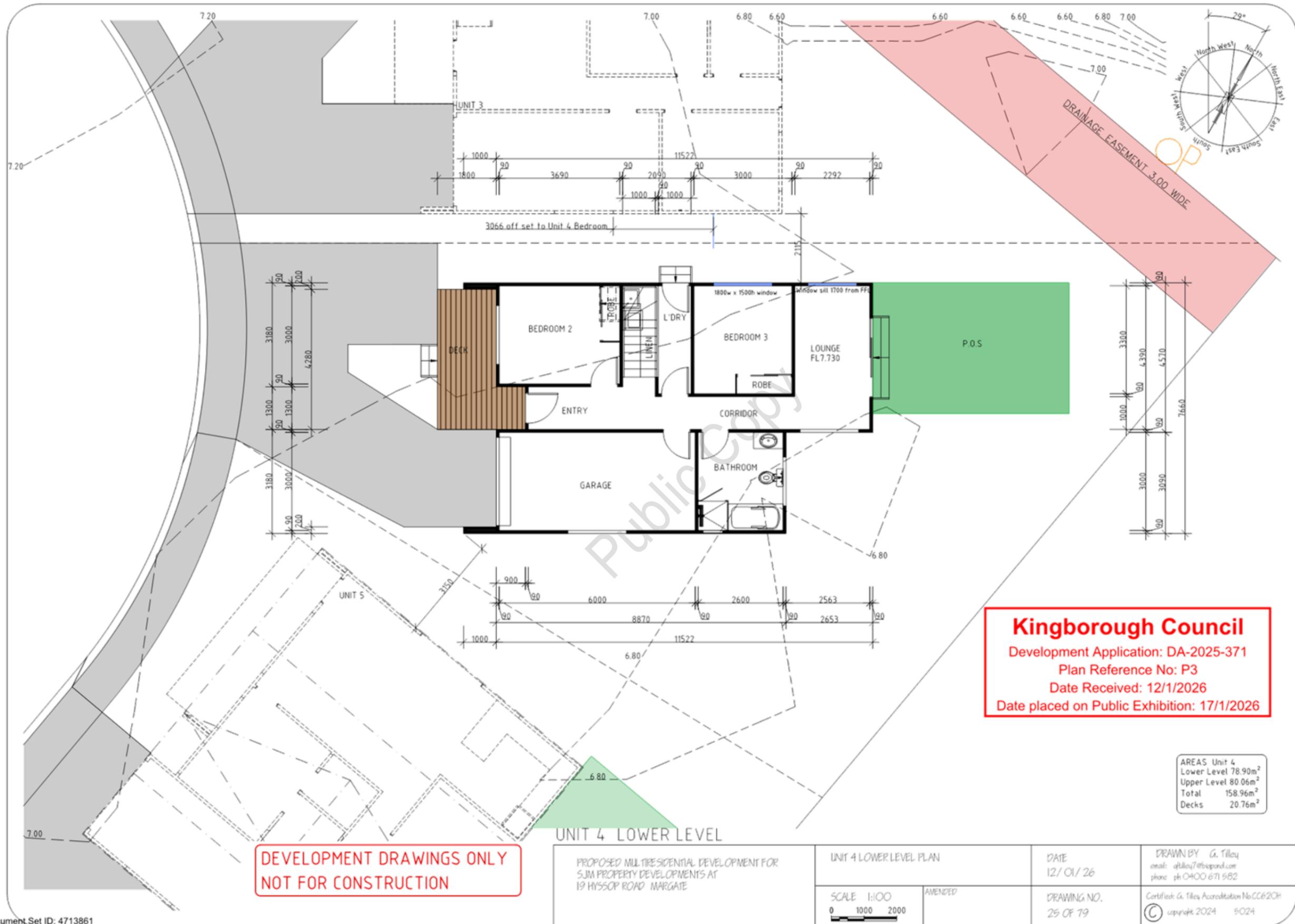
**Kingborough Council**  
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 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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UNIT 3

|  |                            |                  |   |
|--|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 3 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|  | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>24 OF 79   |

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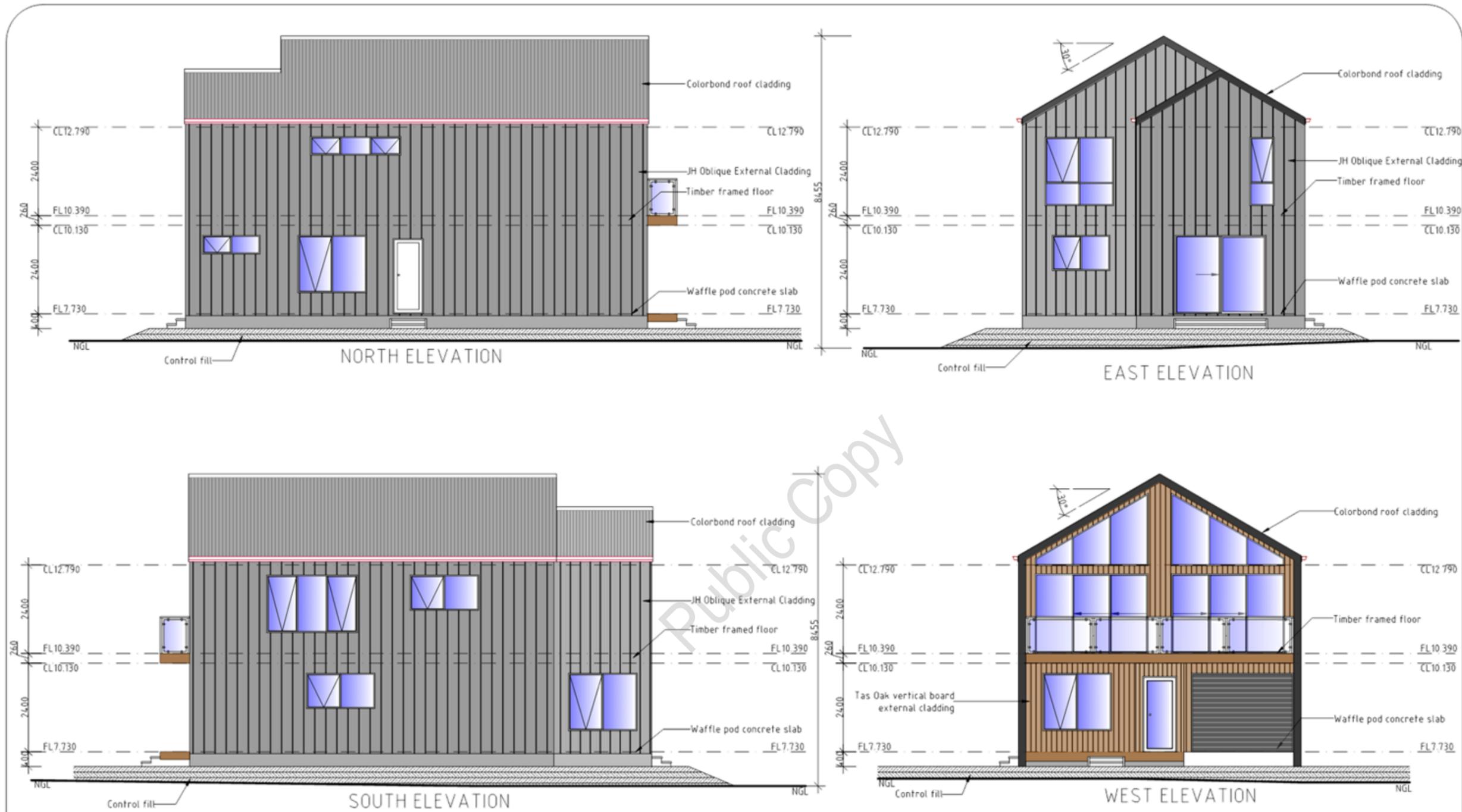
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|                                 |
|---------------------------------|
| AREAS Unit 4                    |
| Lower Level 78.90m <sup>2</sup> |
| Upper Level 80.06m <sup>2</sup> |
| Total 158.96m <sup>2</sup>      |
| Decks 20.76m <sup>2</sup>       |

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**UNIT 4 UPPER LEVEL**

|   |                            |                    |  |
|---|----------------------------|--------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 4 UPPER LEVEL PLAN    | DATE<br>12/ 01/ 26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED            | DRAWING NO.<br>26 OF 19  |
|   |                            |                    | Certified G. Tilley Accreditation No:CC620H<br>© copyright 2024 5:024          |



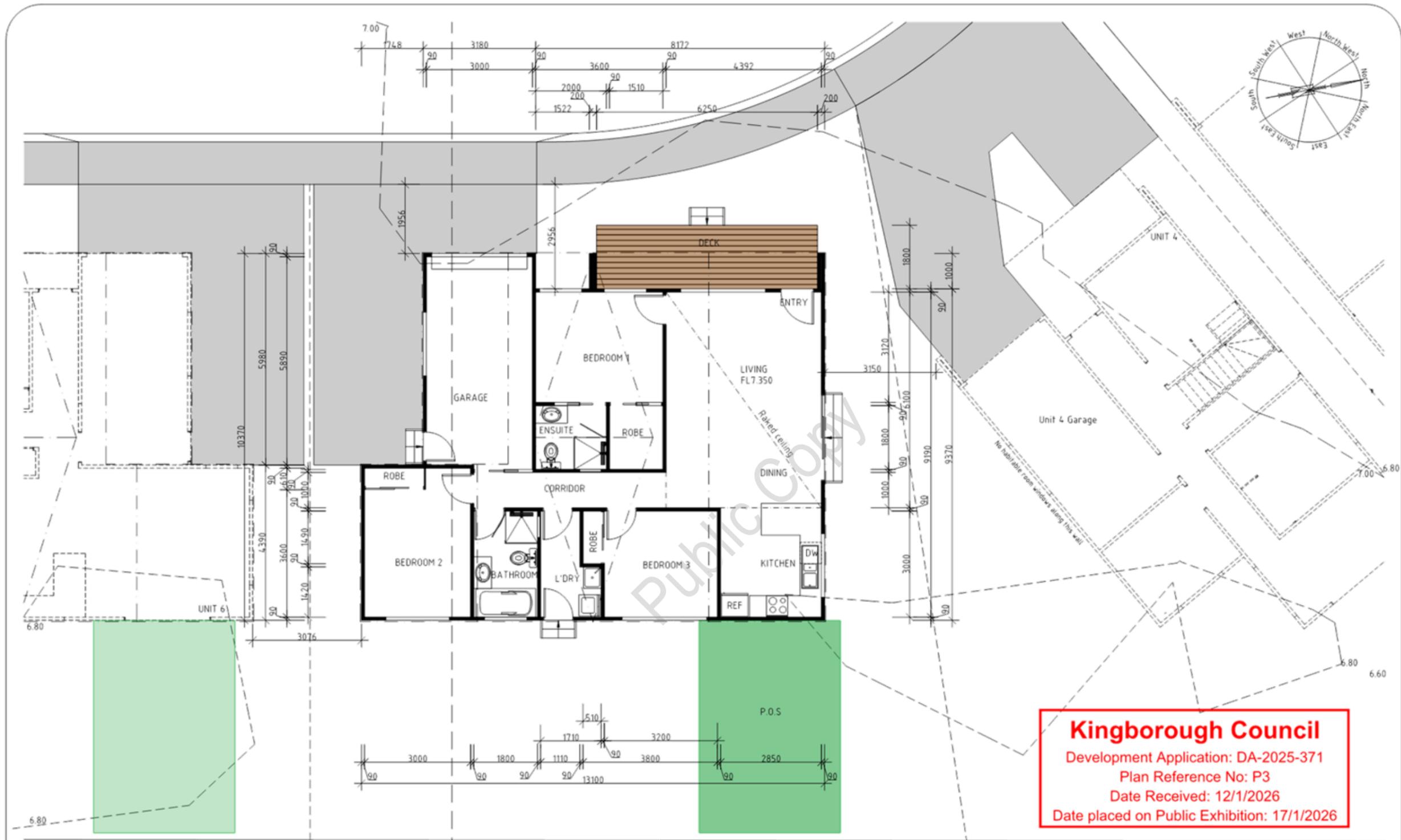
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 Date placed on Public Exhibition: 17/1/2026

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**UNIT 4**

|   |                   |                    |   |
|---|-------------------|--------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 4 ELEVATIONS | DATE<br>12/ 01/ 26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>   | AMENDED            | DRAWING NO.<br>27 OF 79   |
|   |                   |                    | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5:024      |



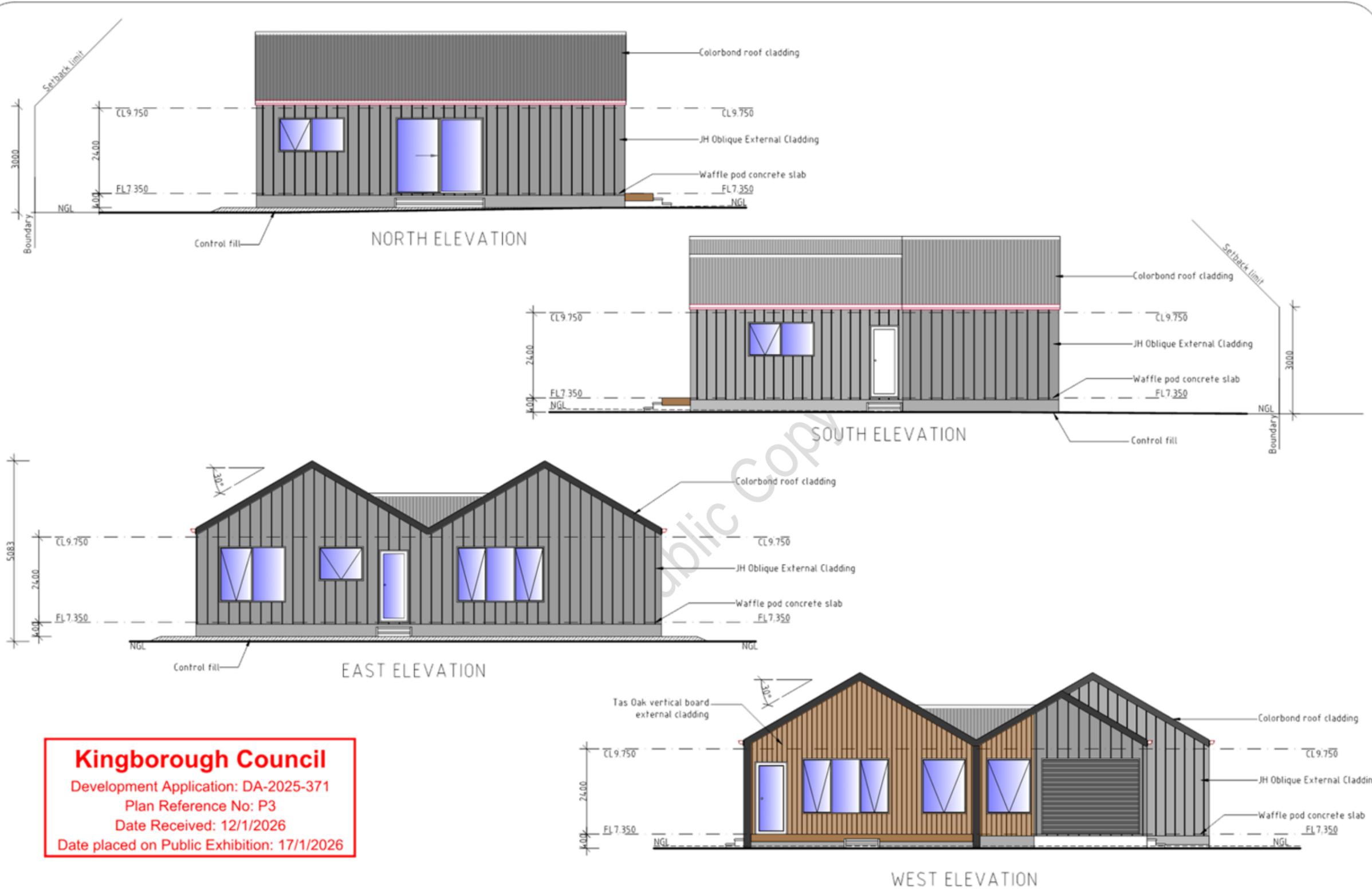
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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|        |                      |
|--------|----------------------|
| AREAS  |                      |
| UNIT 5 | 177.22m <sup>2</sup> |
| Deck   | 11.20m <sup>2</sup>  |

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**UNIT 5**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |                  |  |
|----------------------------|------------------|--|
| UNIT 5 PLAN                | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>28 OF 19  |
|                            |                  | Certified G. Tilley Accreditation No:CC6620H<br>© copyright 2024 5:024         |

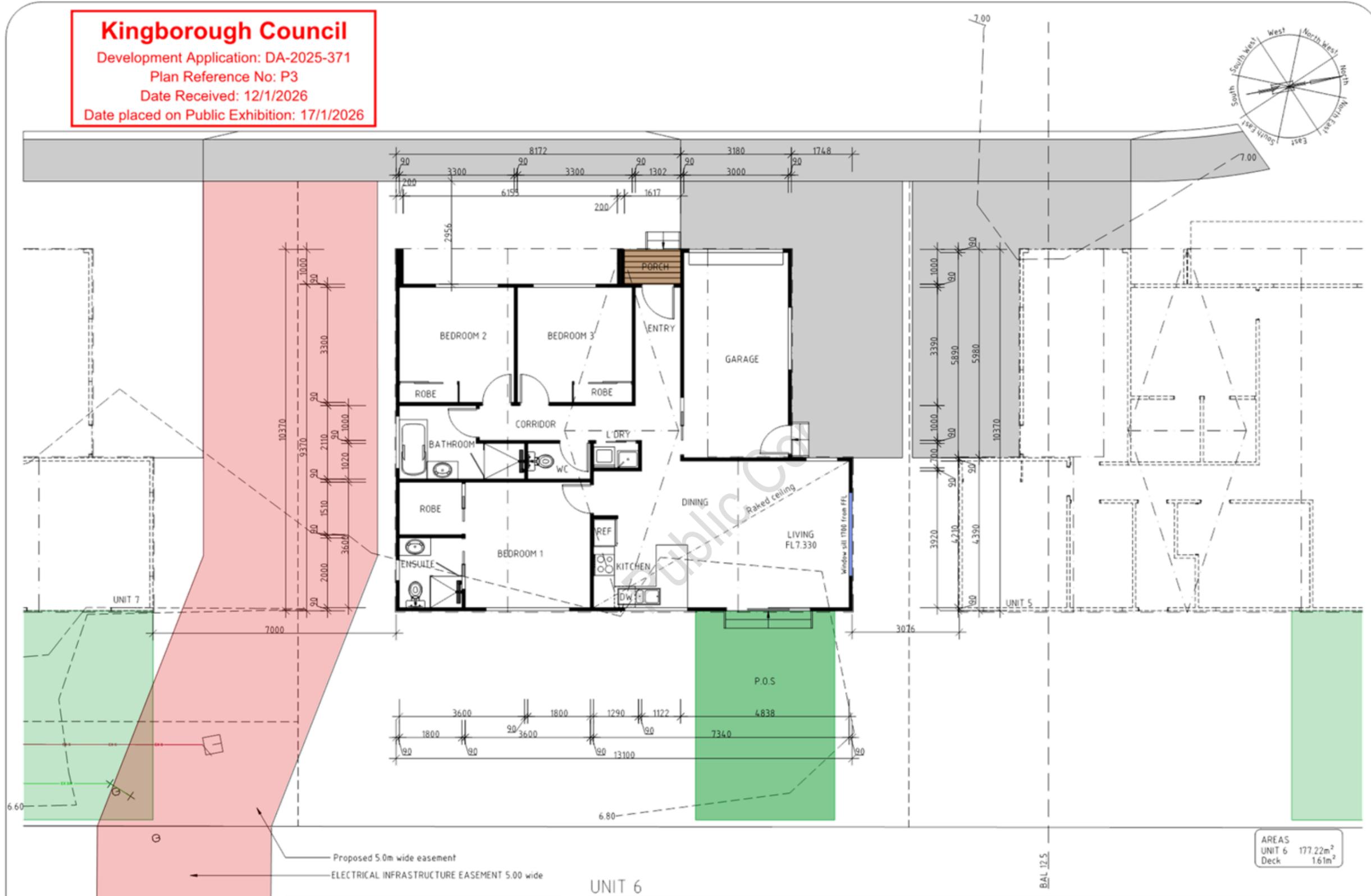


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|   |                            |                  |   |
|---|----------------------------|------------------|---|
| UNIT 5<br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 5 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>29 OF 79   |

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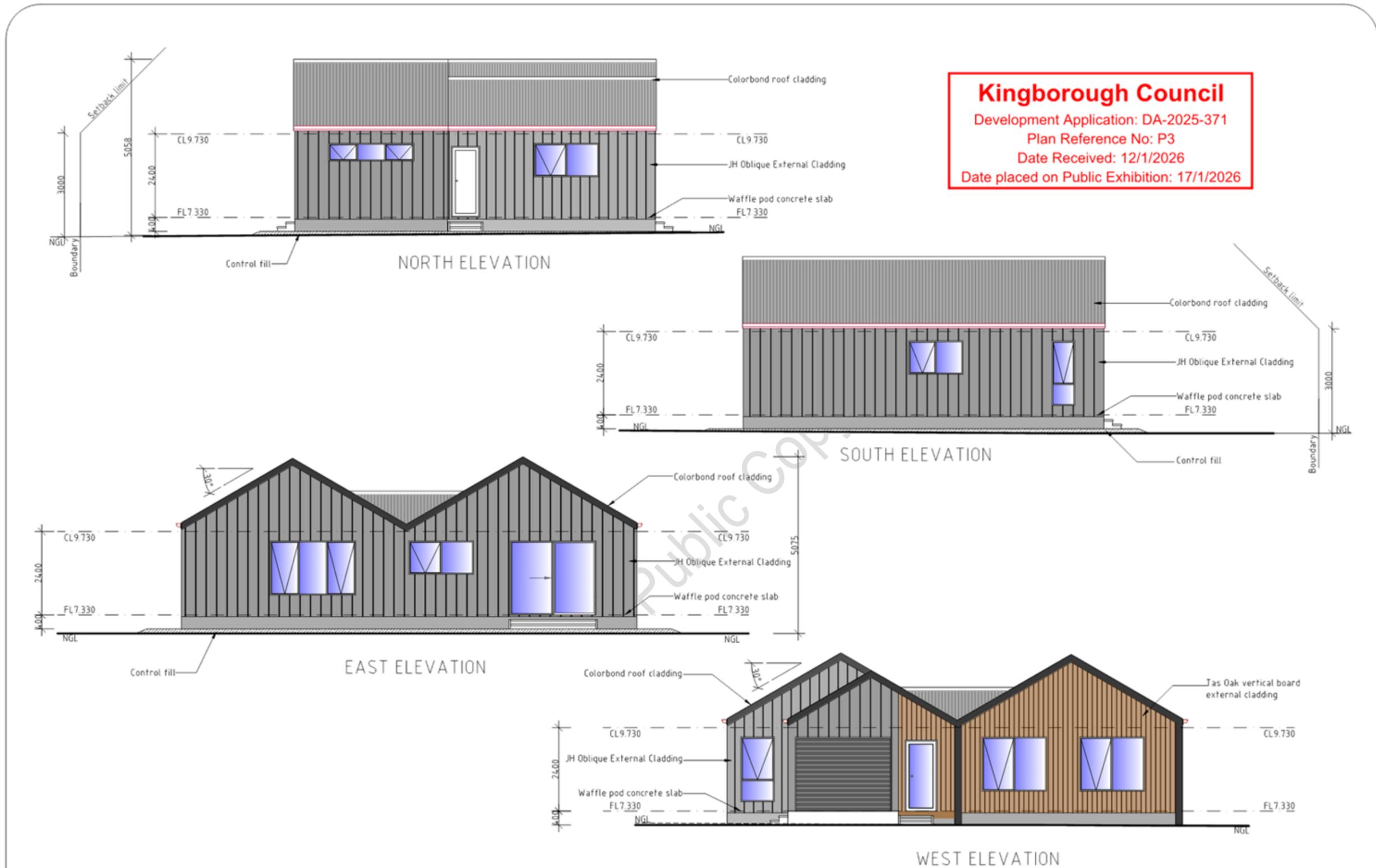


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**UNIT 6**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |  |
|----------------------------|---------|-------------------------|--|
| UNIT 6 PLAN                |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE 1:100<br>0 1000 2000 | REVISED | DRAWING NO.<br>30 OF 79 | Certified G. Tilley Accreditation No: CC6201<br>© copyright 2024 5:024         |

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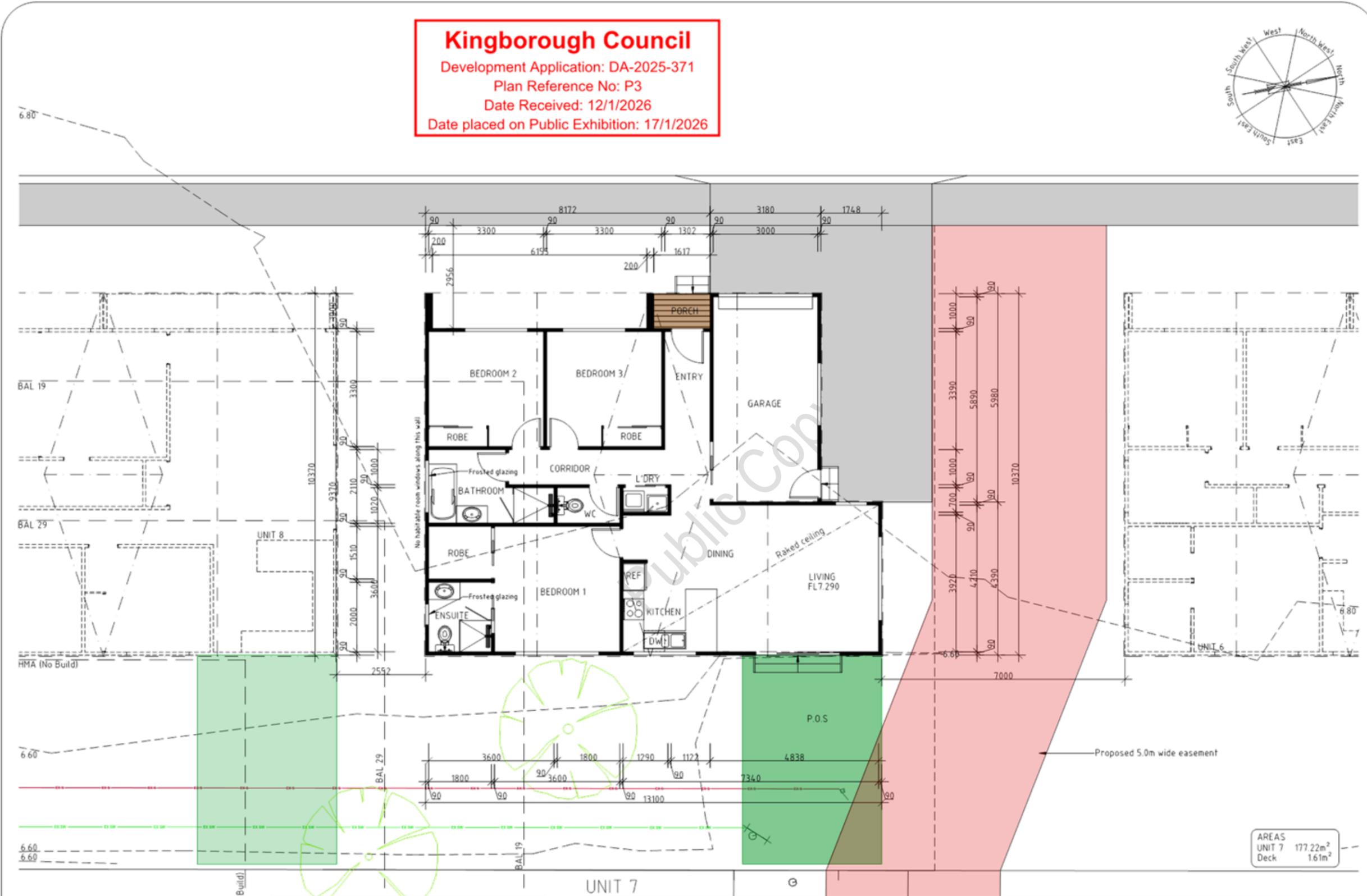


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**UNIT 6**

|   |                            |                  |   |
|---|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 6 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>31 OF 79   |

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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026



AREAS  
 UNIT 7 177.22m<sup>2</sup>  
 Deck 161m<sup>2</sup>

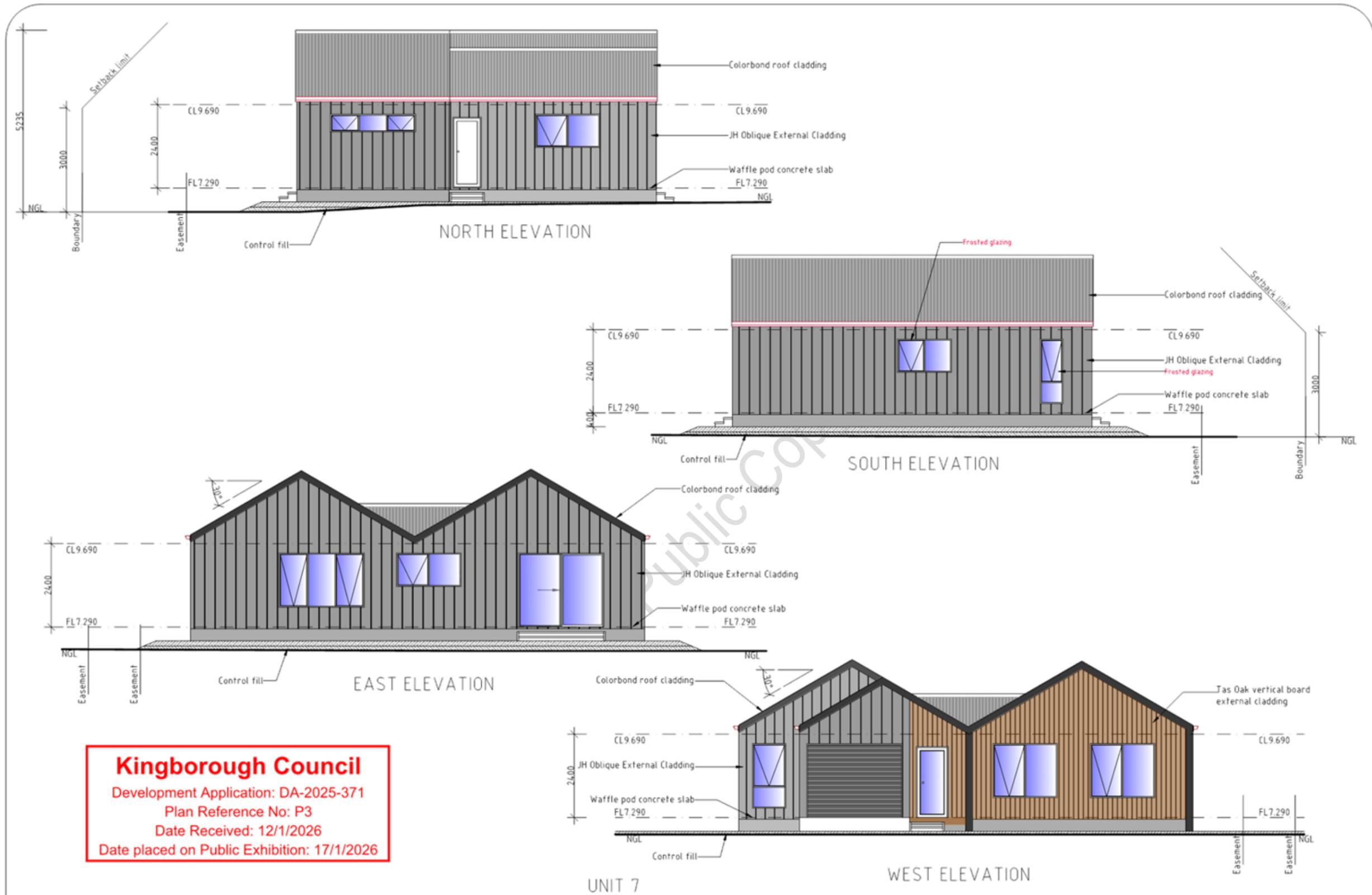
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UNIT 7  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

UNIT 7 PLAN  
 SCALE 1:100  
 0 1000 2000

DATE  
 12/01/26  
 DRAWING NO.  
 32 OF 79

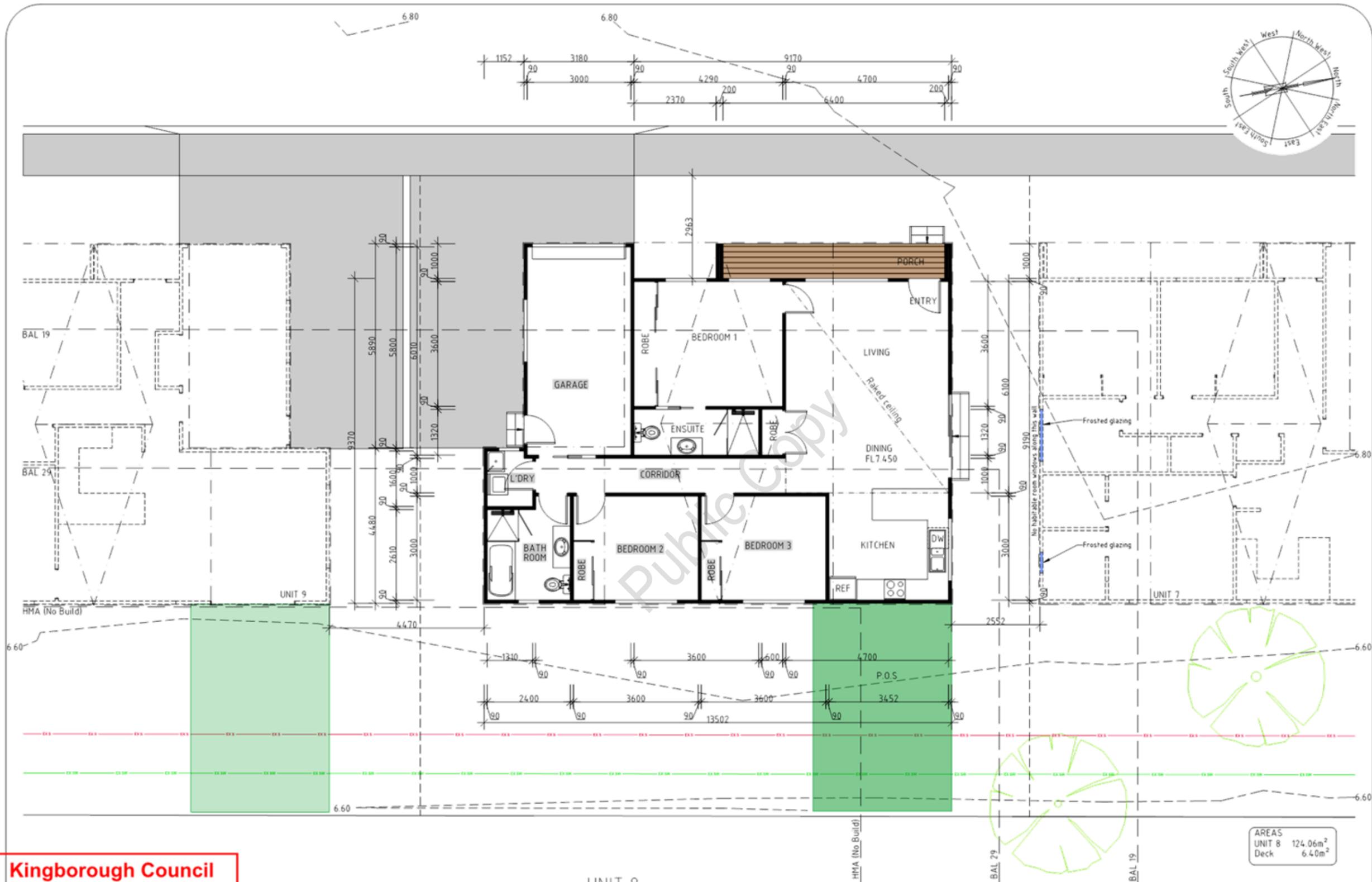
DRAWN BY G. Tilley  
 email: gtilley7@bigpond.com  
 phone: ph 0400 671 982  
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|   |                            |                  |   |
|---|----------------------------|------------------|---|
| UNIT 7<br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 7 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>33 OF 79   |



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 Date Received: 12/1/2026  
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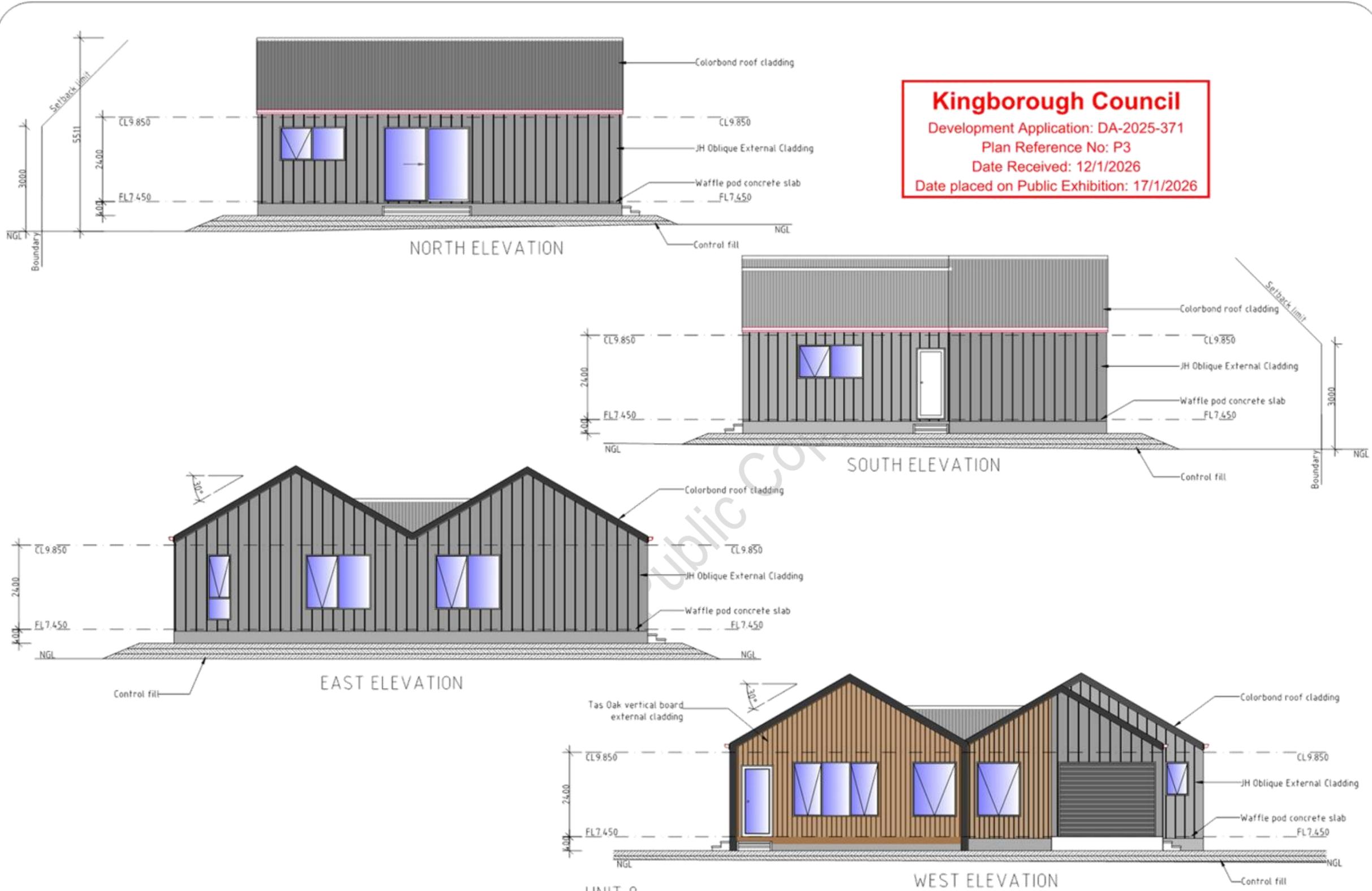
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**UNIT 8**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|             |  |             |   |
|-------------|--|-------------|---|
| UNIT 8 PLAN |  | DATE        | DRAWN BY                                    |
| SCALE 1:100 |  | 12/01/26    | G. Tilley                                   |
| 0 1000 2000 |  |             | email: gtilley7@bigpond.com                 |
| REVISED     |  | DRAWING NO. | phone: ph 0400 671 982                      |
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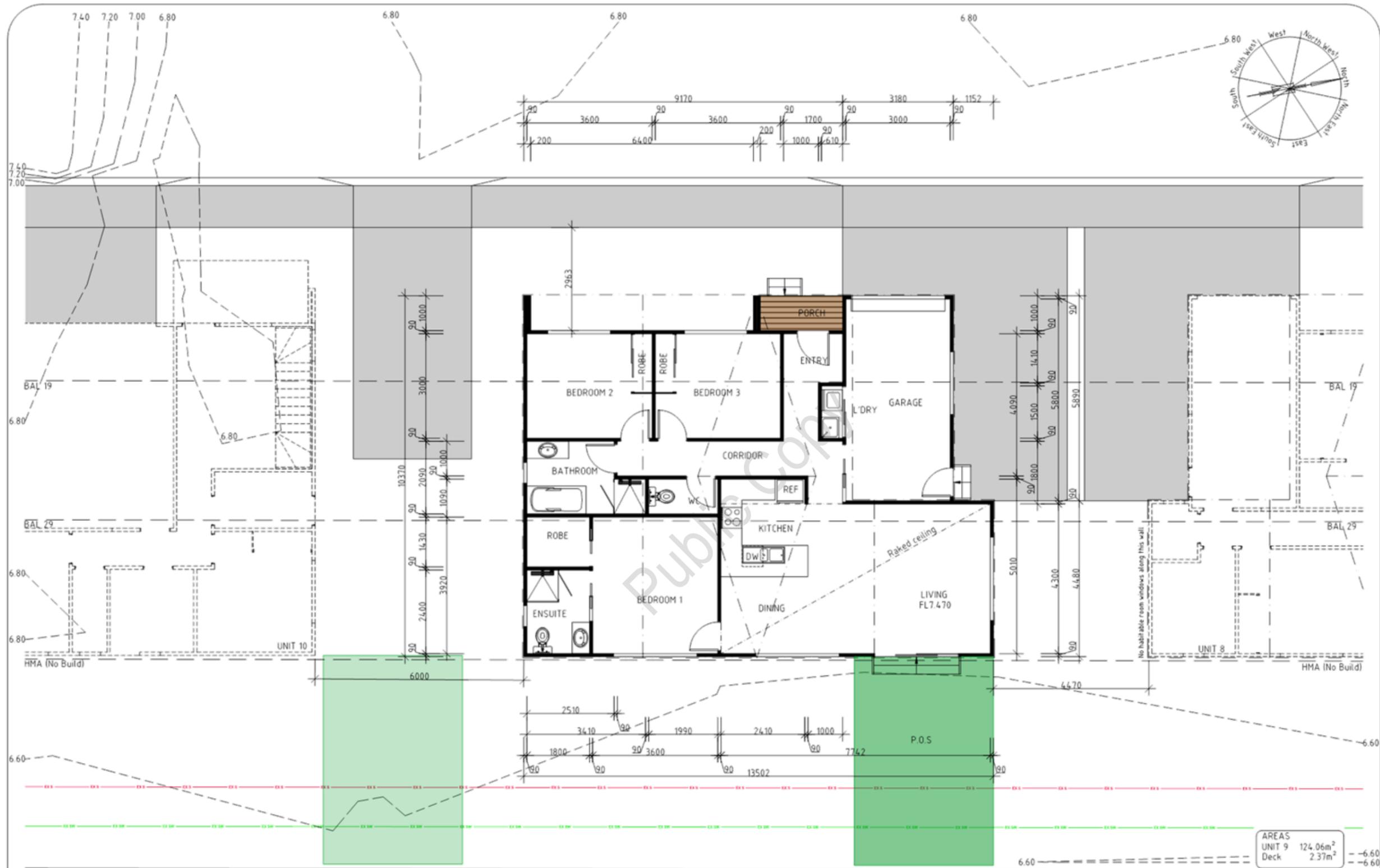
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|  |                            |                  |  |
|--|----------------------------|------------------|--|
| <b>UNIT 8</b><br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HYSSOP ROAD MARGATE | UNIT 8 ELEVATIONS          | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
|  | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>35 OF 19  |



|        |                      |
|--------|----------------------|
| AREAS  |                      |
| UNIT 9 | 124.06m <sup>2</sup> |
| Deck   | 2.37m <sup>2</sup>   |

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 Plan Reference No: P3  
 Date Received: 12/1/2026  
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**UNIT 9**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

**UNIT 9 PLAN**  
 SCALE 1:100  
 0 1000 2000

DATE  
 12/01/26  
 DRAWING NO.  
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 phone: ph 0400 671 982  
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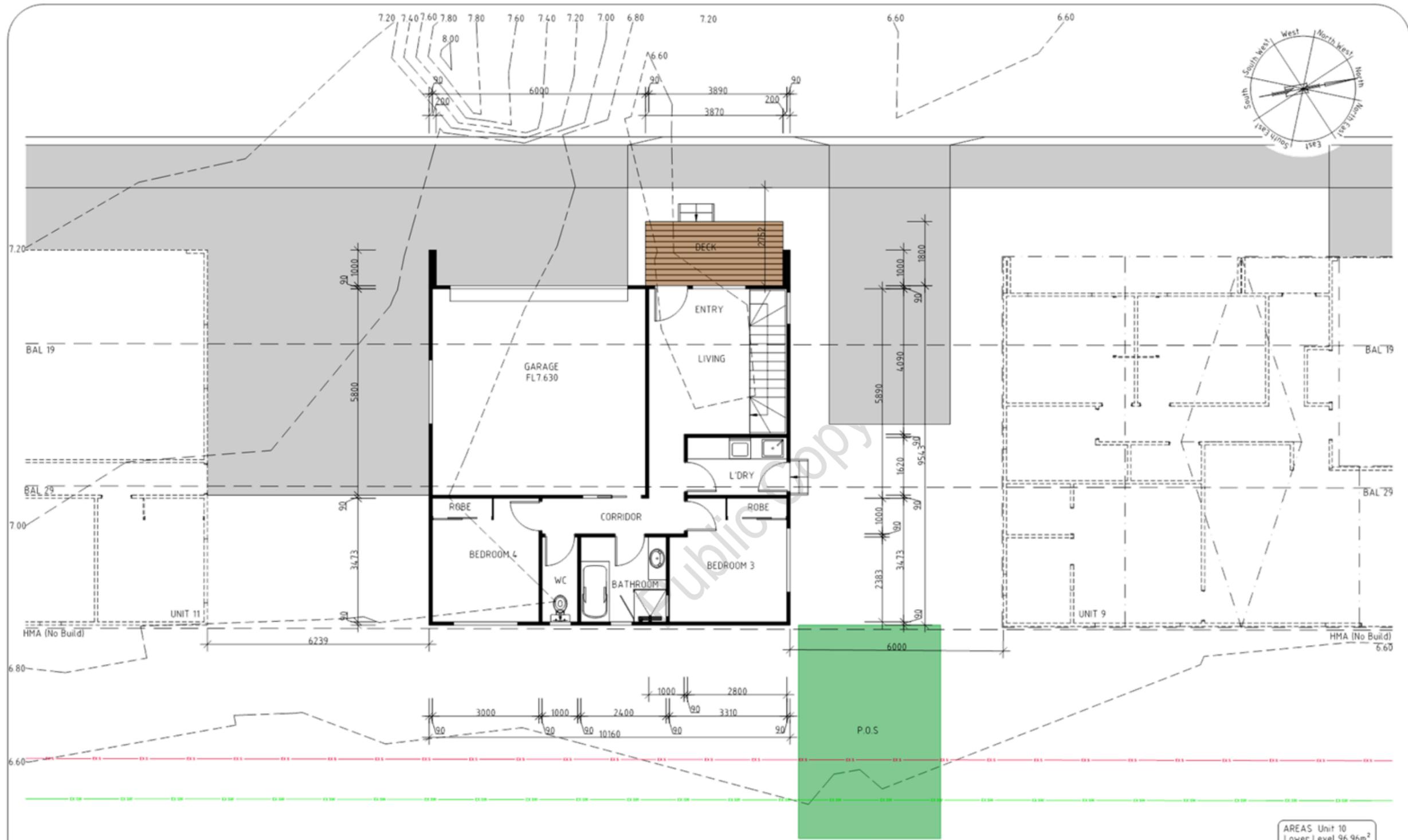


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 Plan Reference No: P3  
 Date Received: 12/1/2026  
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|   |  |                            |         |                         |   |
|---|--|----------------------------|---------|-------------------------|---|
| <b>UNIT 9</b>   |  | <b>UNIT 9 ELEVATIONS</b>   |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HYSSOP ROAD MARGATE |  | SCALE 1:100<br>0 1000 2000 | REVISED | DRAWING NO.<br>57 OF 79 | Certified G. Tilley Accreditation No:CC620H<br>© copyright 2024 6:024         |

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 Version: 2. Version Date: 16/01/2026



|                                 |
|---------------------------------|
| AREAS Unit 10                   |
| Lower Level 96.96m <sup>2</sup> |
| Upper Level 96.96m <sup>2</sup> |
| Total 193.95m <sup>2</sup>      |
| Decks 24.52m <sup>2</sup>       |

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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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**UNIT 10 LOWER LEVEL**

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |  |
|----------------------------|---------|-------------------------|--|
| UNIT 10 LOWER LEVEL PLAN   |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|                                 |
|---------------------------------|
| AREAS Unit 10                   |
| Lower Level 96.96m <sup>2</sup> |
| Upper Level 96.96m <sup>2</sup> |
| Total 193.95m <sup>2</sup>      |
| Decks 24.52m <sup>2</sup>       |

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UNIT 10 UPPER LEVEL

|   |                            |                    |  |
|---|----------------------------|--------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 10 UPPER LEVEL PLAN   | DATE<br>12/ 01/ 26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED            | DRAWING NO.<br>39 OF 79  |
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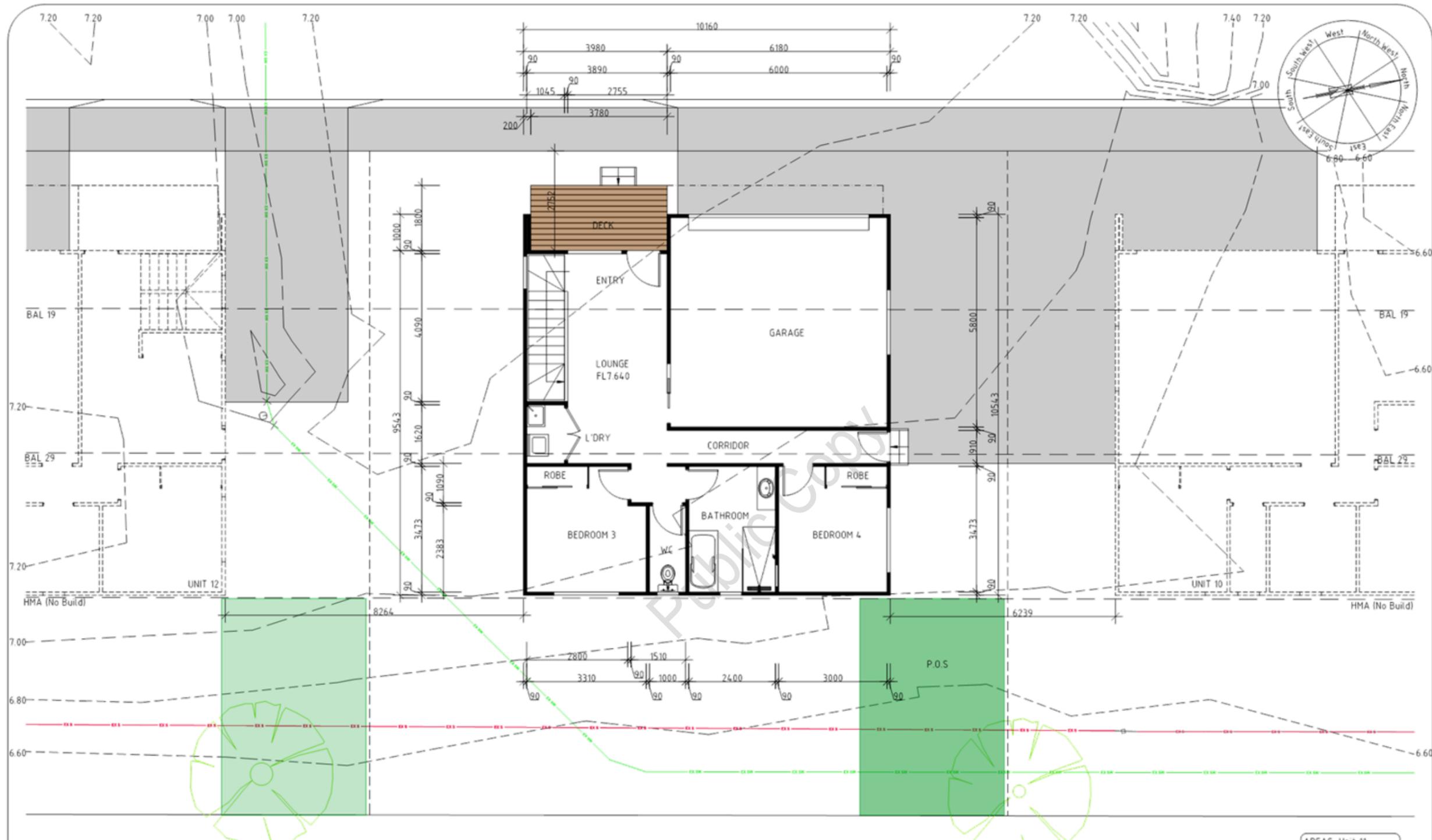
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
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**UNIT 10**

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |  |
|----------------------------|---------|-------------------------|--|
| UNIT 10 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE 1:100<br>0 1000 2000 | AMENDED | DRAWING NO.<br>40 OF 79 | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5:024         |



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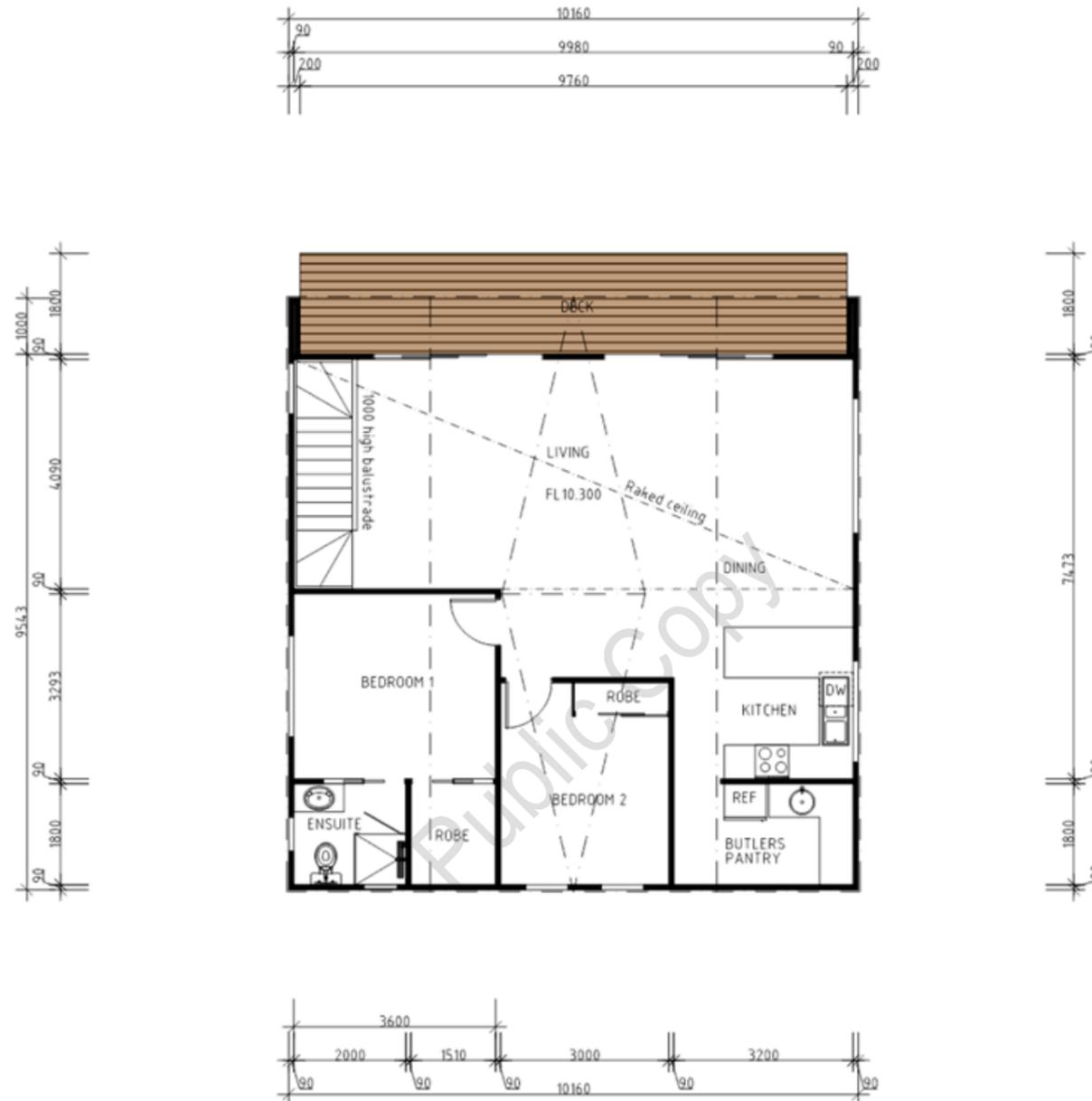
**DEVELOPMENT DRAWINGS ONLY  
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|                                  |
|----------------------------------|
| AREAS Unit 11                    |
| Lower Level 103.14m <sup>2</sup> |
| Upper Level 96.96m <sup>2</sup>  |
| Total 200.10m <sup>2</sup>       |
| Decks 24.36m <sup>2</sup>        |

**UNIT 11 LOWER LEVEL**

|  |                            |                  |  |
|--|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 11 LOWER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|  | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>41 OF 79  |

Document Set ID: 4713861  
 Version: 2. Version Date: 16/01/2026



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 Date placed on Public Exhibition: 17/1/2026

|                                  |
|----------------------------------|
| AREAS Unit 11                    |
| Lower Level 103.14m <sup>2</sup> |
| Upper Level 96.96m <sup>2</sup>  |
| Total 200.10m <sup>2</sup>       |
| Decks 24.36m <sup>2</sup>        |

UNIT 11 UPPER LEVEL

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|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 11 UPPER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>42 OF 79  |
|   |                            |                  | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5/024         |

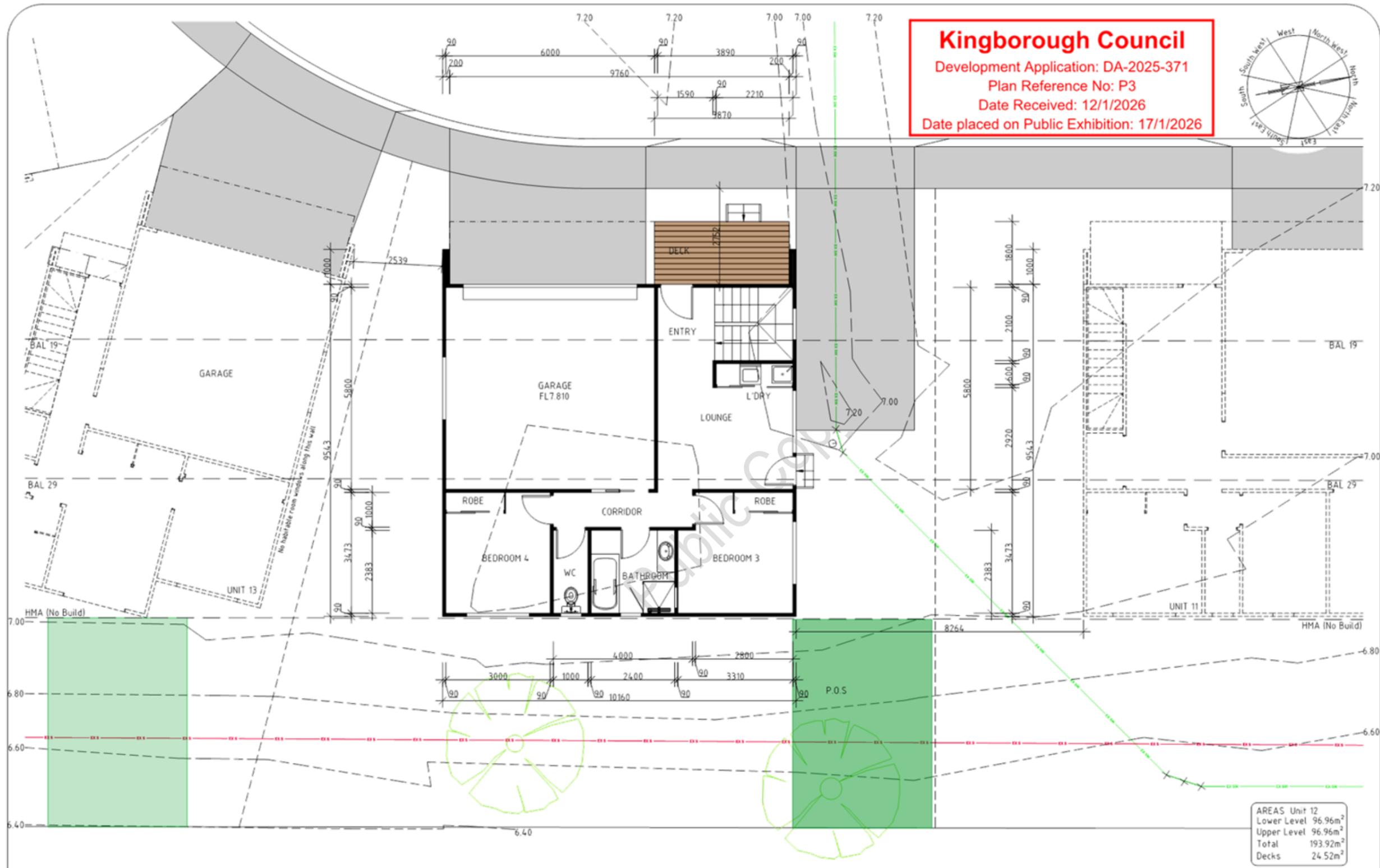


**Kingborough Council**  
 Development Application: DA-2025-371  
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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

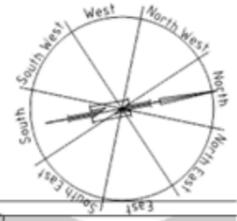
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|  |                            |                  |  |
|--|----------------------------|------------------|--|
| <b>UNIT 11</b><br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 11 ELEVATIONS         | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
|  | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>43 OF 79  |

Document Set ID: 4713861  
 Version: 2. Version Date: 16/01/2026



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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026



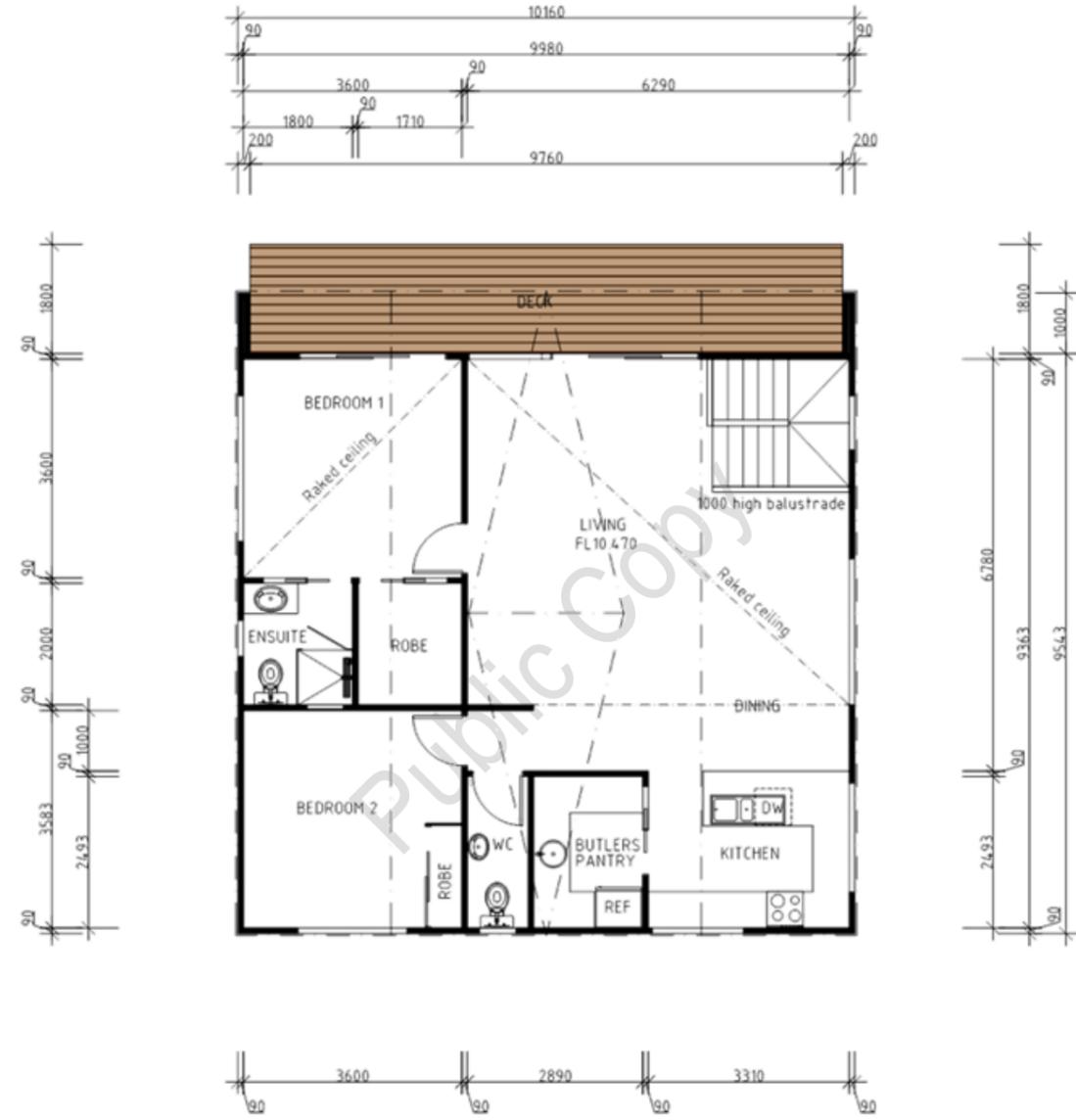
|                                 |
|---------------------------------|
| AREAS Unit 12                   |
| Lower Level 96.96m <sup>2</sup> |
| Upper Level 96.96m <sup>2</sup> |
| Total 193.92m <sup>2</sup>      |
| Decks 24.52m <sup>2</sup>       |

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**UNIT 12 LOWER LEVEL**

|  |                            |                  |  |
|--|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 12 LOWER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|  | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>44 OF 79  |

Document Set ID: 4713861  
 Version: 2. Version Date: 16/01/2026



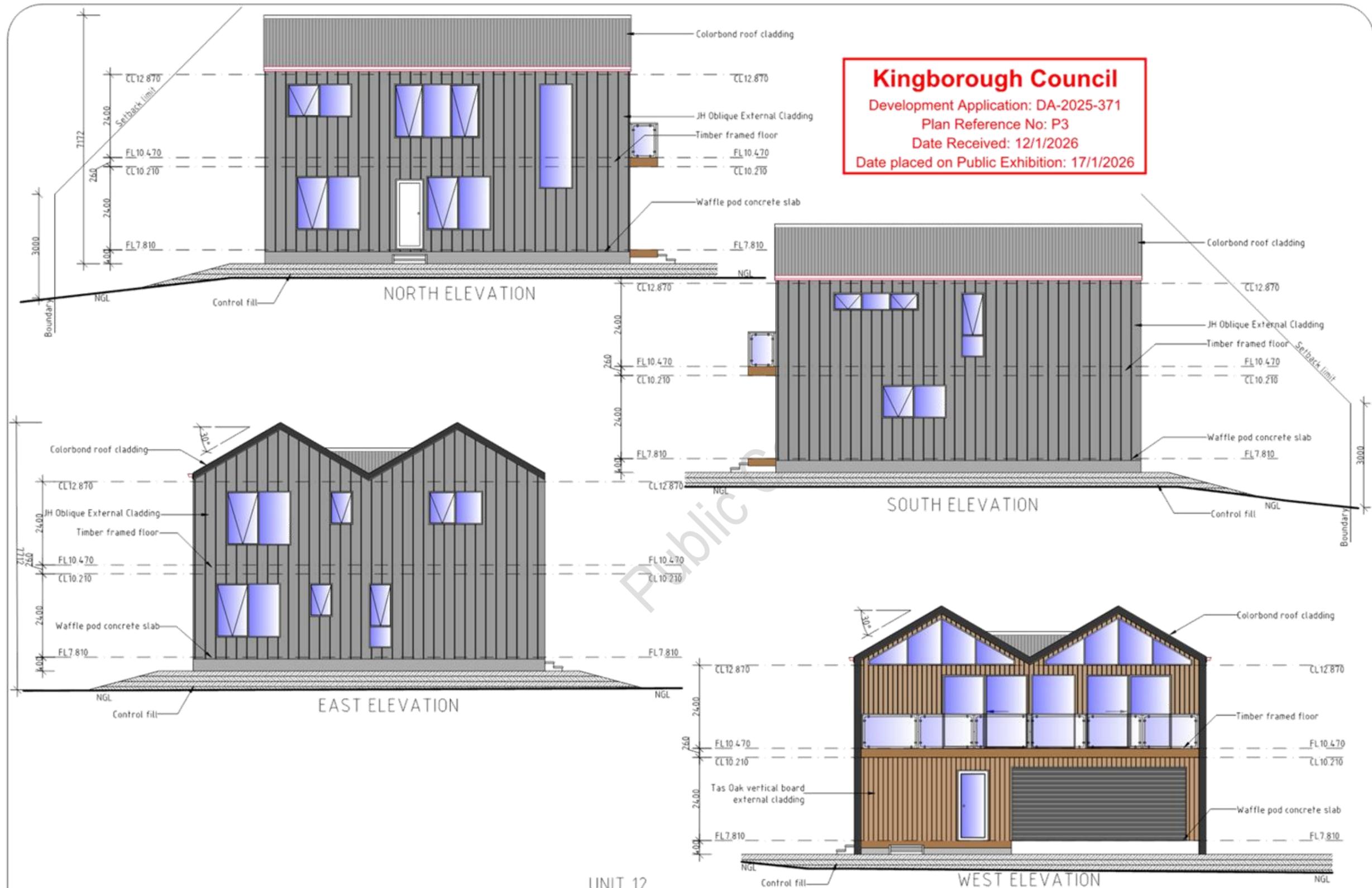
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|               |                      |
|---------------|----------------------|
| AREAS Unit 12 |                      |
| Lower Level   | 96.96m <sup>2</sup>  |
| Upper Level   | 96.96m <sup>2</sup>  |
| Total         | 193.92m <sup>2</sup> |
| Decks         | 24.52m <sup>2</sup>  |

**DEVELOPMENT DRAWINGS ONLY  
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UNIT 12 UPPER LEVEL

|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 12 UPPER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>45 OF 79  |
|   |                            |                  | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5/024         |

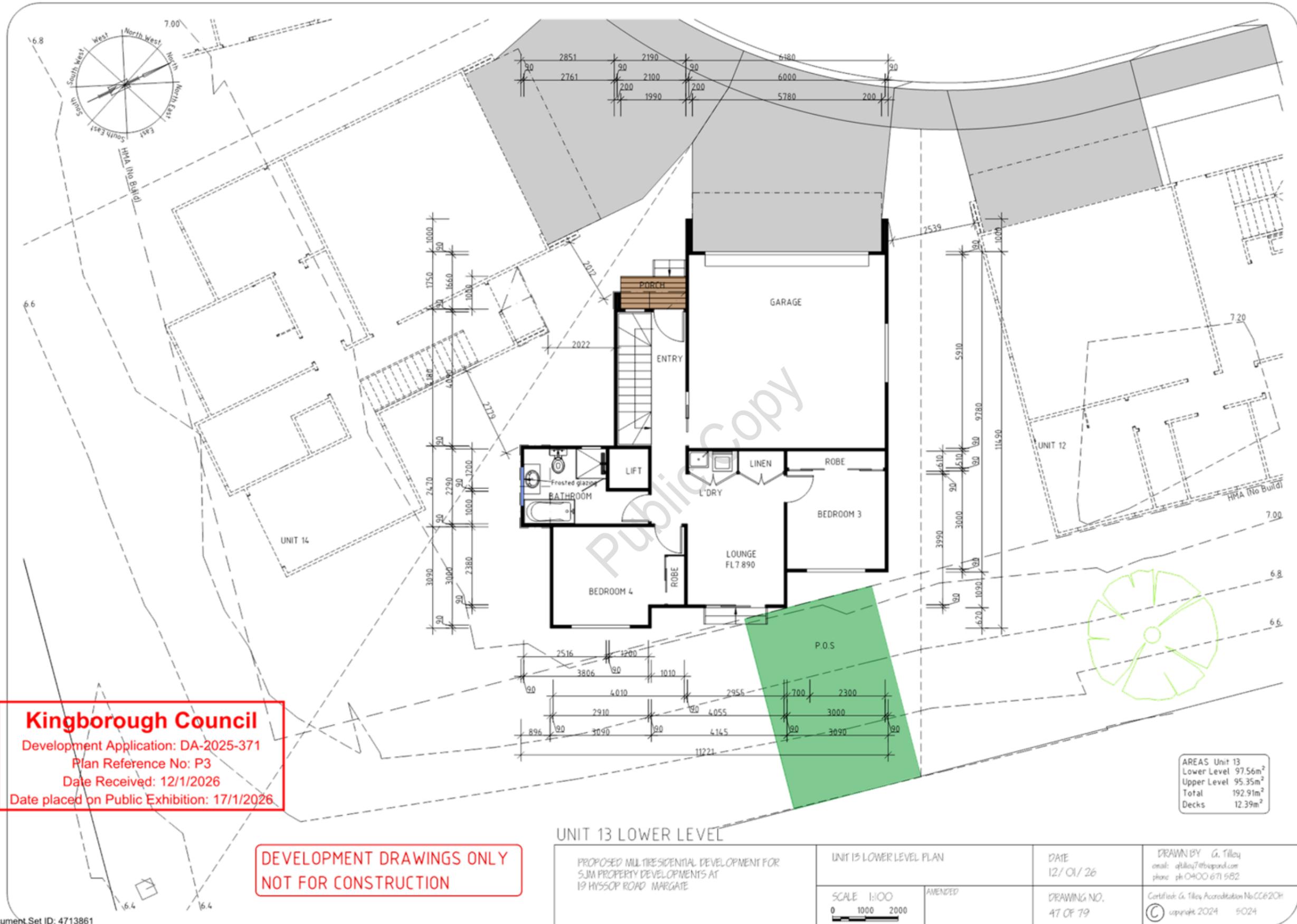


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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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**UNIT 12**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                               |         |                         |   |
|-------------------------------|---------|-------------------------|---|
| UNIT 12 ELEVATIONS            |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE<br>1:100<br>0 1000 2000 | REVISED | DRAWING NO.<br>46 OF 79 | Certified G. Tilley Accreditation No: CC6201<br>© copyright 2024 6:024        |



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|                                 |
|---------------------------------|
| AREAS Unit 13                   |
| Lower Level 97.56m <sup>2</sup> |
| Upper Level 95.35m <sup>2</sup> |
| Total 192.91m <sup>2</sup>      |
| Decks 12.39m <sup>2</sup>       |

**UNIT 13 LOWER LEVEL**

|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 13 LOWER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>47 OF 79  |



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 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|                                 |
|---------------------------------|
| AREAS Unit 13                   |
| Lower Level 97.56m <sup>2</sup> |
| Upper Level 95.35m <sup>2</sup> |
| Total 192.91m <sup>2</sup>      |
| Decks 12.39m <sup>2</sup>       |

**DEVELOPMENT DRAWINGS ONLY  
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**UNIT 13 UPPER LEVEL**

|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 13 UPPER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>48 OF 79  |
|   |                            |                  | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5:024         |

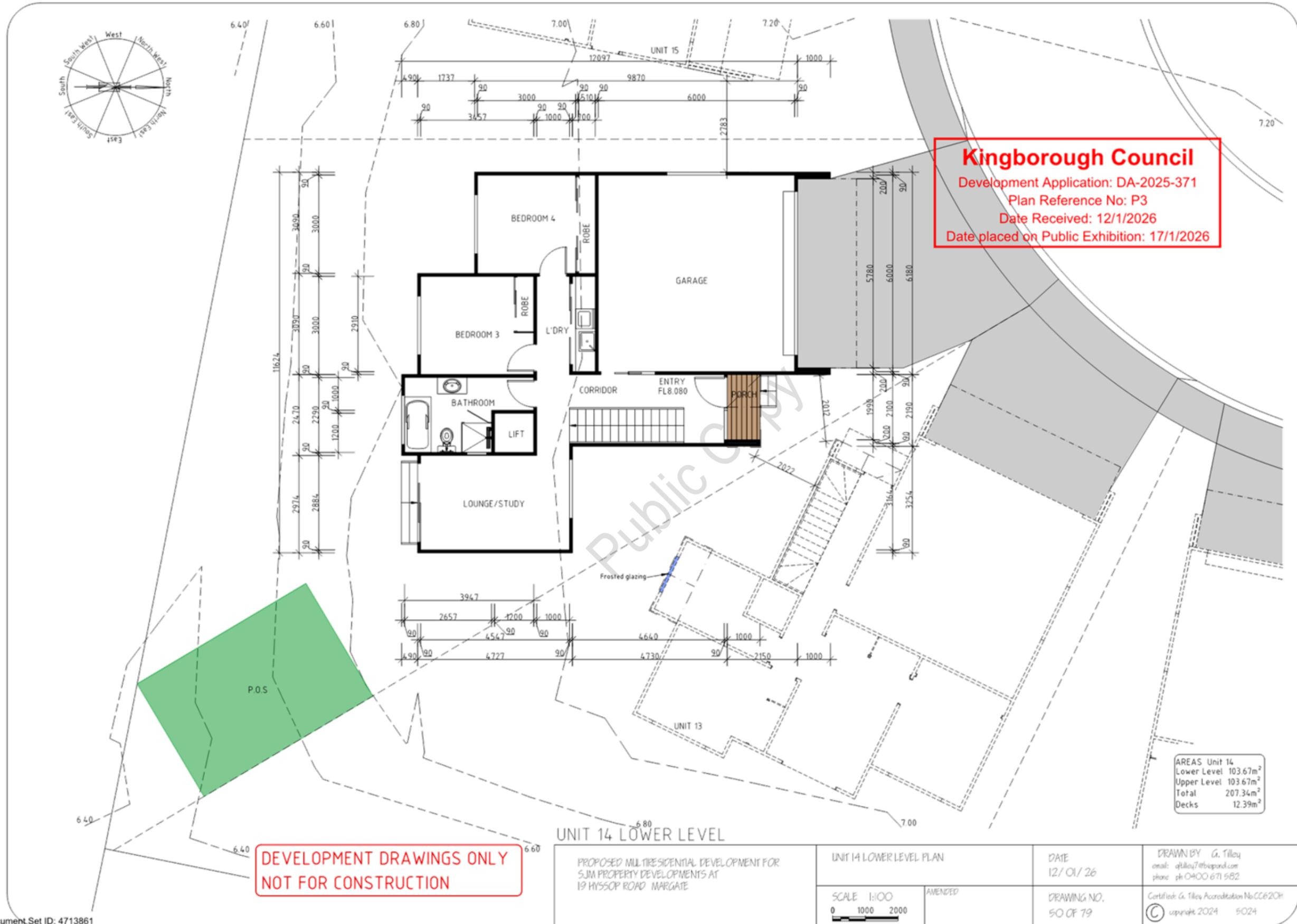


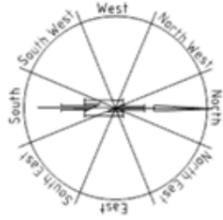
**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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**UNIT 13**

|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 13 ELEVATIONS         | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>49 OF 79  |





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 Development Application: DA-2025-371  
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 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

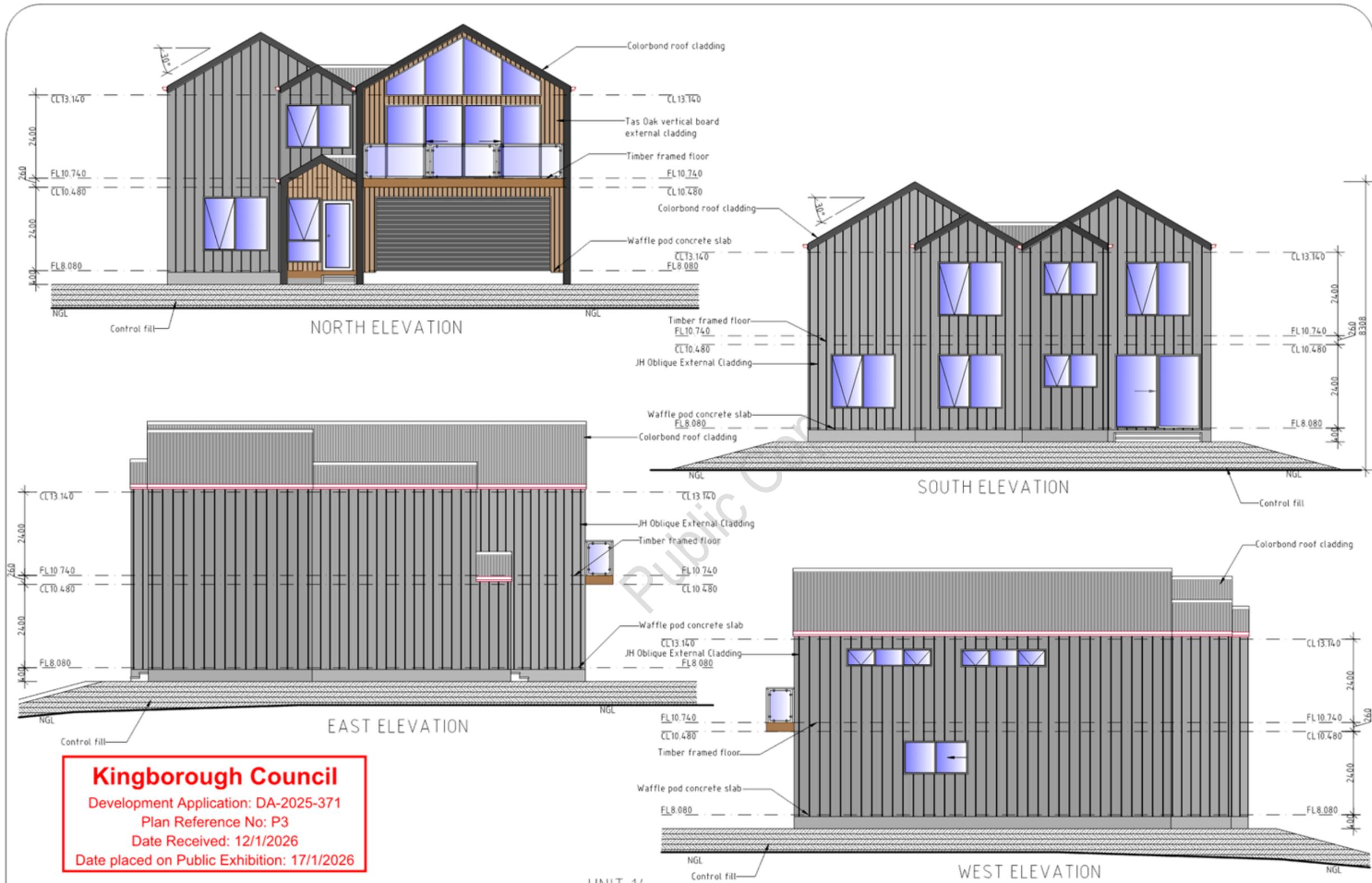
|                                  |
|----------------------------------|
| AREAS Unit 14                    |
| Lower Level 103.67m <sup>2</sup> |
| Upper Level 103.67m <sup>2</sup> |
| Total 207.34m <sup>2</sup>       |
| Decks 12.39m <sup>2</sup>        |

**DEVELOPMENT DRAWINGS ONLY  
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UNIT 14 UPPER LEVEL

|   |                            |                    |   |
|---|----------------------------|--------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 14 UPPER LEVEL PLAN   | DATE<br>12/ 01/ 26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED            | DRAWING NO.<br>51 OF 79   |
|   |                            |                    | Certified G. Tilley Accreditation No: CC620H<br>© copyright 2024 5:024      |

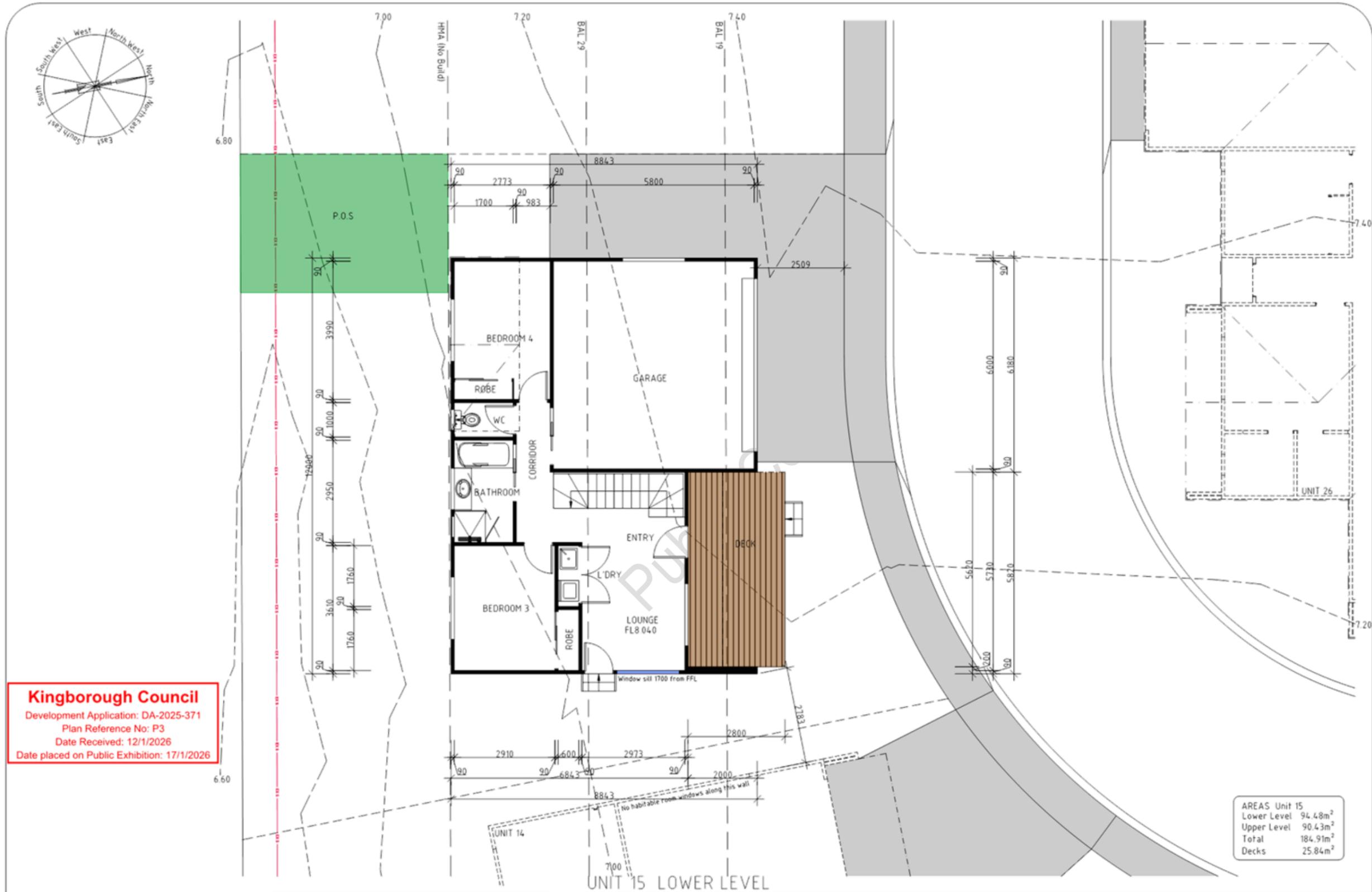
Document Set ID: 4713861  
 Version: 2. Version Date: 16/01/2026



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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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|   |  |                                 |                |                                |   |
|---|--|---------------------------------|----------------|--------------------------------|---|
| <b>UNIT 14</b>  |  | <b>UNIT 14 ELEVATIONS</b>       |                | <b>DATE</b><br>12/01/26        | <b>DRAWN BY</b> G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |  | <b>SCALE</b> 1:100<br>0 200 400 | <b>REVISED</b> | <b>DRAWING NO.</b><br>52 OF 79 | <b>Certified G. Tilley Accreditation No</b> CC620H<br>© copyright 2024 5/024      |



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 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

|               |                      |
|---------------|----------------------|
| AREAS Unit 15 |                      |
| Lower Level   | 94.48m <sup>2</sup>  |
| Upper Level   | 90.43m <sup>2</sup>  |
| Total         | 184.91m <sup>2</sup> |
| Decks         | 25.84m <sup>2</sup>  |

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**UNIT 15 LOWER LEVEL**

|  |                            |                  |  |
|--|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 15 LOWER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|  | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>55 OF 79  |



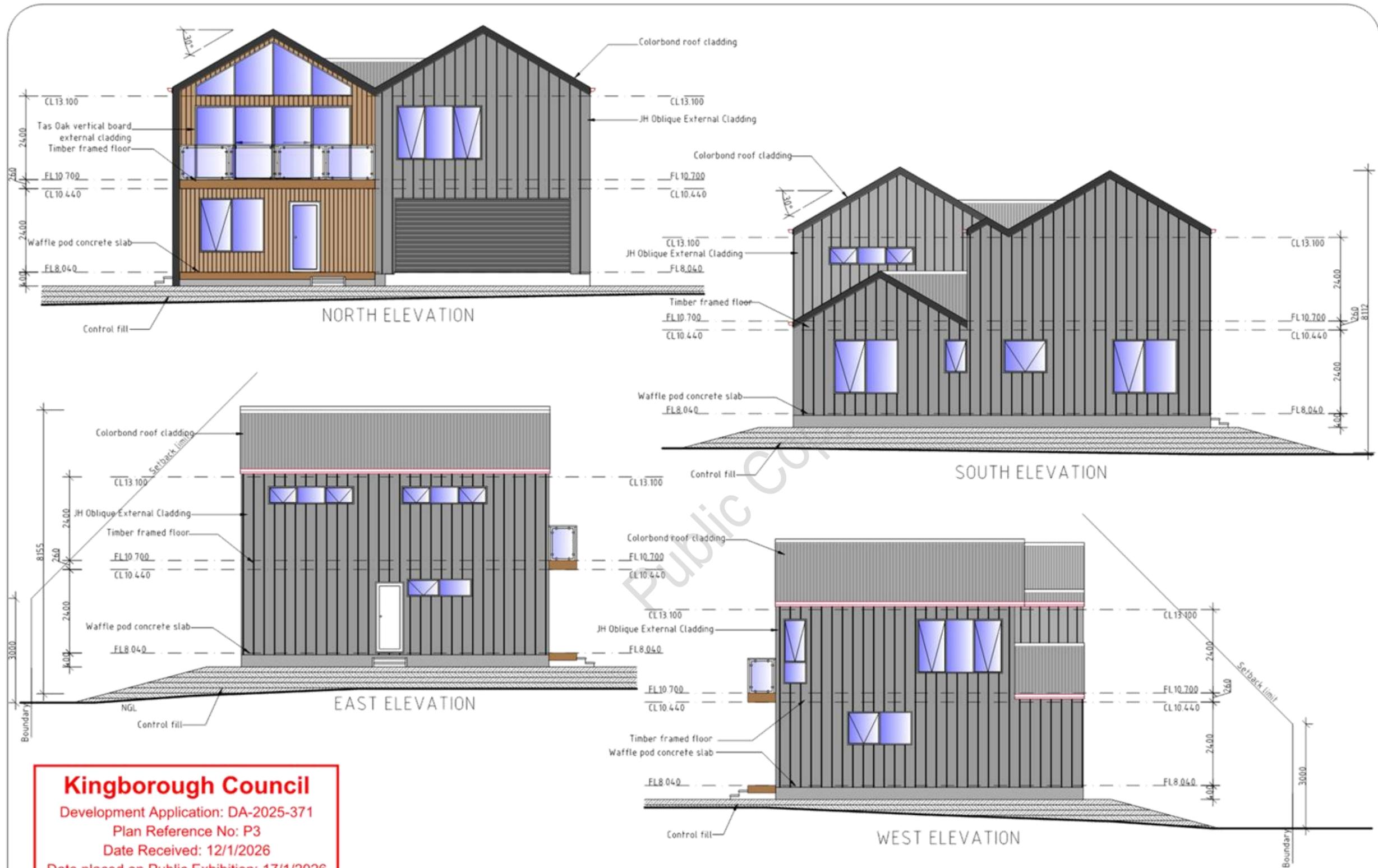
**Kingborough Council**  
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 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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|                                 |
|---------------------------------|
| AREAS Unit 15                   |
| Lower Level 94.48m <sup>2</sup> |
| Upper Level 90.43m <sup>2</sup> |
| Total 184.91m <sup>2</sup>      |
| Decks 25.84m <sup>2</sup>       |

UNIT 15 UPPER LEVEL

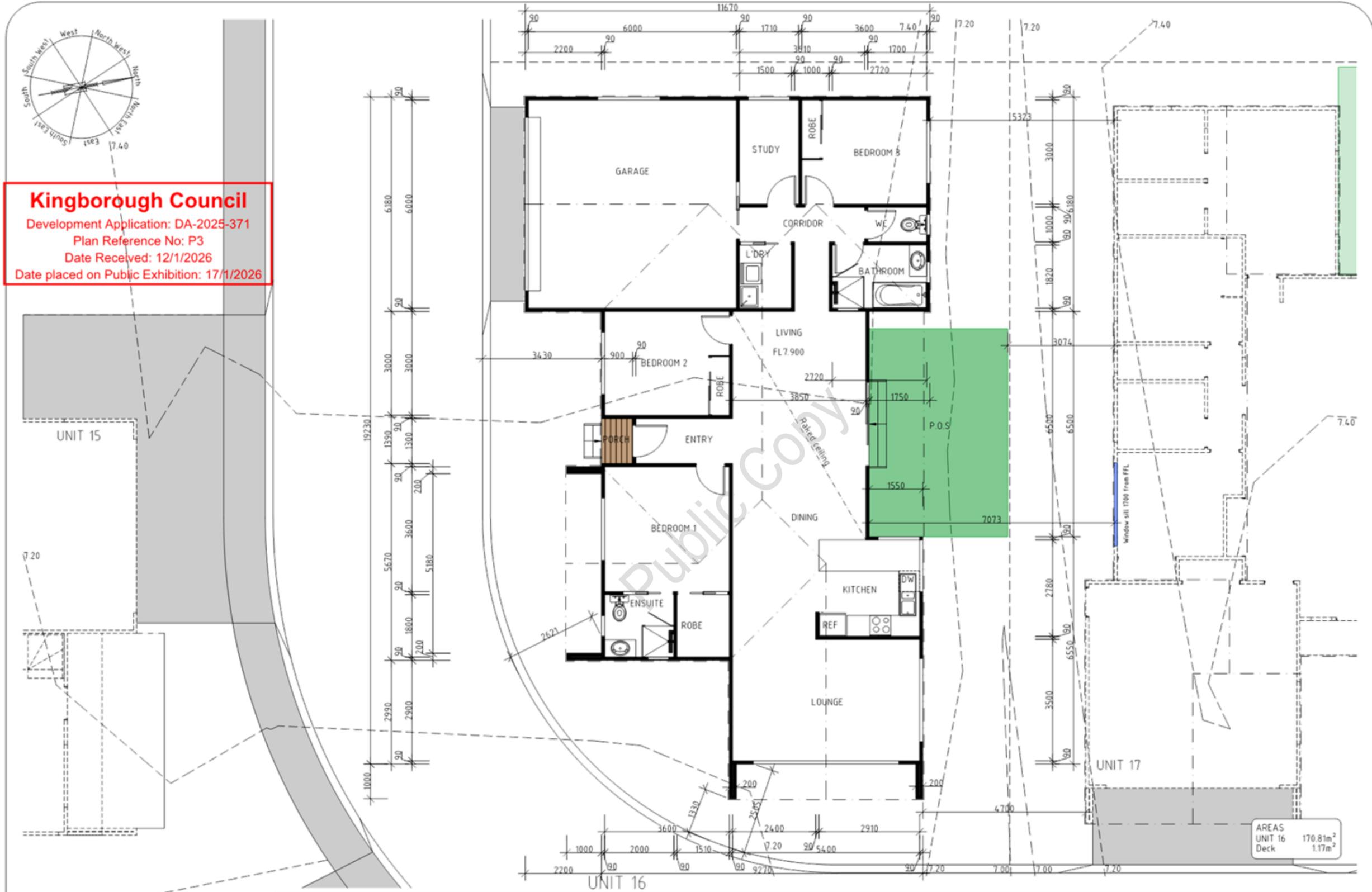
|   |                            |                  |  |
|---|----------------------------|------------------|--|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 15 UPPER LEVEL PLAN   | DATE<br>12/01/26 | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>54 OF 79  |
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|   |  |                            |                  |  |
|---|--|----------------------------|------------------|--|
| UNIT 15   |  | UNIT 15 ELEVATIONS         | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HUSSOP ROAD MARGATE |  | SCALE 1:100<br>0 1000 2000 | AMENDED          | DRAWING NO.<br>55 OF 79  |
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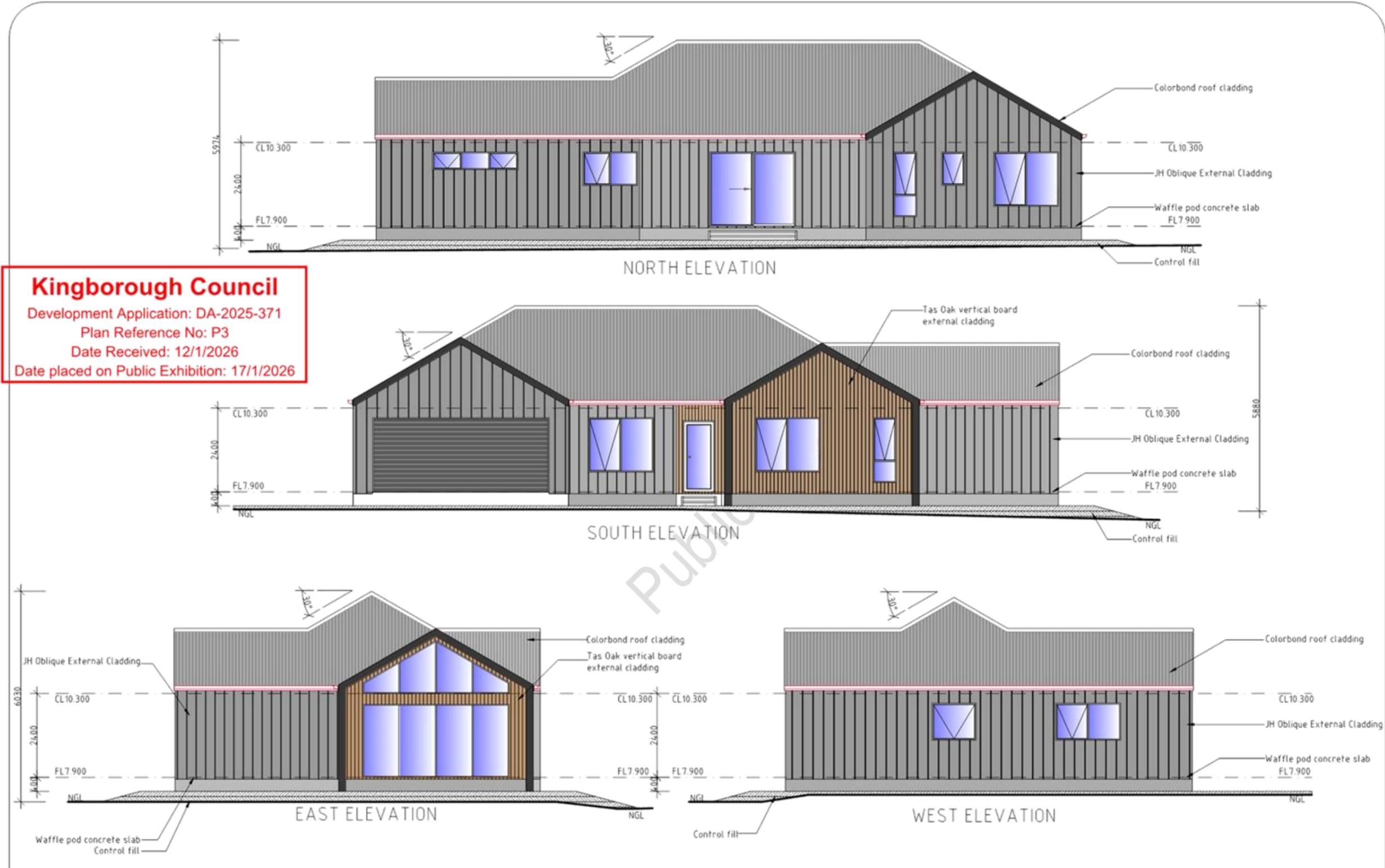


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 Date placed on Public Exhibition: 17/1/2026

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|  |  |  |                         |   |
|--|--|--|-------------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |  | UNIT 16 PLAN<br>SCALE 1:100<br>0 1000 2000 | DATE<br>12/01/26        | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| UNIT 16<br>AREA<br>UNIT 16 170.81m <sup>2</sup><br>Deck 1.17m <sup>2</sup>                           |  | AMENDED                                    | DRAWING NO.<br>56 OF 79 | Certified G. Tilley Accreditation No. CC620H<br>© copyright 2024 5024       |

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 Version: 2. Version Date: 16/01/2026

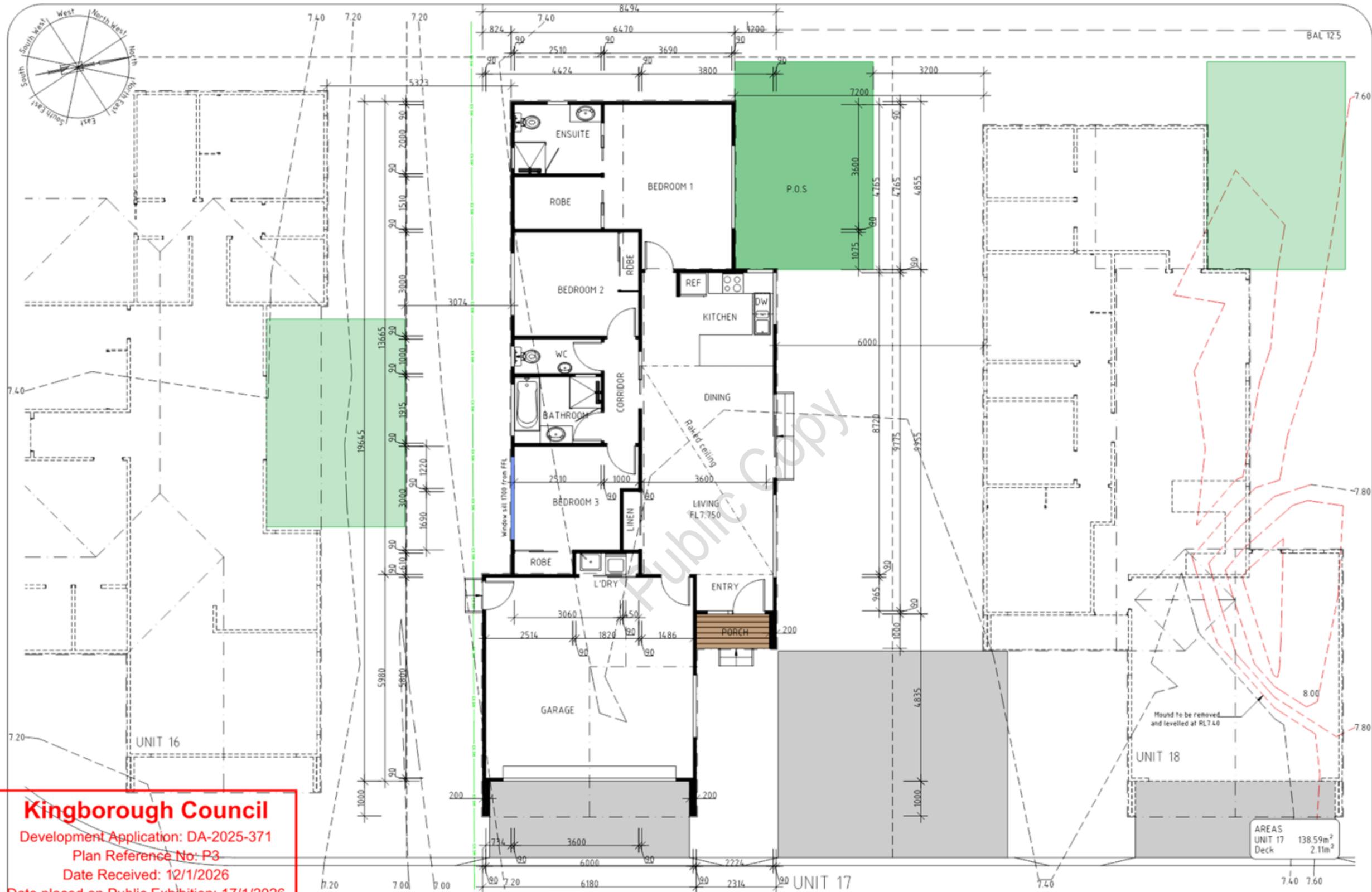


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|  |  |                            |         |                         |  |
|--|--|----------------------------|---------|-------------------------|--|
| UNIT 16<br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |  | UNIT 16 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
|  |  | SCALE 1:100<br>0 1000 2000 | AMENDED | DRAWING NO.<br>57 OF 79 | Certified G. Tilley Accreditation No: CC6201<br>© copyright 2024 6/024     |

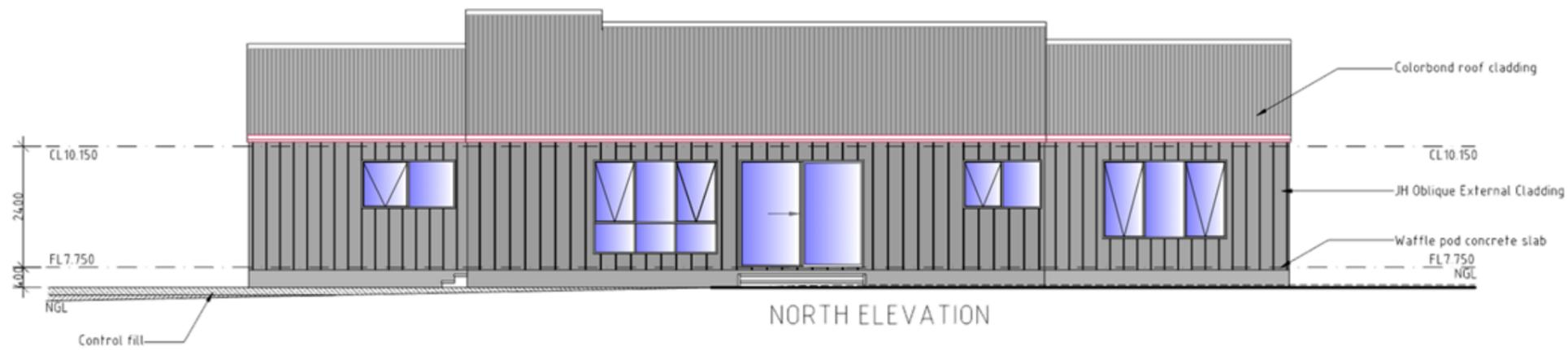
Document Set ID: 4713861  
 Version: 2. Version Date: 16/01/2026



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 Date placed on Public Exhibition: 17/1/2026

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|---|---------|-------------------------|---|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |         | UNIT 17 PLAN            | DATE<br>12/01/26  | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
| SCALE 1:100<br>0 1000 2000  | REVISED | DRAWING NO.<br>58 OF 79 | Certified G. Tilley Accreditation No. CC620H<br>© copyright 2024 5024 |   |



NORTH ELEVATION



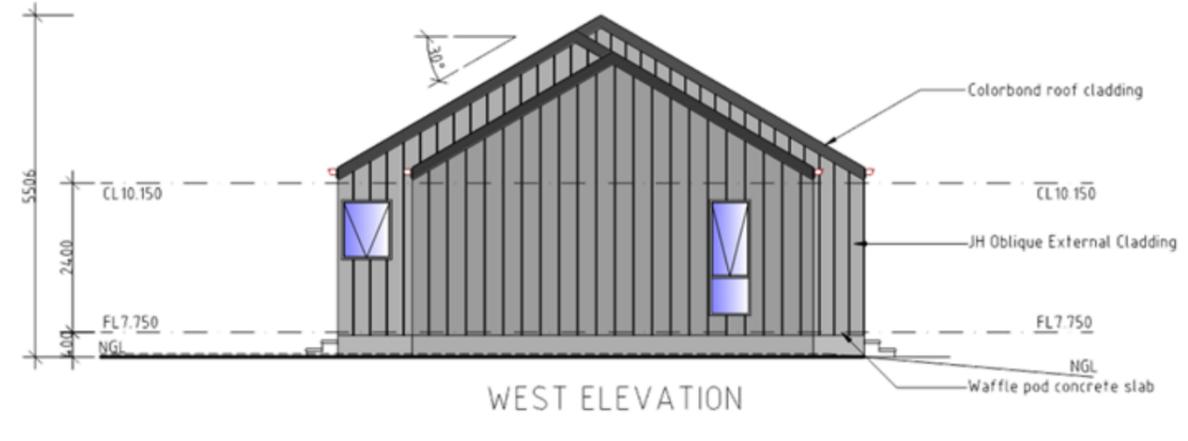
SOUTH ELEVATION

**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

Public



EAST ELEVATION

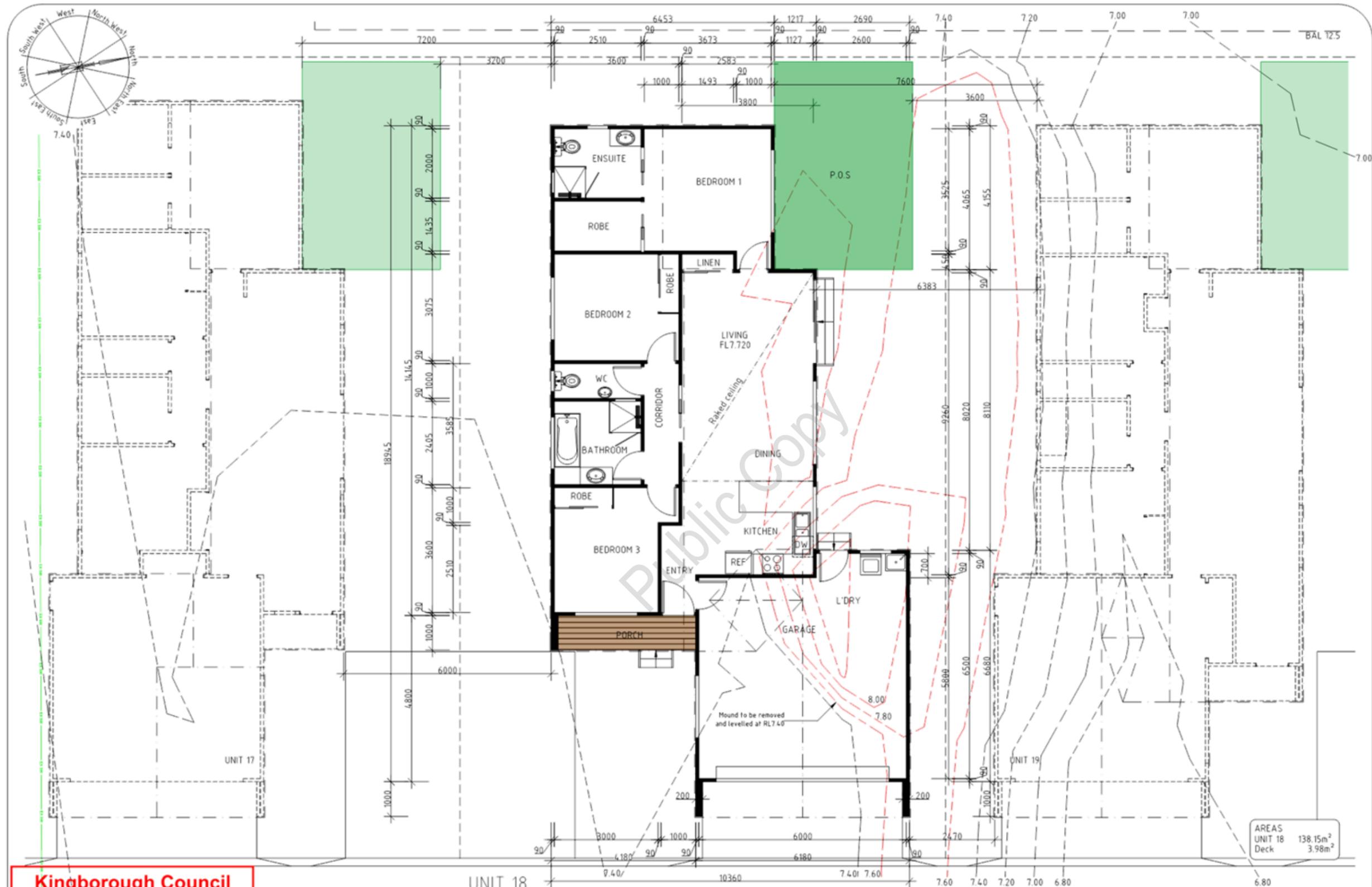


WEST ELEVATION

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UNIT 17

|   |                            |                  |   |
|---|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 17 ELEVATIONS         | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
|   | SCALE 1:100<br>0 1000 2000 | REVISED          | DRAWING NO.<br>59 OF 79   |



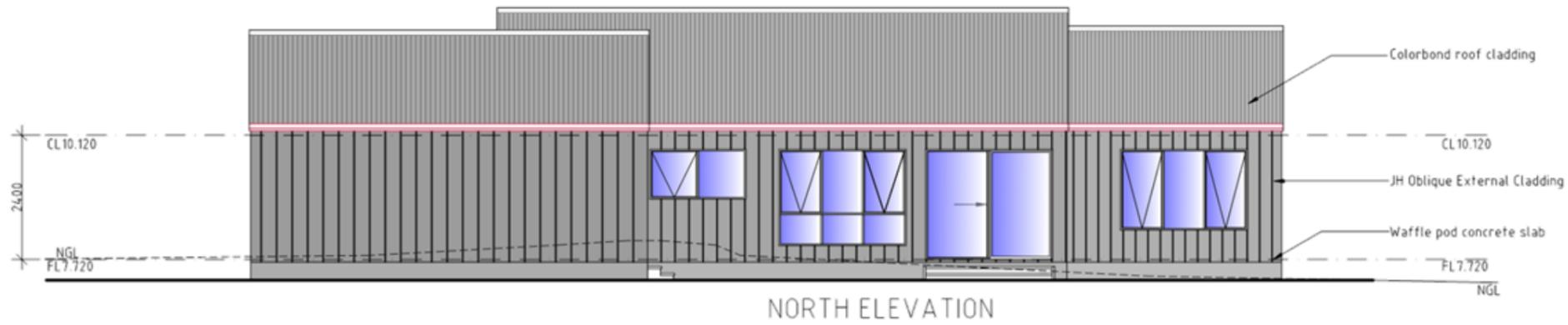
AREAS  
UNIT 18 138.15m<sup>2</sup>  
Deck 3.98m<sup>2</sup>

**Kingborough Council**  
Development Application: DA-2025-371  
Plan Reference No: P3  
Date Received: 12/1/2026  
Date placed on Public Exhibition: 17/1/2026

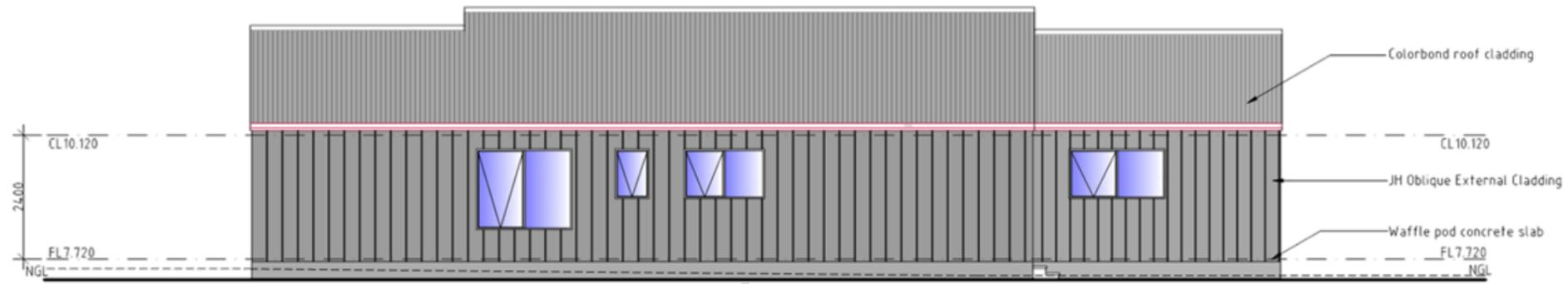
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|   |         |                         |  |   |
|---|---------|-------------------------|--|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |         | UNIT 18 PLAN            | DATE<br>12/01/26   | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
| SCALE 1:100<br>0 1000 2000  | AMENDED | DRAWING NO.<br>60 OF 79 | Certified G. Tilley Accreditation No. CC620H<br>© copyright 2024 5/024 |   |

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NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



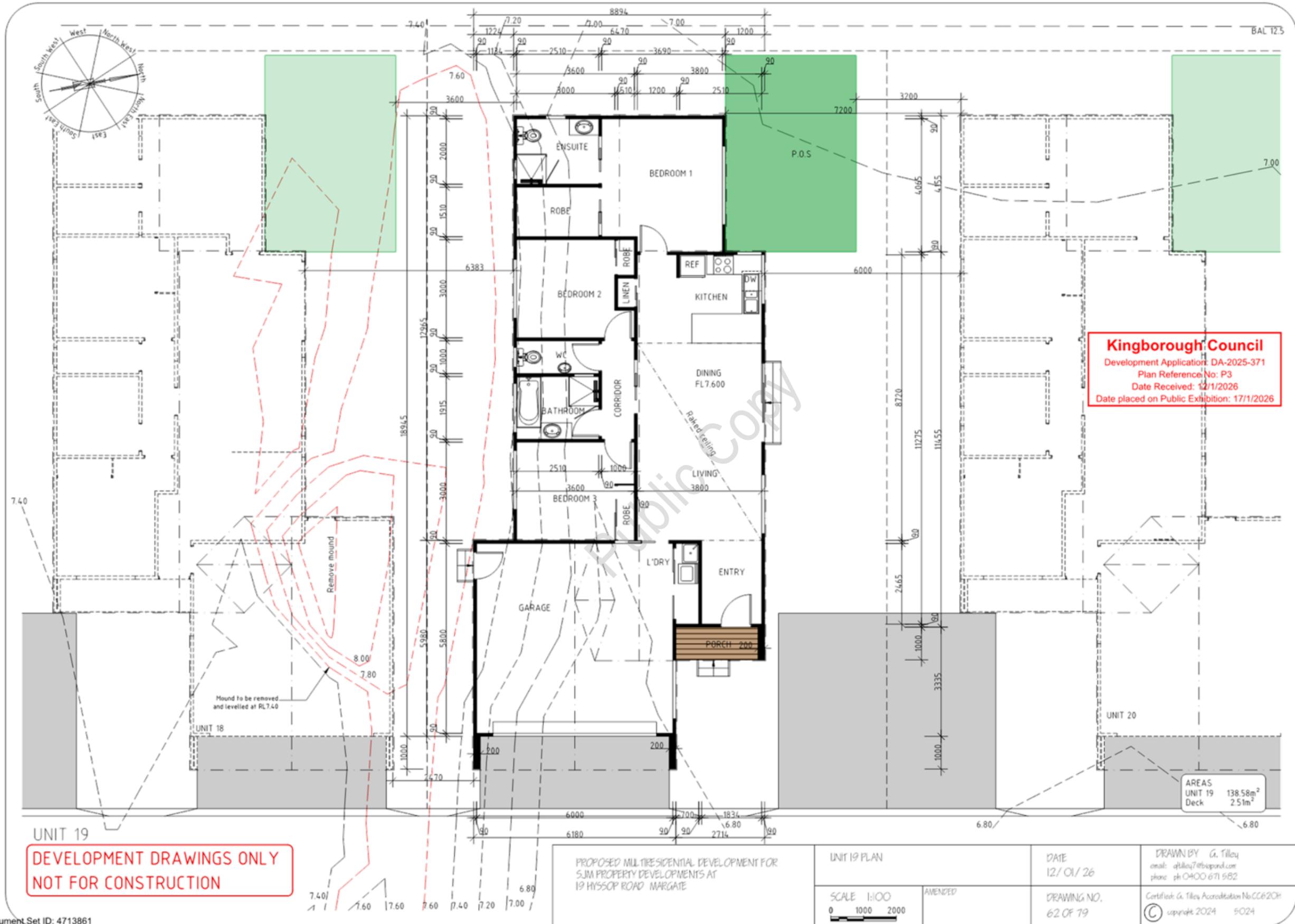
WEST ELEVATION

**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
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UNIT 18  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |   |
|----------------------------|---------|-------------------------|---|
| UNIT 18 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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**UNIT 19**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |  |
|----------------------------|---------|-------------------------|--|
| UNIT 19 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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UNIT 20 PLAN  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

UNIT 20 PLAN  
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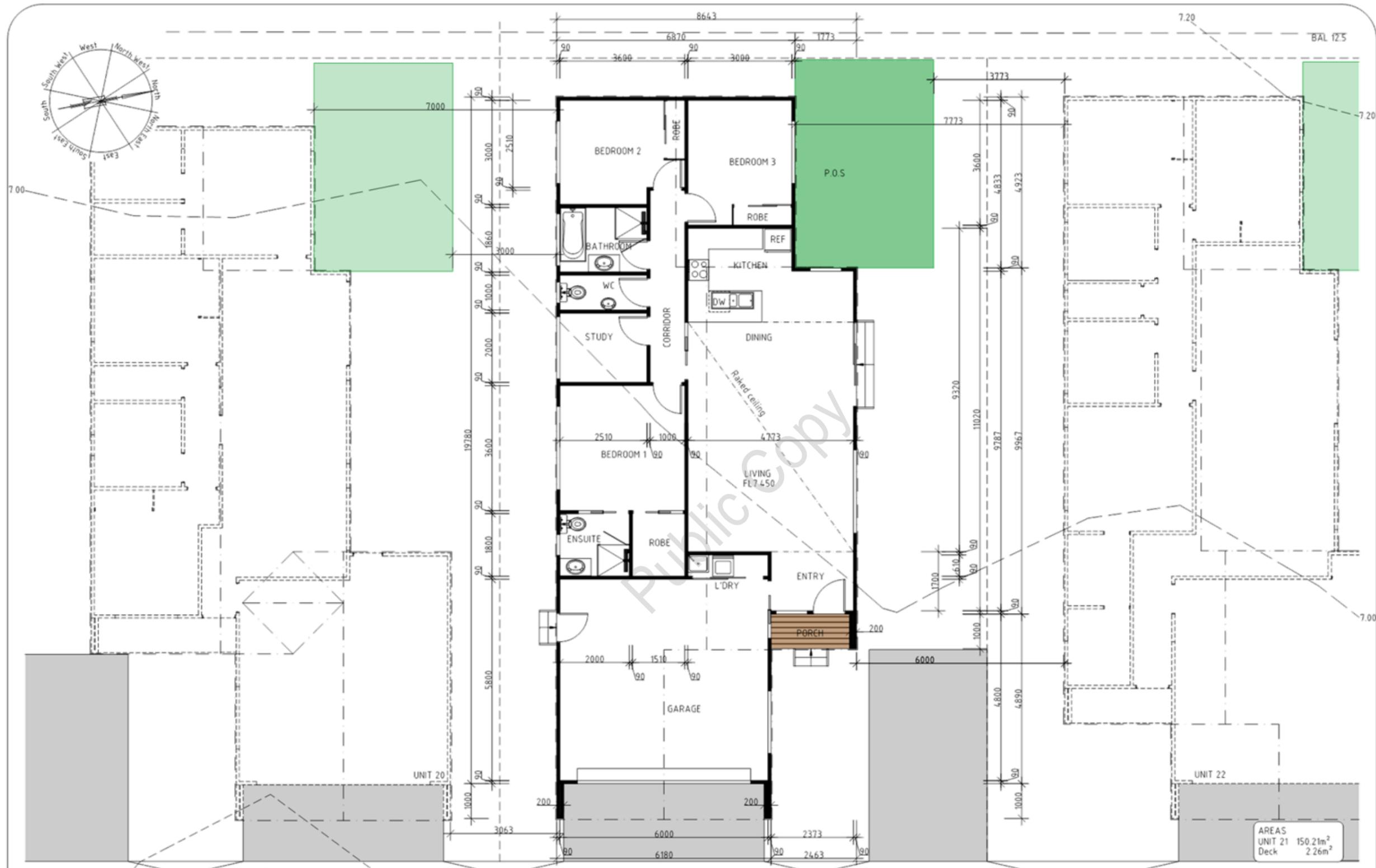
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 Plan Reference No: P3  
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**UNIT 20**

|   |                            |                  |   |
|---|----------------------------|------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJA PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | UNIT 20 ELEVATIONS         | DATE<br>12/01/26 | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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AREAS  
UNIT 21 150.21m<sup>2</sup>  
Deck 2.26m<sup>2</sup>

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**UNIT 21**  
PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
SJM PROPERTY DEVELOPMENTS AT  
19 HISSOP ROAD MARGATE

UNIT 21 PLAN  
SCALE 1:100  
0 1000 2000

DATE  
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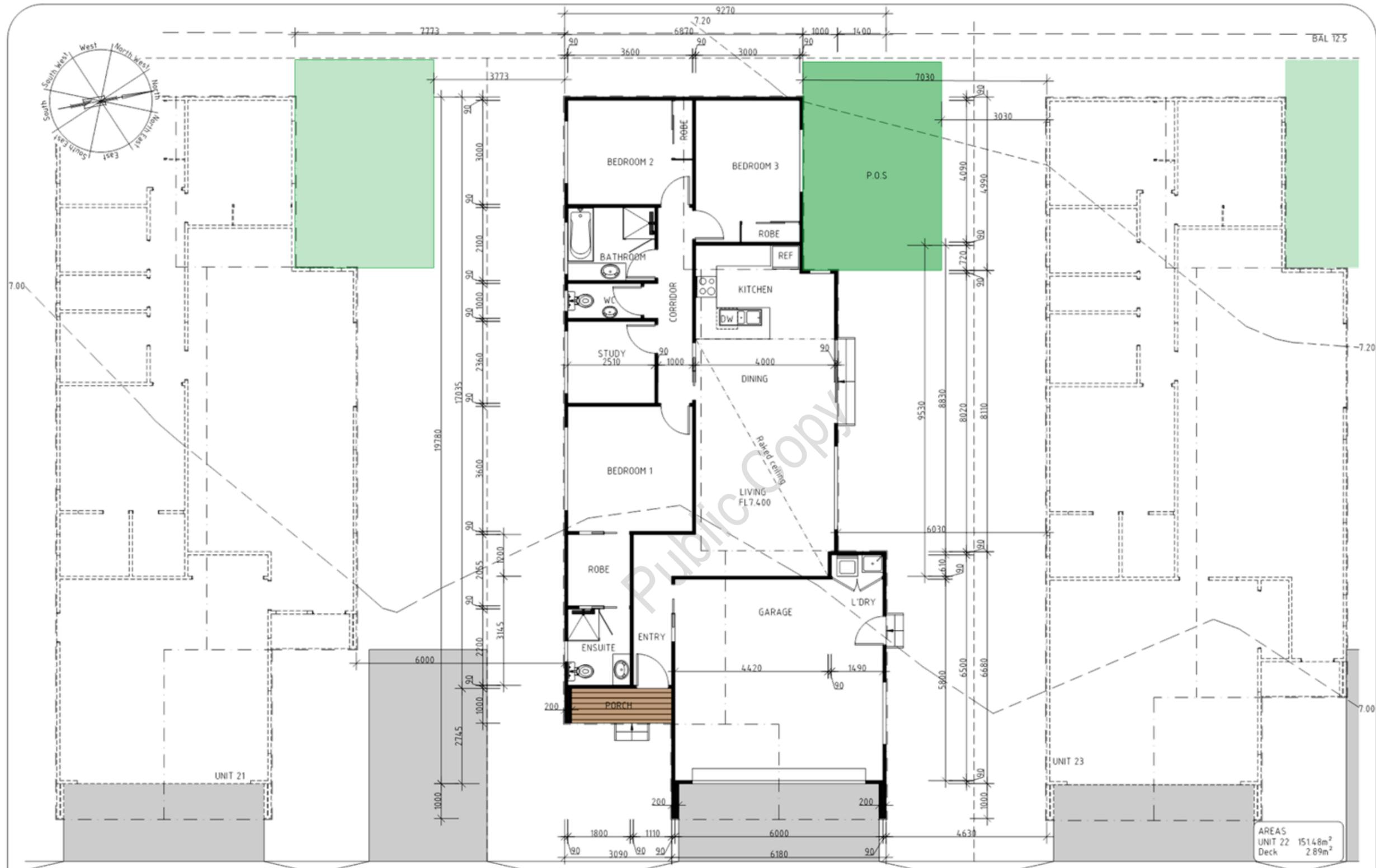
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**UNIT 21**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HYSSOP ROAD MARGATE

|                            |         |                         |   |
|----------------------------|---------|-------------------------|---|
| UNIT 21 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
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**UNIT 22**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

**UNIT 22 PLAN**  
 SCALE 1:100  
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**DATE**  
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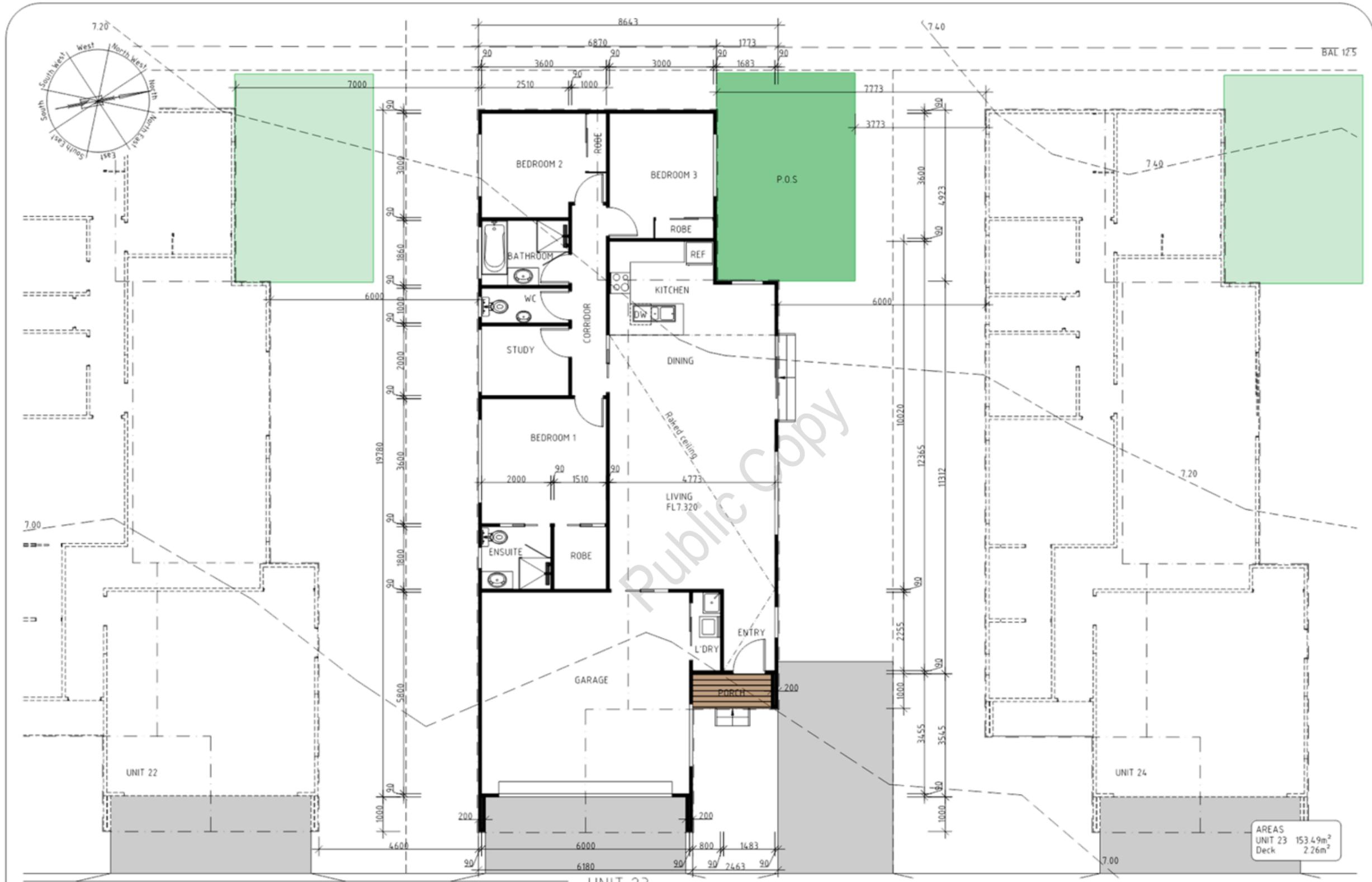


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 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026

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|---|--|----------------------------|---------|-------------------------|---|
| UNIT 22<br>PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>S/JM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE |  | UNIT 22 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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AREAS  
UNIT 23 153.49m<sup>2</sup>  
Deck 2.26m<sup>2</sup>

**Kingborough Council**  
Development Application: DA-2025-371  
Plan Reference No: P3  
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Date placed on Public Exhibition: 17/1/2026

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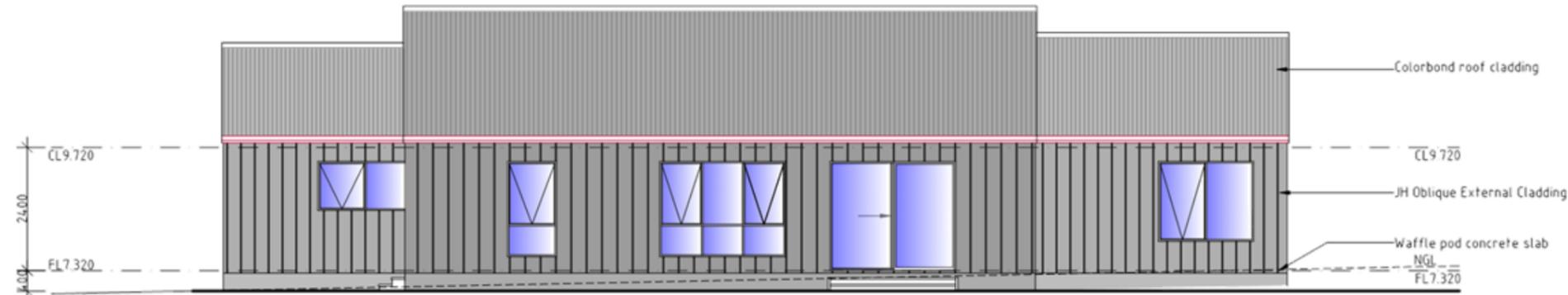
UNIT 23  
PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
SJM PROPERTY DEVELOPMENTS AT  
19 HISSOP ROAD MARGATE

UNIT 23 PLAN  
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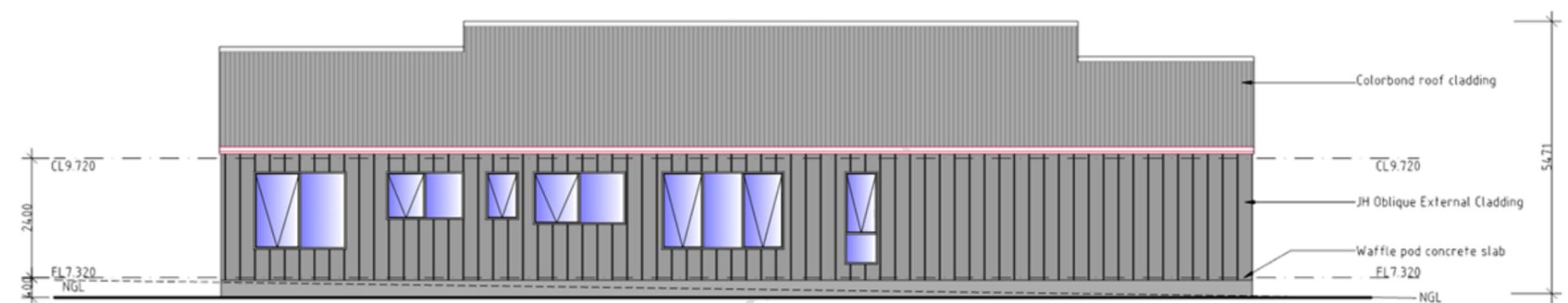
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NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION

**Kingborough Council**  
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**UNIT 23**

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

**UNIT 23 ELEVATIONS**

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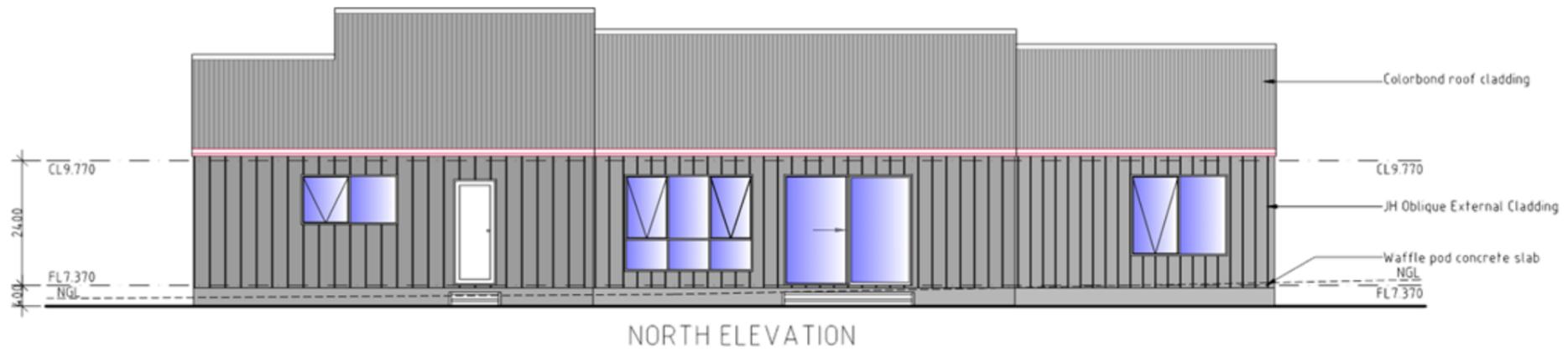
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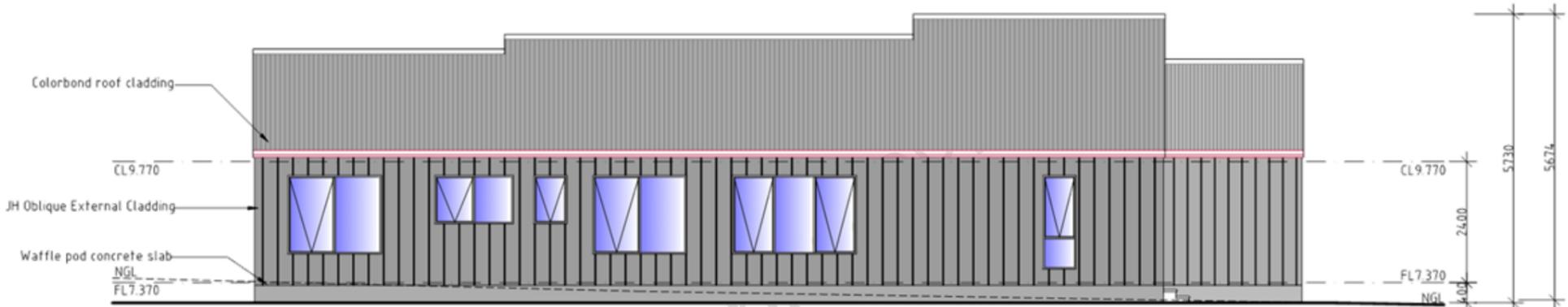
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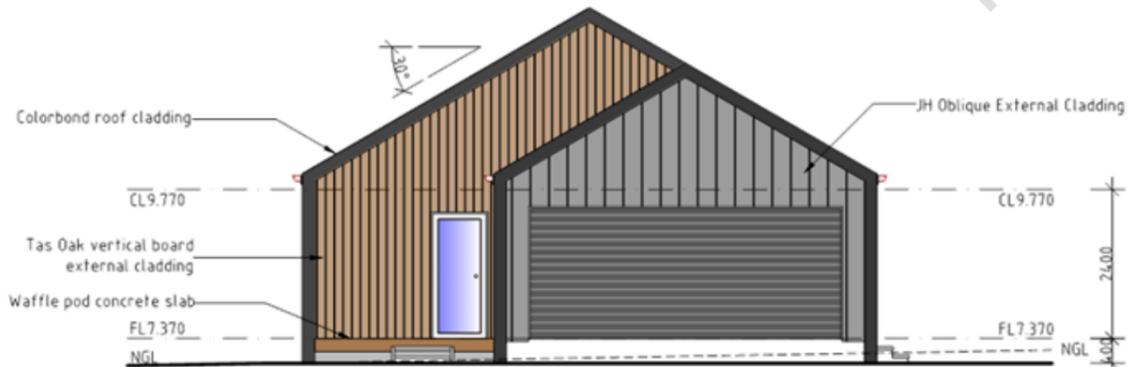




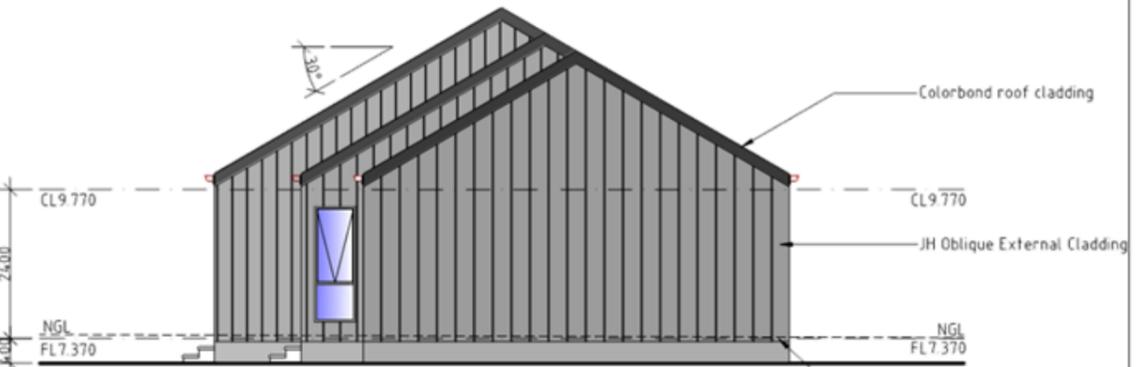
NORTH ELEVATION



SOUTH ELEVATION



EAST ELEVATION



WEST ELEVATION

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 Plan Reference No: P3  
 Date Received: 12/1/2026  
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**UNIT 24**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 S/JM PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                            |         |                         |  |
|----------------------------|---------|-------------------------|--|
| UNIT 24 ELEVATIONS         |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 982 |
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 Plan Reference No: P3  
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 Date placed on Public Exhibition: 17/1/2026

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**UNIT 25**

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HYSLOP ROAD MARGATE

**UNIT 25 ELEVATIONS**

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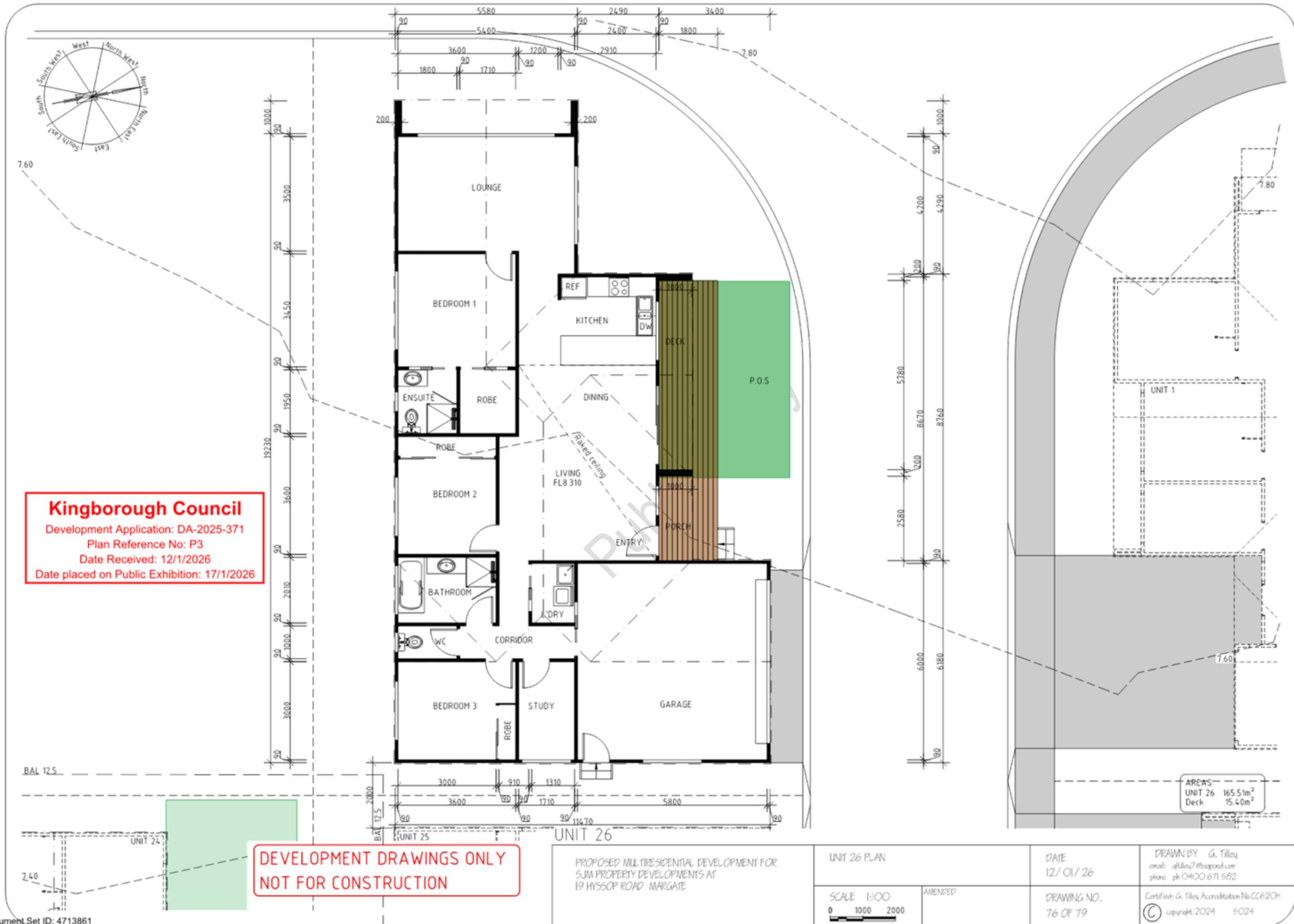
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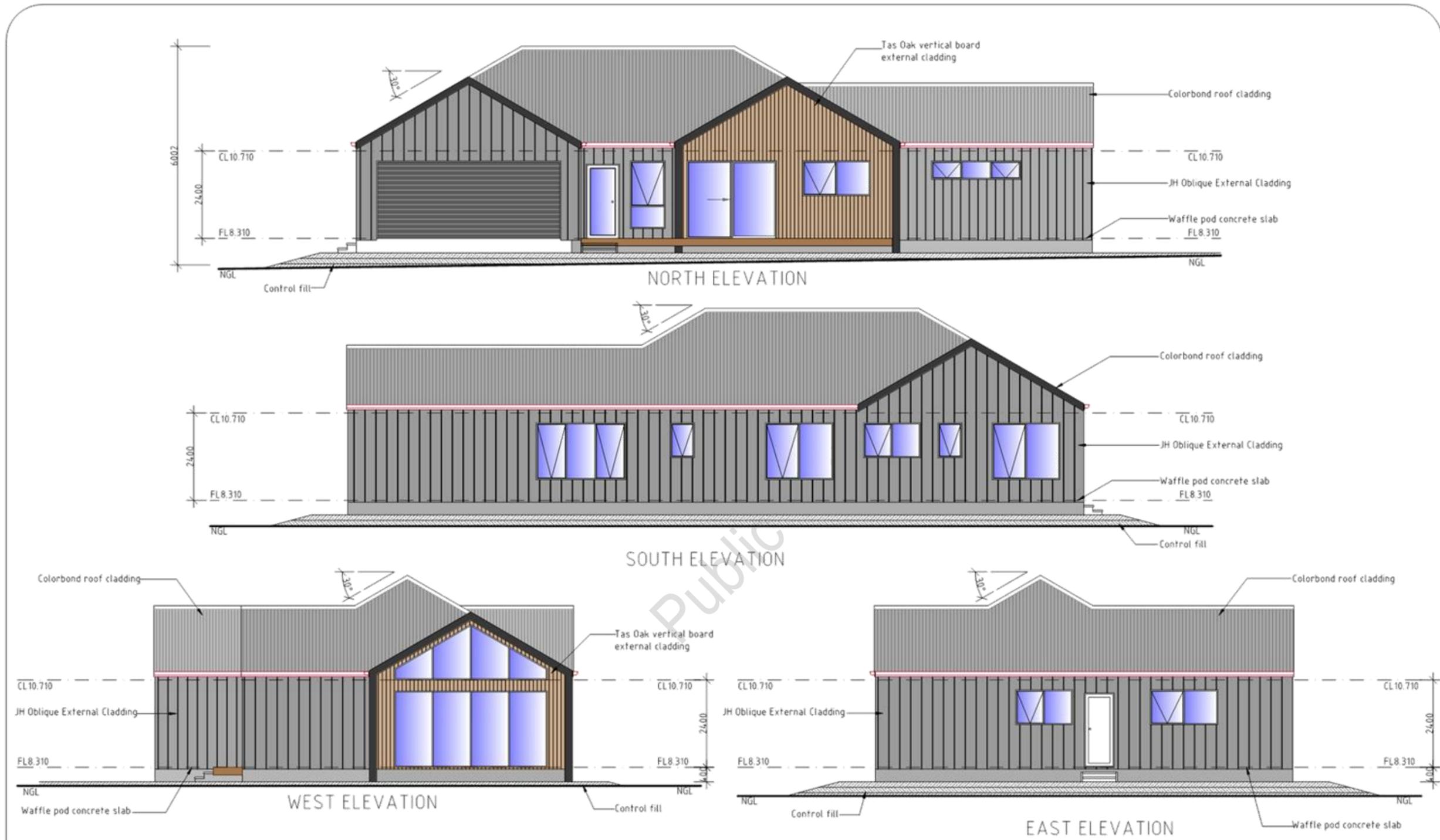
DATE  
 12/01/26

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 Plan Reference No: P3  
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**UNIT 26**  
 PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

|                               |         |                         |   |
|-------------------------------|---------|-------------------------|---|
| UNIT 26 ELEVATIONS            |         | DATE<br>12/01/26        | DRAWN BY<br>G. Tilley<br>email: gtilley@bigpond.com<br>phone: ph 0400 671 982 |
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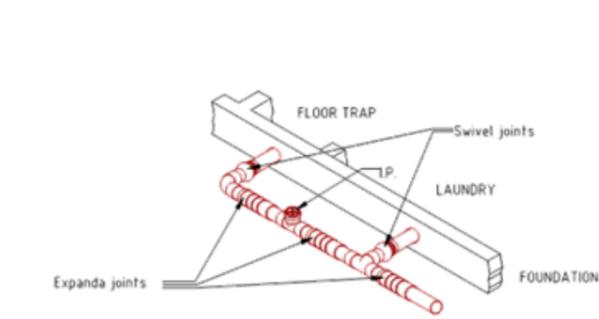
**WET AREAS TO COMPLY WITH NCC VOL. 2 PART H4D2, ABCB HOUSING PROVISIONS PART 10.2 AND AS 3740**

**WATERPROOFING OF ENCLOSED & UNENCLOSED SHOWERS:**  
**FLOOR:** Waterproof entire floor if no preformed shower base provided  
**WALLS:** Waterproof to not less than 1800mm above the floor substrate  
**WALL JUNCTIONS AND JOINTS:** Waterproof internal and external corners and horizontal joints within a height of 1800mm above the floor level with not less than 40mm width either side of the junction  
**WALL/FLOOR JUNCTIONS:** Waterproof internal and external corners and joints  
**PENETRATIONS:** Waterproof all penetrations  
**AREAS OUTSIDE THE SHOWER ON CONCRETE SLAB OR FC FLOORING:**  
**FLOORS:** Entire floor to be water resistant  
**WALLS/FLOOR JUNCTIONS:** Waterproof all wall/floor junctions and where a flashing is used, the horizontal leg must be not less than 40mm  
**AREAS OUTSIDE THE SHOWER ON TIMBER FLOOR:**  
**FLOORS:** Waterproof entire floor  
**WALL/FLOOR JUNCTIONS:** Waterproof all wall/floor junctions and where a flashing is used, the horizontal leg must be not less than 40mm  
**AREAS ADJACENT TO NON-FREESTANDING BATHS AND SPAS (without showers):**  
**FLOOR:** Water resistant to entire floor on concrete or FC flooring; or Waterproof to entire floor on timber floor  
**WALLS:** Water resistant walls to a height of not less than 150mm above the vessels, for the full extent, where the vessel is within 75mm of a wall.  
**WALL JUNCTIONS AND JOINTS:** Water resistant within 150mm above the vessel for the extent of the vessel to a width of 40mm either side of the junction  
**WALL/FLOOR JUNCTIONS:** Waterproof for the extent of the vessel  
**AREAS ADJACENT TO INSERTED BATHS AND SPAS (without showers):**  
**FLOOR:** Water resistant to entire floor on concrete or FC flooring; or Waterproof to entire floor on timber floor  
**HORIZONTAL SURFACES:** Waterproof shelf adjoining bath or spa and include a waterstop under the vessel lip  
**WALLS:** Waterproof walls to not less than 150mm above the lip of the vessel  
**WALL JUNCTIONS AND JOINTS:** Waterproof junctions within 150mm of vessel to a width of 40mm either side of the junction  
**WALL/FLOOR JUNCTIONS:** Waterproof wall/floor junctions 25mm above finished floor level  
**PENETRATIONS:** Waterproof penetrations where they occur in horizontal surfaces, seal penetrations where they occur in vertical surfaces  
**OTHER AREAS (LAUNDRIES AND WCs):**  
**FLOOR:** Water resistant floor to entire room  
**WALLS:** Water resistant wall to a height of not less than 150mm above the vessel for the extent of the vessel, where the vessel is within 75mm of wall  
**WALL JUNCTIONS AND JOINTS:** Waterproof junctions where a vessel is fixed to a wall  
**WALL/FLOOR JUNCTIONS:** Water resistant wall/floor junctions with horizontal leg not less than 40mm where flashing used  
**PENETRATIONS:** Waterproof penetrations where they occur in surfaces required to be waterproof or water resistant.  
**WATERPROOFING SYSTEMS:**  
 Waterproofing systems to be in accordance with ABCB Housing Provisions Part 10.2.6.  
**FALLS TO WET AREA FLOORS:**  
 Where a floor waste is installed the continuous fall of a floor plane to the waste must be no less than 180 and no more than 150.  
**STEPDOWN SHOWERS:**  
 Where stepdown showers are used, the shower area must be stepped down a minimum of 25mm below the finished floor level outside the shower. Refer to ABCB Housing Provisions Part 10.2.15 & relevant figures for details.  
**HOB CONSTRUCTION:**  
 Shower hobs are to be constructed in accordance with ABCB Housing Provisions Part 10.2.16.  
**ENCLOSED SHOWERS WITH LEVEL THRESHOLD:**  
 Enclosed showers with a level threshold must be provided with a waterstop in accordance with ABCB Housing Provisions Part 10.2.17 & relevant figures.

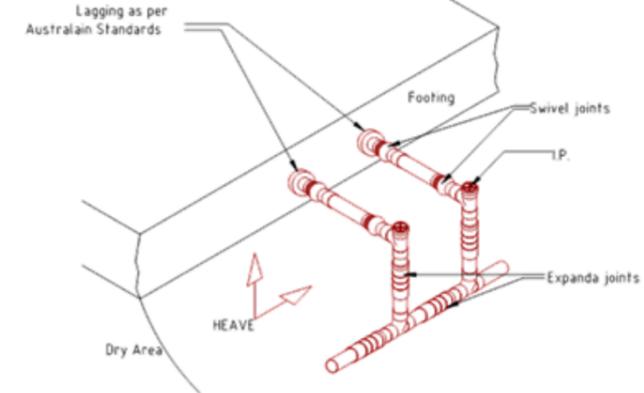
**UNENCLOSED SHOWERS:**  
 Unenclosed showers are to have a waterstop min. 1500mm from the shower rose with the vertical leg finishing flush with the top surface of the floor  
 Waterproof all all joins and junctions. Waterproof entire bathroom floor where unenclosed showers are installed. Refer to ABCB Housing Provisions Part 10.2.18 & relevant figures for details.  
**PENETRATIONS:**  
 All penetrations in showers and wet areas must be waterproofed in accordance with ABCB Housing Provisions part 10.2.23.  
**FLASHINGS/JUNCTIONS:**  
 All flashings and junctions in wet areas to be installed in accordance with ABCB Housing Provisions Part 10.2.24 & relevant figures.  
**SHOWER SCREENS:**  
 1900H Semi-frameless shower screens to comply with ABCB Housing Provisions Table 8.4.6 & AS 1288:2021. Minimum 6mm toughened safety organic coated glass, labelled to comply with industry standards. Install shower screens in accordance with ABCB Housing Provisions Part 10.2.32.  
**HYDRAULIC NOTES:**  
 1. All plumbing shall be in accordance with the Tasmanian Plumbing Regulations, AS 3500 and to the local authority approval.  
 2. The location of the existing services where shown are approximate only and shall be confirmed on site where possible. Determine location of existing power, Telstra, water and drainage services prior to commencing new work.  
 3. Conceal all pipework in ceiling space, ducts, cavities, wall chases, cupboards etc. unless otherwise approved.  
 4. Refer to designers drawings and fixture and equipment technical specifications for pipework connections.  
 5. Make good all disturbed surfaces to match existing.  
 6. Remove all excess soil and surplus materials from site.  
 7. All plumbing to be installed by a licensed plumber.

Install inspection openings at major bends for stormwater and all low points of downpipes.  
 All plumbing & drainage to be in accordance with local Council requirements. Provide surface drain to back of bulk excavation to drain leveled pad prior to commencing footing excavation.  
 Stormwater line (100mm uPVC)  
 Sewer line (100mm uPVC)  
**SERVICES**  
 The heated water system must be designed & installed with Part B2 of NCC Vol. 3 - Plumbing Code of Australia  
 Thermal insulation for heated water piping must:  
 a) be protected against the effects of weather and sunlight; and  
 b) be able to withstand the temperatures within the piping; and  
 c) use thermal insulation in accordance with AS/NZS 4859.1

Heated water piping that is not within a conditioned space must be thermally insulated as follows:  
 1. Internal piping:  
 a) All flow and return internal piping that is -  
 (i) within an unventilated wall spaces  
 (ii) within an internal floor between storeys; or  
 (iii) between ceiling and insulation and a ceiling  
 Must have a minimum R-value of 0.2 (ie. 9mm of closed cell polymer insulation)  
 2. Piping located within a ventilated wall space, an enclosed building subfloor or a roof space:  
 a) All flow and return piping  
 b) Cold water supply piping and Relief valve piping within 500mm of the connection to central water heating system  
 Must have a minimum R-value of 0.45 (ie. 19mm of closed cell polymer insulation)  
 3. Piping located outside the building or in an unenclosed building sub-floor or roof space:  
 a) All flow and return piping  
 b) Cold water supply piping and Relief valve piping within 500mm of the connection to central water heating system  
 Must have a minimum R-value of 0.6 (ie. 25mm of closed cell polymer insulation)  
 Piping within an insulated timber framed wall, such as that passing through a wall stud, is considered to comply with the above insulation requirements.



GUIDELINES FOR PVC-U DRAINAGE SYSTEM WITH EXPANSION AND SWIVEL JOINT LOCATIONS FOR REACTIVE SOILS



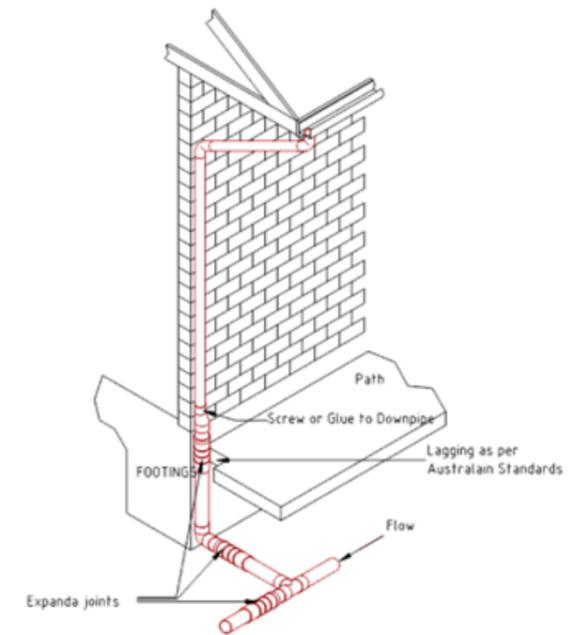
GUIDELINES FOR PVC-U DRAINAGE SYSTEM WITH EXPANSION AND SWIVEL JOINT LOCATIONS FOR REACTIVE SOILS

**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
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| Hot & Cold Water Nominal Diameters |           |
|------------------------------------|-----------|
| Branch off takes                   | Min. DN20 |
| Max. off take length 6m            | DN18      |
| Max. off take length 3m            | DN15      |
| Max. off take length 1m            | DN10      |

| Insulation Schedule      |                  |                                       |
|--------------------------|------------------|---------------------------------------|
| Heated water pipes       | Type             | Insulation                            |
|                          | Circulating Line | 32-40<br>25mm Rockwool with foil wrap |
| Branch Line              | Offtake          | 19mm Bradflex<br>13mm Bradflex        |
|                          | Size Range       | 20-25<br>18                           |
| Cold water pipes exposed | Type             | Insulation                            |
|                          | All              | >20<br>13mm Bradflex                  |
| Other cold water pipes   | Type             | Insulation                            |
|                          | All              | All<br>Not required                   |
| Size Range               | All              |                                       |

NOTE: Water pipes associated directly with plan equipment shall be insulated in accordance with the manufacturers instructions for a typical installation



GUIDELINES FOR PVC-U DRAINAGE SYSTEM WITH EXPANSION AND SWIVEL JOINT LOCATIONS FOR REACTIVE SOILS

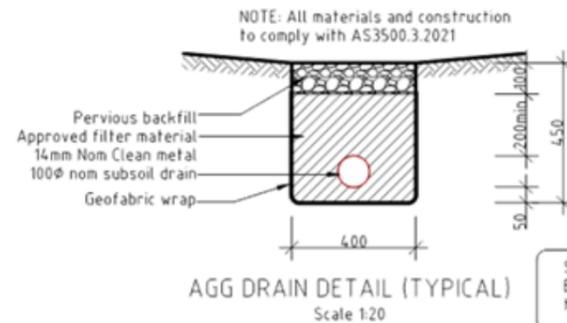
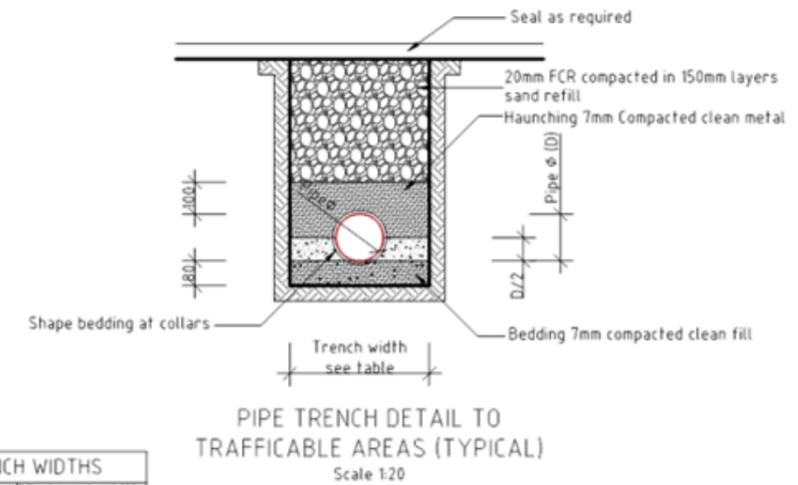
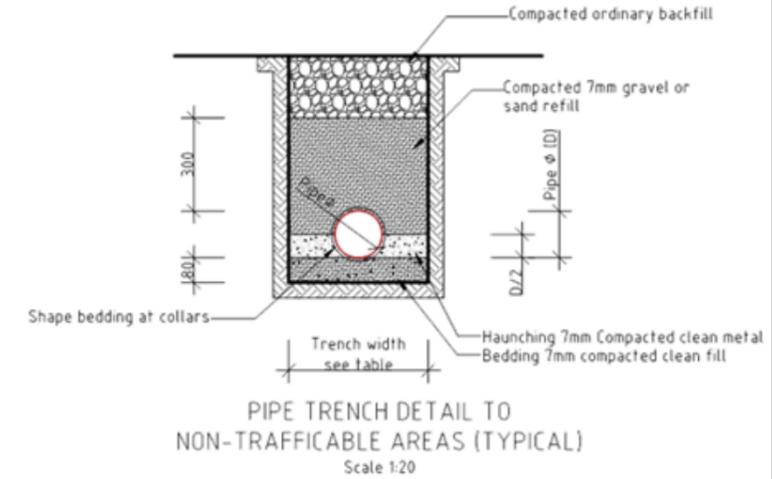
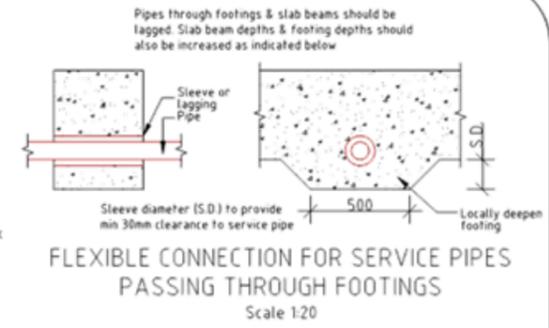
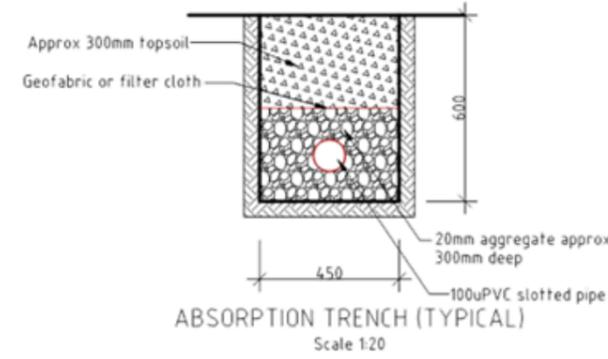
Surface drainage to conform with NCC Vol. 2 Part H2D2. NOTE: 50mm fall required over first 1m from building.

**IMPORTANT NOTICE FOR ATTENTION OF OWNER:**  
 The owners attention is drawn to the fact that foundations and associated drainage in all sites requires continuing maintenance to assist footing performance. Advice for foundation maintenance is contained in the CSRI0 Building Technology File 18 and it is the owners responsibility to maintain the site in accordance with that document.

**DEVELOPMENT DRAWINGS ONLY  
 NOT FOR CONSTRUCTION**

|   |                |         |                         |   |
|---|----------------|---------|-------------------------|---|
| PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR<br>SJM PROPERTY DEVELOPMENTS AT<br>19 HISSOP ROAD MARGATE | PLUMBING NOTES |         | DATE<br>12/01/2026      | DRAWN BY G. Tilley<br>email: gtilley7@bigpond.com<br>phone: ph 0400 671 582 |
|   | SCALE N/A      | REVISED | DRAWING NO.<br>78 OF 79 | Certified G. Tilley Accreditation No:CC620H<br>© copyright 2024 5:024       |

**Kingborough Council**  
 Development Application: DA-2025-371  
 Plan Reference No: P3  
 Date Received: 12/1/2026  
 Date placed on Public Exhibition: 17/1/2026



| TRENCH WIDTHS  |                   |
|----------------|-------------------|
| Pipe diameter  | Min trench width  |
| Less than 50mm | 250               |
| 75-100mm       | 450               |
| 150-300mm      | 600               |
| >300mm         | $\phi$ plus 300mm |

Surface drainage to conform with BCA Vol2 Part 3.12.2. NOTE: 50mm fall required over the first 1 meter from the building

**IMPORTANT NOTICE FOR ATTENTION OF OWNER.**  
 The owner's attention is drawn to the fact that foundations and associated drainage in all sites requires continuing maintenance to assist footing performance. Advice for foundation maintenance is contained in the CSIRO Building Technology File 18 and it is the owner's responsibility to maintain the site in accordance with the document.

**DEVELOPMENT DRAWINGS ONLY  
 NOT FOR CONSTRUCTION**

PROPOSED MULTIRESIDENTIAL DEVELOPMENT FOR  
 SJA PROPERTY DEVELOPMENTS AT  
 19 HISSOP ROAD MARGATE

PLUMBING DETAILS

DATE  
 12/01/2026

DRAWN BY G. Tilley  
 email: gtilley7@bigpond.com  
 phone: ph 0400 671 582

SCALE 1:20  
 0 200 400

AMENDED

DRAWING NO.  
 19 OF 19

Certified G. Tilley Accreditation No:CC620H  
 Copyright 2024 5:024

**19 Hyssop Rd - (76 Esplanade)  
Margate, Tas**

**SunTracker - Shadow Diagrams-REV2**

Scale 1:150

Units 13,14,15  
Private Open Space (POS)  
9am to 3pm - June 21

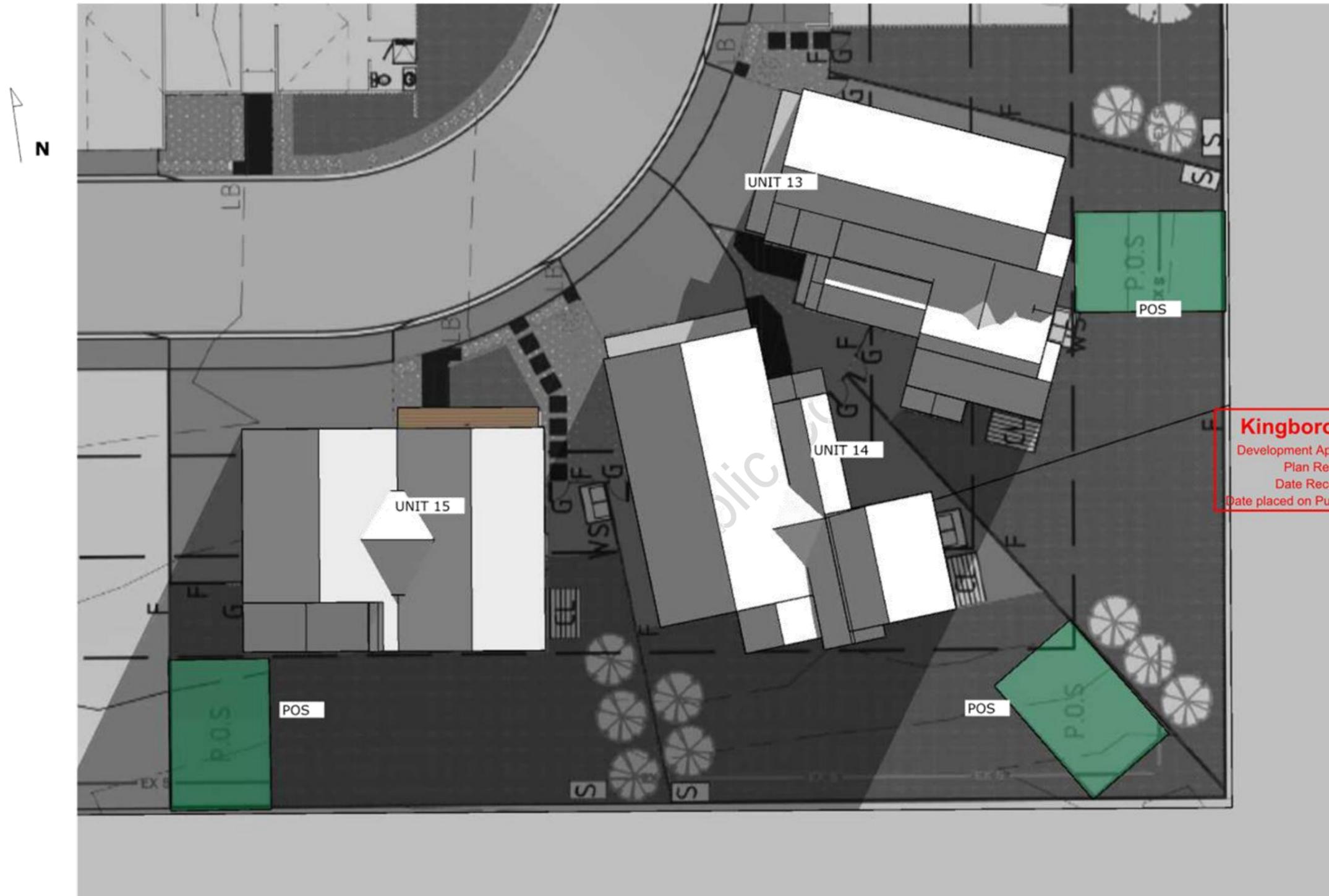
**Kingborough Council**  
Development Application: DA-2025-371  
Plan Reference No: P3  
Date Received: 12/1/2026  
Date placed on Public Exhibition: 17/1/2026

| HOURS<br>Area Name | 9:00   | 10:00  | 11:00  | 12:00 | 13:00 | 14:00 | 15:00 | Avg Sun% | Hours |
|--------------------|--------|--------|--------|-------|-------|-------|-------|----------|-------|
| UNIT13             | 100.0% | 100.0% | 100.0% | 99.7% | 83.5% | 62.7% | 37.6% | 83.4%    | 5.0h  |
| Unit 14            | 100.0% | 100.0% | 100.0% | 57.0% | 18.0% | 24.5% | 2.6%  | 57.4%    | 3.0h  |
| unit 15            | 0.0%   | 0.0%   | 7.2%   | 75.5% | 93.3% | 97.3% | 98.8% | 53.2%    | 3.0h  |

Reports by Sergei Nester www.viewbuild.com m: 0458 787 111

SunTracker - Shadow Diagrams

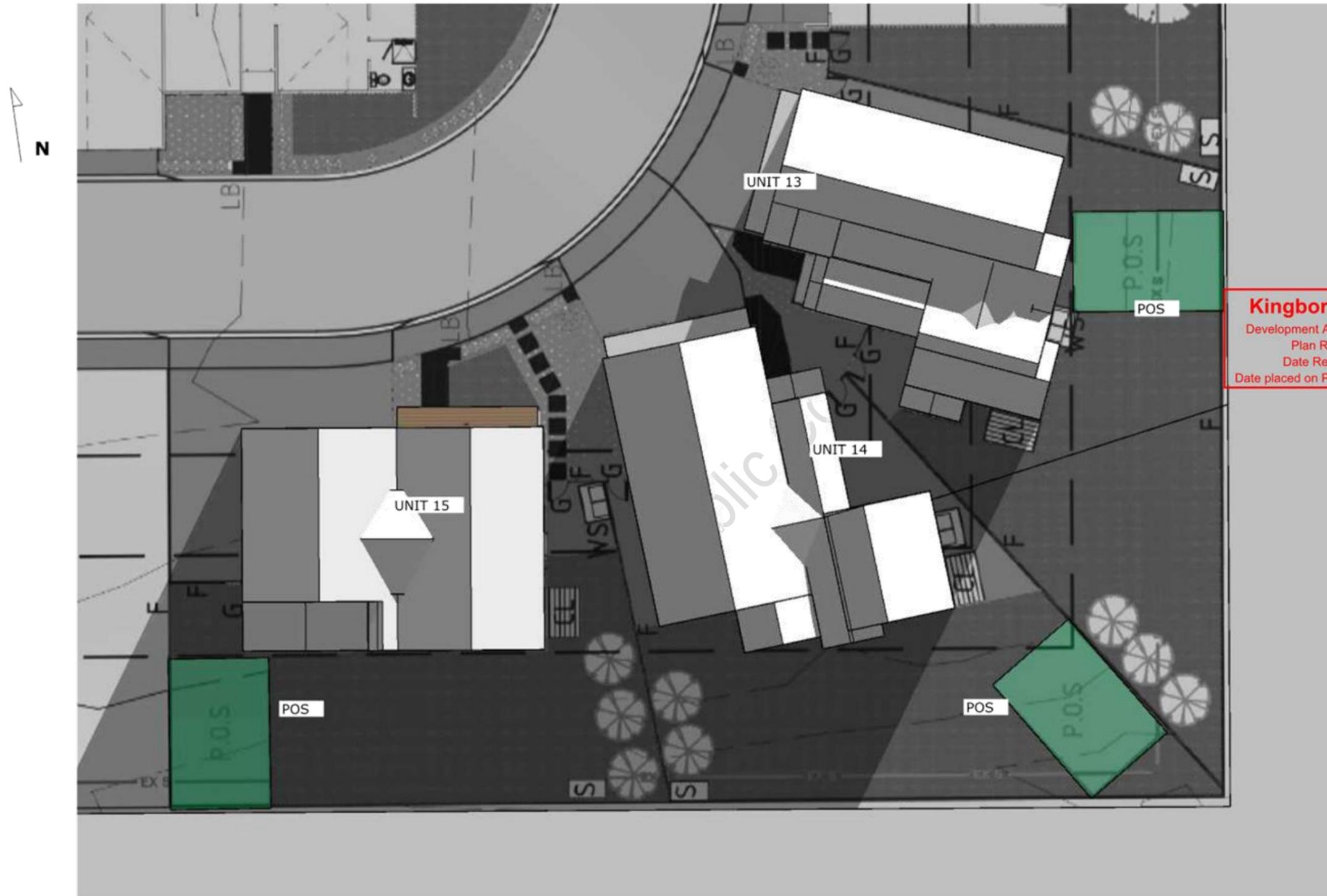
0900 - JUNE 21  
1:150



Shadowing by Sergei Nester www.viewbuild.com m: 0458 787 111

SunTracker - Shadow Diagrams

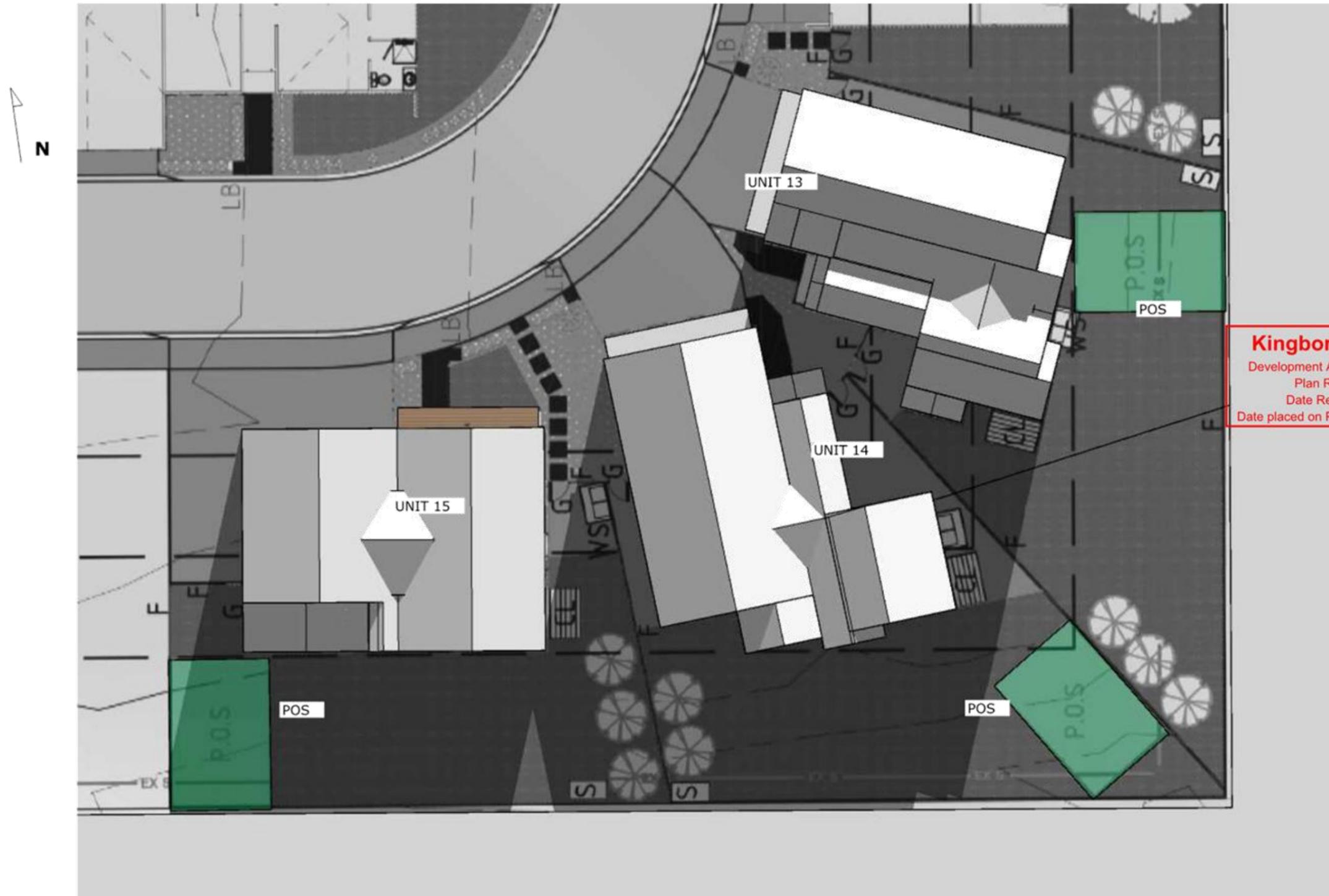
1000 - JUNE 21  
1:150



Shading by Sergei Nester www.viewbuild.com m: 0458 787 111

SunTracker - Shadow Diagrams

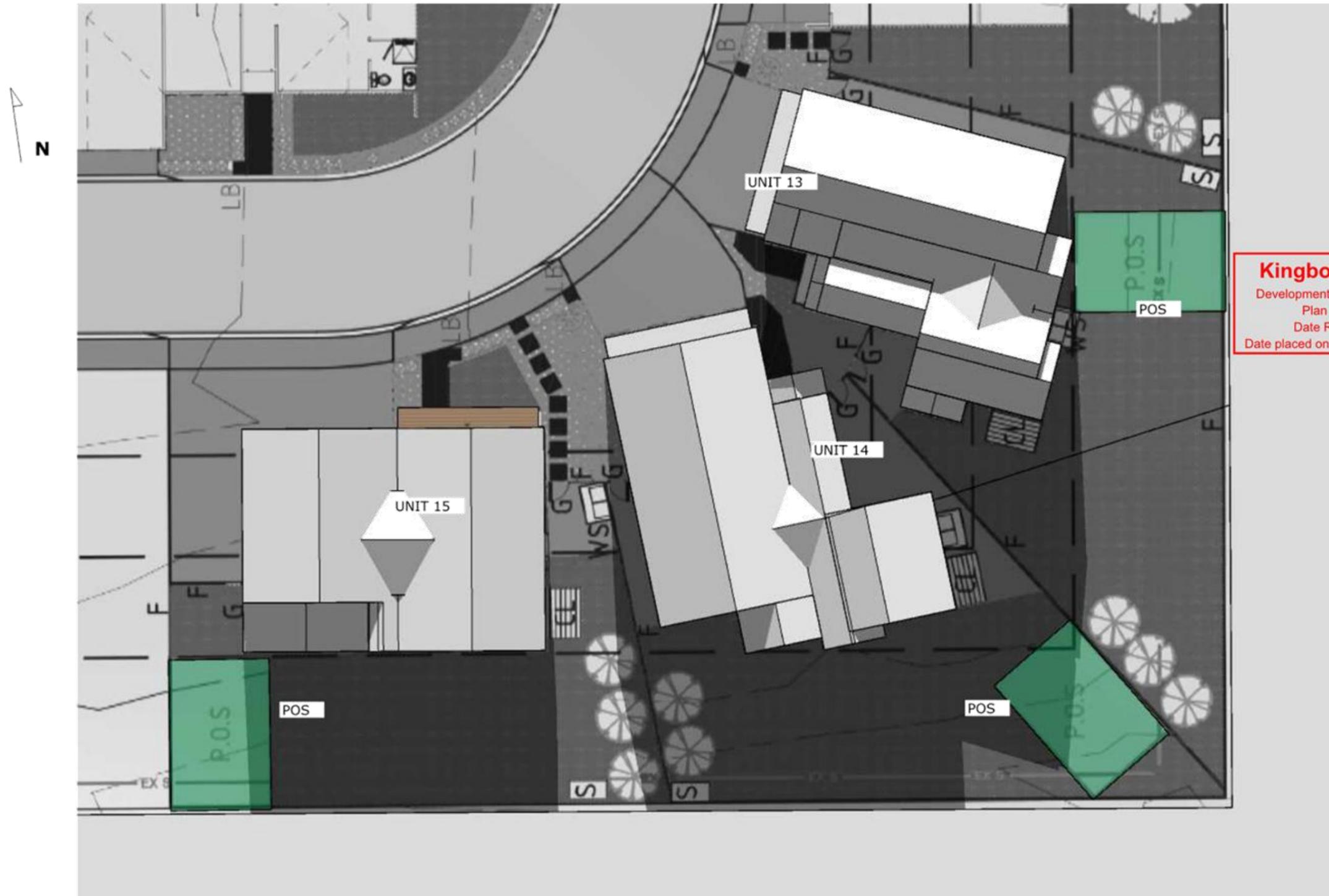
1100 - JUNE 21  
1:150



Shadowing by Sergei Nester www.viewbuild.com m: 0458 787 111

SunTracker - Shadow Diagrams

1200 - JUNE 21  
1:150

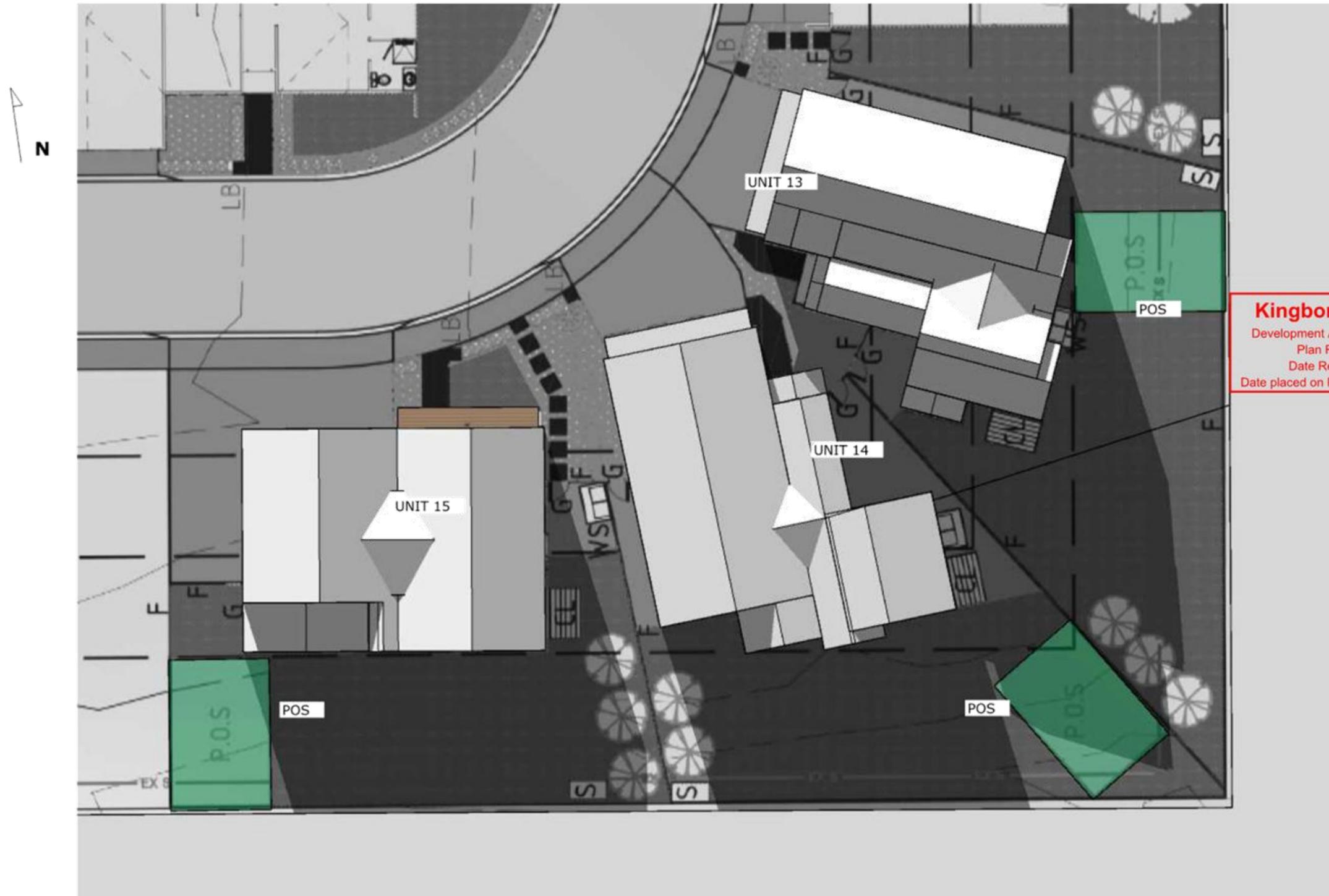


**Kingborough Council**  
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Shadowing by Sergei Nester www.viewbuild.com m: 0458 787 111

SunTracker - Shadow Diagrams

1300 - JUNE 21  
1:150

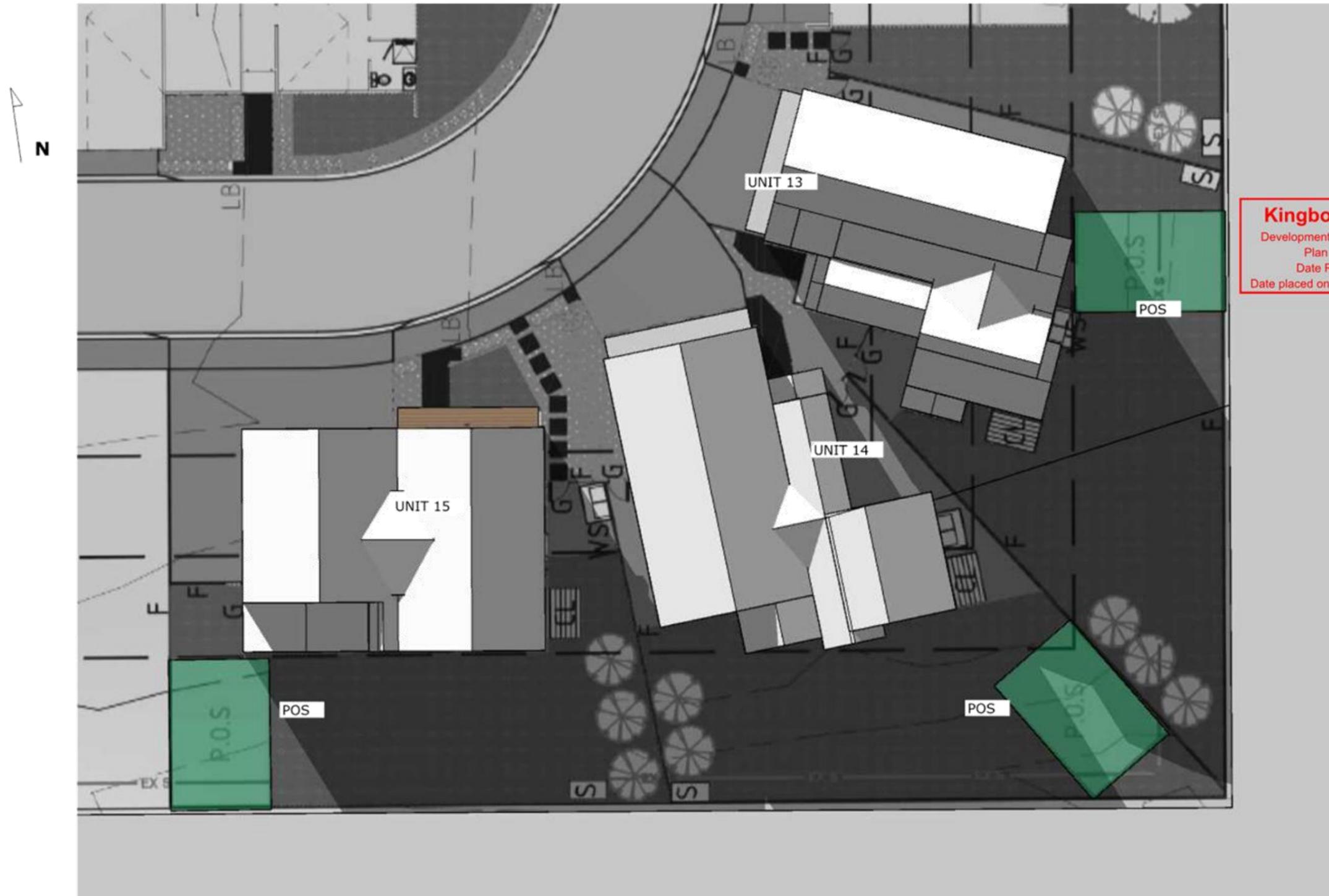


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SunTracker - Shadow Diagrams

1400 - JUNE 21  
1:150

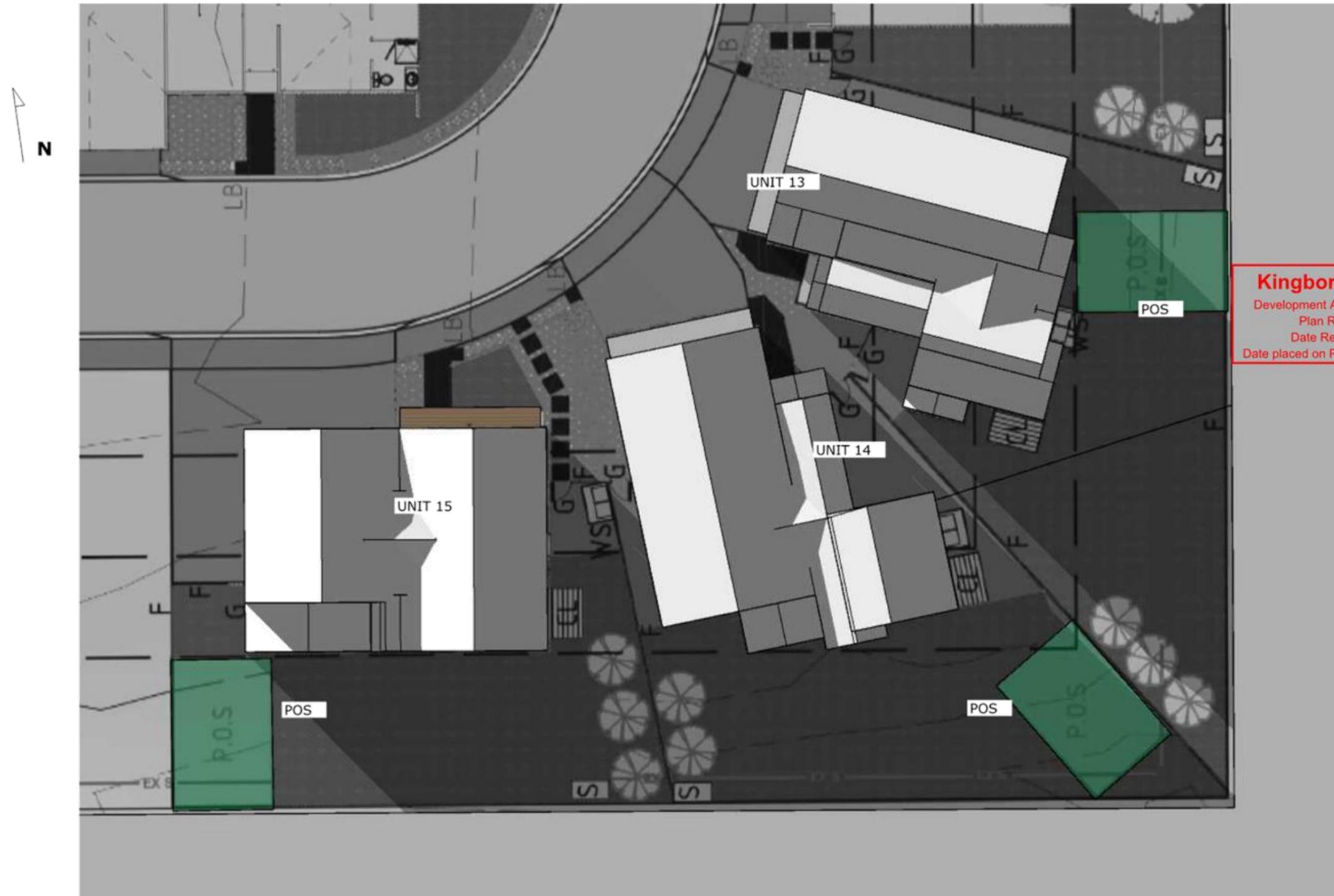


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SunTracker - Shadow Diagrams

1500 - JUNE 21  
1:150



**Kingborough Council**  
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Shadowing by Sergei Nester www.viewbuild.com m: 0458 787 111



## Submission to Planning Authority Notice

### Application details

|                             |                     |
|-----------------------------|---------------------|
| Council Planning Permit No. | DA-2025-371         |
| Council notice date         | 6/11/2025           |
| TasWater Reference No.      | TWDA 2025/01349-KIN |
| Date of response            | 09/12/2025          |
| TasWater Contact            | Phil Papps          |
| Phone No.                   | 0474 931 272        |

### Response issued to

|                            |                                  |
|----------------------------|----------------------------------|
| Council name               | KINGBOROUGH COUNCIL              |
| Contact details            | kc@kingborough.tas.gov.au        |
| Development details        |                                  |
| Address                    | 19 HYSSOP RD, MARGATE            |
| Property ID (PID)          | 9016395                          |
| Description of development | Multiple Dwellings x 26 (Staged) |

### Schedule of drawings/documents

| Prepared by             | Drawing/document No.     | Revision No. | Issue date |
|-------------------------|--------------------------|--------------|------------|
| AllUrbanPlanning        | Planning Report          | V1           | 21/10/2025 |
| G. Tilley               | Plans / 01 - 78          | --           | 09/10/2025 |
| GE Consulting Engineers | Sewer & Water Plan / C11 | A            | 09/10/2025 |

### Conditions

Pursuant to the *Water and Sewerage Industry Act 2008 (TAS)* Section 56P(1) TasWater imposes the following conditions on the permit for this application:

#### CONNECTIONS, METERING & BACKFLOW

1. A suitably sized water supply with metered connection(s) and sewerage system and connection(s) to the development must be designed and constructed to TasWater's satisfaction and be in accordance with any other conditions in this permit.
2. Any removal/supply and installation of water meters and/or the removal of redundant and/or installation of new and modified property service connections must be carried out by TasWater at the developer's cost.
3. Prior to commencing construction/use of the development, any water connection utilised for construction/use of the development must have a backflow prevention device and water meter installed, to the satisfaction of TasWater.

Tasmanian Water & Sewerage Corporation Pty Ltd  
 GPO Box 1393 Hobart, TAS 7001  
[development@taswater.com.au](mailto:development@taswater.com.au)  
 ABN: 47 162 220 653

Page 1 of 3

**INFRASTRUCTURE WORKS**

4. Prior to applying for a Certificate for Certifiable Work, the developer must physically locate all existing infrastructure to provide sufficient information for accurate design and physical works to be undertaken.
5. Plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) must, to the satisfaction of TasWater show, all existing, redundant and/or proposed property services and mains.
6. Prior to undertaking any works related to water and sewerage, physical markers must be in place that clearly identify where water and/or sewer connections are to be made in accordance with any approved plan to TasWater's satisfaction.
7. The developer must take all precautions to protect existing TasWater infrastructure. Any damage caused to existing TasWater infrastructure during the construction period must be promptly reported to TasWater and repaired by TasWater at the developer's cost.
8. Ground levels over the TasWater assets and/or easements must not be altered without the written approval of TasWater.

**TREES NEAR TASWATER INFRASTRUCTURE**

9. The application for Certificate(s) for Certifiable Work (Building and/or Plumbing) must include an amended landscaping plan that clearly identifies all trees with a canopy diameter of 2 metres or greater proposed within 2 metres laterally of TasWater's DN300 sewer trunk main.
  - a. Any tree located within 2 metres laterally of TasWater infrastructure must have its root system contained within a closed root barrier structure (e.g., tub, raised garden bed, or other suitable system) designed to prevent root intrusion into the pipe trench, to TasWater's satisfaction.
  - b. No new trees are permitted within 1 metre laterally of TasWater infrastructure.

**DEVELOPER CHARGES**

10. Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$34,788.60 to TasWater for water infrastructure for 19.8 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.
11. Prior to TasWater issuing a Certificate(s) for Certifiable Work (Building) and/or (Plumbing), the applicant or landowner as the case may be, must pay a developer charge totalling \$43,925.00 to TasWater for sewerage infrastructure for 25.0 additional Equivalent Tenements, indexed by the Consumer Price Index All groups (Hobart) from the date of this Submission to Planning Authority Notice until the date it is paid to TasWater.
12. In the event Council approves a staging plan, prior to TasWater issuing Certificate(s) for Certifiable Work (Building) and/or (Plumbing) for each stage, the developer must pay the developer charges commensurate with the number of Equivalent Tenements in each stage, as approved by Council.

**DEVELOPMENT ASSESSMENT FEES**

13. The applicant or landowner, must pay a development assessment fee of \$802.53 to TasWater, as approved by the Economic Regulator and the fee will be indexed, until the date paid to TasWater.



## Advice

### General

For information on TasWater development standards, please visit

<https://www.taswater.com.au/building-and-development/technical-standards>

For application forms please visit

<https://www.taswater.com.au/building-and-development/application-information/application-for-development-services-form>

### Important Notice Regarding Plumbing Plans and Associated Costs

The SPAN includes references to documents submitted as part of the application. These plans are acceptable for planning purposes only and are subject to further detailed assessment and review during the next stage of the development proposal.

TasWater's assessment staff will ensure that the design contains sufficient detail to assess compliance with relevant codes and regulations. Additionally, the plans must be clear enough for a TasWater contractor to carry out any water or sewerage-related work.

Depending on the nature of the project, your application may require Building and/or Plumbing permits or could be exempt from these requirements. Regardless, TasWater's assessment process and associated time are recoverable through an assessment fee.

Please be aware that your consultant may need to make revisions to their documentation to ensure the details are fit for construction. Any costs associated with updating these plans should be discussed directly with your consultant.

### Developer Charges

For information on Developer Charges please visit the following webpage -

<https://www.taswater.com.au/building-and-development/developer-charges>

### Water Submetering

As of July 1, 2022, TasWater's Sub-Metering Policy no longer permits TasWater sub-meters to be installed for new Strata developments. Please ensure plans submitted with the application for Certificate(s) for Certifiable Work (Building and/or Plumbing) reflect this. For clarity, TasWater does not object to private sub-metering arrangements. Further information is available on our website ([www.taswater.com.au](http://www.taswater.com.au)) within our Sub-Metering Policy and Water Metering Guidelines.

### Service Locations

Please note that the developer is responsible for arranging to locate the existing TasWater infrastructure and clearly showing it on the drawings. Existing TasWater infrastructure may be located by a surveyor and/or a private contractor engaged at the developers cost to locate the infrastructure.

### Water Servicing

The development is located within the Margate -Sandfly water pressure zone (KBW08) which has a pressure head of 102m. The proposed water property connection to the DN100 water main (Asset A416578) in Incana Rd will be at around 8m (AHD) resulting in a static head of approximately 94m without considering friction related head loss. The developer may wish to arrange with TasWater to undertake a fire hydrant test at the nearest hydrant (Asset A3509463) in Incana Rd and/or request water modelling be undertaken to confirm the required pressures and flows can be achieved to service internal private fire hydrants.

## Declaration

The drawings/documents and conditions stated above constitute TasWater's Submission to Planning Authority Notice.

## OPEN SESSION RESUMES

### 13 PETITIONS STILL BEING ACTIONED

---

There are no petitions still being actioned.

### 14 PETITIONS RECEIVED IN LAST PERIOD

---

At the time the Agenda was compiled no Petitions had been received.

### 15 OFFICERS REPORTS TO COUNCIL

---

#### 15.1 PETITION - KERBSIDE COLLECTION SERVICE, LESLIE VALE

**File Number:**

**Author:** Stuart Baldwin, Manager Kingborough Waste Services

**Authoriser:** Craig Mackey, Director Engineering Services

**Strategic Plan Reference**

Key Priority Area: 2. Growing together with well-planned spaces and infrastructure.

Strategic Outcome: 2.1 Services and assets which meet the current and future requirements of the community.

#### 1. PURPOSE

- 1.1 The purpose of this report is to consider a petition requesting Council extend the kerbside collection service to Leslie Vale.

#### 2. BACKGROUND

- 2.1 A petition has been received that requests Council expand the kerbside collection service to Leslie Vale. The petitioners statement reads:

*"The Kingborough Council investigated extending the kerbside service along Leslie Road to the intersection of Huon Road in September 2019.*

*The responses from the survey were that the majority of residents voted against extending the service past the current end point on Leslie Road at the recreation ground(Opposite McKenzies Road).*

*The Leslie Vale Community Group recently conducted a survey via the Leslie Vale Community Group Facebook page, to assess whether there was community support to extend the kerbside collection along Leslie Road to the intersection of the Huon Road, as well as conjoining roads. These included: McKenzies Road, Parkdale Drive, Bullock Road and Wolfes Road.*

*Results of the survey showed that 62% of those who voted were in favour of kerbside collection. There were additional votes for kerbside collection, but these extended past the intersection of Leslie Road & Huon Road and as such were not included in the final results.*

*Should an EOI be conducted which results in kerbside collection being extended to the intersection of Huon Road, this would result in future opportunities for kerbside collection to extend into Neika and Longley.*

*There has been correspondence with the Manager Kingborough Waste Services who has advised to undertake this formal petition process to bring this matter before the council.”*

### 3. STATUTORY REQUIREMENTS

- 3.1 The petition containing 52 signatories meets the requirements of Section 57 of the *Local Government Act 1993* and Council is therefore required to determine any action to be taken in respect to the subject of the petition.
- 3.2 There are no statutory requirements requiring Council to expand the kerbside waste and recycling service, however, it aligns with the Council adopted Waste Management Strategy, the State Government Waste Action Plan and the Council adopted position on climate change.

### 4. DISCUSSION

- 4.1 Council's usual approach with extending the kerbside collection service is to determine whether:
  - 4.1.1 the service integrates with our current serviced area i.e. can be delivered economically;
  - 4.1.2 the service contractor can service the area i.e. are the roads wide enough with suitable turning facilities to allow a collection service to be undertaken safely; and
  - 4.1.3 there is majority community support for the extension.
- 4.2 The proposed extension area is adjacent to a currently serviced area, and it is likely that the contractor would be able to provide a service to the area if required.
- 4.3 Investigations with the current kerbside collection contractor identified the following roads on the proposed extension were suitable for the collection vehicles and the kerbside collection service.
  - 4.3.1 Leslie Vale Road from the current end point of the collection route at the recreation ground through to the intersection Huon Road.
  - 4.3.2 Parkdale Drive from the intersection of Leslie Vale Road through to 102 Parkdale Drive.
  - 4.3.3 Bullock Drive.
  - 4.3.4 Wolfes Road from the intersection of Leslie Vale Road through to the intersection of Jindalee Drive.
- 4.4 Expression of interest letters were delivered to all affected residents on the proposed collection routes detailing the costs involved requesting their feedback on the extension of the service.
- 4.5 The replies from the expression of interest mail out were as follows:
  - 4.5.1 **Leslie Vale Road from the current end point of the collection route at the recreation ground through to the intersection Huon Road.**

Responses from the expression of interest letters were that 2 residents were opposed to the extension, with 4 residents not responding to the expression of interest letter.

**4.5.2 Parkdale Drive from the intersection of Leslie Vale Road through to 102 Parkdale Drive.**

Responses from the expression of interest letters were that 7 residents were in support of the extension, 7 residents were opposed to the extension, with 7 residents not responding to the expression of interest letter.

**4.5.3 Bullock Drive.**

Responses from the expression of interest letters were that 7 residents were in support of the extension, 5 residents were opposed to the extension, with 6 residents not responding to the expression of interest letter.

**4.5.4 Wolfes Road from the intersection of Leslie Vale Road through to the intersection of Jindalee Drive.**

Responses from the expression of interest letters were that 2 residents were in support of the extension, 2 residents were opposed to the extension, with 10 residents not responding to the expression of interest letter.

**4.6 The following comments were common amongst the responses:**

4.6.1 The service is not required.

4.6.2 The service is too expensive.

4.6.3 Our driveway is too long.

4.6.4 Our driveway is too steep to move the bins.

**5. FINANCE**

5.1 There is no cost to Council if the collection service is not extended.

5.2 If Council decided to extend the service, the costs of the Council's kerbside collection services would be recovered by service charges and factored in Council's annual budget.

**6. ENVIRONMENT**

6.1 There are the following environmental benefits associated with expanding the kerbside collection service:

6.1.1 Improved recycling rates and diversion of product from landfill

6.1.2 Discouraging illegal littering.

**7. COMMUNICATION AND CONSULTATION**

7.1 Consultation was undertaken with the current kerbside collection contractor to identify the suitability of the proposed extension routes for the collection vehicles and the kerbside collection service.

7.2 Consultation has been undertaken with the affected residents on the proposed extension areas and Council's resolution on this matter will be further communicated to them.

**8. RISK**

8.1 There is ultimately a risk of negative feedback from some residents who will not be happy with any decision that Council makes as to whether to extend the service or not.

**9. CONCLUSION**

- 9.1 The extension of service is adjacent to the current collection service boundary, suitable for the collection vehicles and as such can be considered.
- 9.2 There is limited support among surveyed residents for extending the kerbside service to the Leslie Vale area. Of the residents surveyed, 27 did not respond, 16 indicated they do not support the extension for varied reasons, and 16 indicated they would like the extension to proceed.

**10. RECOMMENDATION**

That Council does not extend the kerbside collection service to the Leslie Vale area surveyed.

**ATTACHMENTS**

Nil

Public Copy

**15.2 KINGBOROUGH BICYCLE ADVISORY COMMITTEE MINUTES 6 FEBRUARY 2026**

**File Number:** 28.258

**Author:** Anthony Verdouw, Executive Officer Engineering Services

**Authoriser:** Craig Mackey, Director Engineering Services

**Strategic Plan Reference**

Key Priority Area: 1. Helping our community stay safe, healthy and well connected.

Strategic Outcome: 1.1 A Council that purposefully engages with its community.

**1. PURPOSE**

- 1.1 The purpose of this report is to note the Minutes of the meeting of the Kingborough Bicycle Advisory Committee (KBAC) held on 6 February 2026 and to present a motion adopted by the Committee.

**2. BACKGROUND**

- 2.1 KBAC met on 6 February 2026 and the Minutes of the meeting are attached to this report.

**3. STATUTORY REQUIREMENTS**

- 3.1 KBAC is a committee appointed by Council in accordance with section 24 of the *Local Government Act 1993*.

**4. DISCUSSION**

- 4.1 At the 6 February 2026 KBAC meeting the Committee adopted a motion that:

*Council write to the Minister for Infrastructure and Transport regarding an update on the Channel Highway Corridor Study 2020 recommendations and advocate for upgraded sealed shoulders along the Channel Highway between Margate and Kingston as recommended in the Study.*

- 4.2 The Department of State Growth released the Channel Highway Corridor Study Report (Algona Road to Sandfly Road) in September 2020.

- 4.3 The report outlined a number of short list options for future improvements along the highway corridor. Shoulder widening on the Channel Highway to a consistent 1.5m wide standard was listed as a high priority safety opportunity.

- 4.4 KBAC have long raised concerns about cyclist safety along the Channel Highway corridor between Margate and Huntingfield and would like to see short-listed opportunities raised in the Corridor Study progressed or scheduled for implementation.

**5. FINANCE**

- 5.1 There are no direct financial implications associated with the matters raised by the Committee.

**6. ENVIRONMENT**

- 6.1 There are no immediate environmental considerations associated with the matters raised by the Committee.

**7. COMMUNICATION AND CONSULTATION**

7.1 KBAC will be consulted on Council's resolutions regarding the matters that have been raised.

**8. RISK**

8.1 There are no apparent risks associated with the matters raised by the Committee.

**9. CONCLUSION**

9.1 The Kingborough Bicycle Advisory Committee met on 6 February 2026.

9.2 The Committee adopted one motion for Council consideration.

**10. RECOMMENDATION**

That Council:

- (a) Notes the minutes of the Kingborough Bicycle Advisory Committee.
- (b) Writes to the Minister for Infrastructure and Transport regarding progress on the Channel Highway Corridor Study recommendations and advocates for upgraded sealed shoulders along the highway between Margate and Kingston as recommended in the Study.

**ATTACHMENTS**

1. **KBAC Minutes 6 February 2026**

Public Copy

# Minutes

## Kingborough Bicycle Advisory Committee

Meeting No. 2026-1

Friday 6 February 2026

The logo for Kingborough, featuring the word "Kingborough" in a serif font with a green swoosh underneath.

Minutes for a Meeting of the Kingborough Bicycle Advisory Committee held at the Kingborough Council Chambers, Kingston, on Friday 6 February 2026 at 9:00am.

**PRESENT**

|  |                     | <b>PRESENT</b> | <b>APOLOGY</b> |
|--|---------------------|----------------|----------------|
| Chairperson                            | Cr Amanda Midgley   | ✓              |                |
| Deputy Chairperson                     | Cr David Bain       |                | X              |
| <b>Members:</b>                        | David Bonny         | ✓              |                |
|  | Karen Camino        | ✓              |                |
|  | Damian Devlin       |                | X              |
|  | Mark Donnellon      | ✓              |                |
|  | Michael Holloway    | ✓              |                |
|  | Eddie Jager         |                | X              |
|  | Kelvin Lewis        | ✓              |                |
|  | Rob Sheers          | ✓              |                |
| Cycling South                          | Mary McParland      | ✓              |                |
| Bicycle Network                        | Alison Hetherington | ✓              |                |
| <b>Council Officers In Attendance:</b> |                     |                |                |
| Executive Officer                      | Anthony Verdouw     | ✓              |                |
| Administration Officer                 | Aleira Chalker      |                | X              |
| Recreation Officer                     | Su Spratt           |                | X              |
| Director Engineering Services          | Craig Mackey        | ✓              |                |
| <b>Other Attendees:</b>                |                     |                |                |
| Councillor                             | Cr Flora Fox        | ✓              |                |

**ACKNOWLEDGEMENT OF TRADITIONAL OWNERS**

The Chairperson acknowledged and paid respect to the Tasmanian Aboriginal Community as the traditional and original owners and continuing custodians of the land on which we meet and acknowledged elders past and present.

**LEAVE OF ABSENCE**

**DECLARATIONS OF INTEREST**

**CONFIRMATION OF MINUTES**

MOVED: Mark Donnellon  
 SECONDED: Mary McParland

That the Minutes of the Committee meeting held on Friday 7 November 2025, as circulated be confirmed.

**GENERAL BUSINESS**

1. Motion to Council and Endorsement of new Committee Member

Resolution from Council at the meeting held on 1 December 2025 outlined below:

|   |
|---|
| <p><b>15.6 KINGBOROUGH BICYCLE ADVISORY COMMITTEE MINUTES 7 NOVEMBER 2025</b><br/>                 Moved: Cr Amanda Midgley<br/>                 Seconded: Cr Flora Fox</p> <p>That Council:</p> <p>(a) Notes the minutes of the Kingborough Bicycle Advisory Committee.</p> <p>(b) Writes to the Minister for Transport regarding the status of the State's <i>Walk, Wheel Ride Strategy</i> and ongoing Active Transport funding, noting the benefits previous State funding has provided to implement the Greater Hobart Cycling Plan.</p> <p>(c) Endorse Karen Camino as a community representative on the Committee to the completion of the 2025-2026 KBAC term.</p> <p style="text-align: right;"><b>CARRIED</b></p> |
|---|

Council's resolution was noted and the Chair welcomed Karen Camino to the committee.

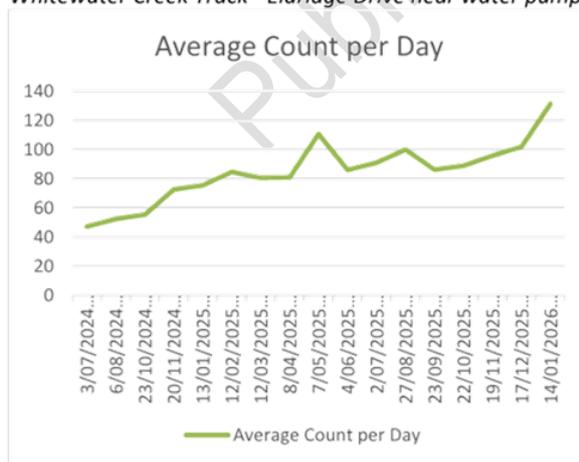
2. Cycling Strategy – Strategic and Advocacy Actions

Review and discussion of Strategic and Advocacy Actions.

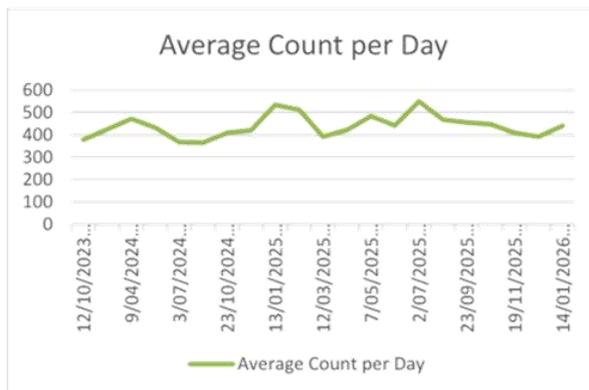
**Action 3 – Track counters:**

- Permanent counter installed on Whitewater Track. The others will be moved around to various tracks regularly.

*Whitewater Creek Track - Eldridge Drive near water pump:*



*Whitewater Creek Track – Outlet underpass:*



**Action 5 & Action 12** – Road safety audits to eliminate hazards for cycling – maintain existing network.

KBAC members are invited to report on specific issues and can use Snap Send Solve or raise service requests out of session.

**Action 7 & 8** – Promote safe cycling and places to ride in Kingborough.

**Action 10** – Wayfinding strategy – signage and mapping.

3. Cycling Strategy – Infrastructure Actions Review

Review and discussion of Infrastructure Actions.

**DSG Projects:**

*Council staff will consolidate a list of items for further discussion and updates with DSG staff. Craig Mackey to liaise with Ben Goodsir of DSG to facilitate discussions.*

**Action 15** – Channel Hwy Sealed Shoulders (Huntingfield to Margate section)

- Noted by the Committee as a high priority safety issue for commuter and recreational cyclists – continue advocacy.
- Noted that Council sent letters to State Growth in 2021 regarding sealed shoulders and a future shared path.

**MOTION:**

That Council write to the Minister for Infrastructure and Transport regarding an update on the Channel Highway Corridor Study 2020 recommendations and advocate for upgraded sealed shoulders along the Channel Highway between Margate and Kingston as recommended in the Study.

Moved: Rob Sheers

Seconded: Mark Donnellon

CARRIED

- Noted that a pedestrian and cyclist crossing option of North West Bay River was also a high priority opportunity in the Channel Highway Corridor Study.

**Action 17** – Channel Trail – Margate to Huntingfield.

- Update required from DSG – a consultant was engaged to further define alignment and progress concept design.

*Question for DSG if project update could be provided and if concept design could be shared with the Committee.*

**Action 23** – Huntingfield to Kingston CBD – Channel Hwy

*DSG update required on this item noting that concept plans have previously been prepared.*

**Council Projects:**

**Action 18** – Algona Road Shared Path

- Feasibility study and route options report complete. Remaining project funds to be used to undertake preliminary survey and design on priority section(s) in coming months.

*DSG update required on the status of the Algona Road Rapid transit bus lanes concept.*

**Action 20** – Huntingfield Park & Ride Connections

- Sirius Drive to Park and Ride connection – design work scheduled
- Hollyhock to Park and Ride path connection – design work scheduled

**Action 21** – Channel Hwy Taroona - bike Lane upgrades

- Designs are complete. Development Application has been submitted, and the DA plans have been advertised.

**Action 24** – Roslyn Ave design

- Concept plans for uphill bike lanes and cost estimate presented to the Committee. Project has site constraints and a high-cost estimate.
- Committee have requested that the project remains on the 5-year plan as it is a high-priority high-use road cycling link.

**Action 26** – Channel Trail – Snug to Lower Snug

- Old Station Rd and Davies Rd, Lower Snug – shared path – project nearly complete with fencing to be installed soon.

*Noted the narrow pinch-point pedestrian footpath and low guard rail on the two bridges over Snug River at the end of the Margate to Snug Path. Council officers advised they are not aware of any plans to upgrade or widen these paths and advised the bridges are DSG assets.*

- Channel Highway, Snug footpath – between Sunsail St and Snug Tiers Rd – project complete.

**Action 28** – Sandfly Road Sealed Shoulders

- Sealed shoulders on Sandfly Road focussing on priority uphill sections.
- Site works underway and progressing over February and March 2026. Site inspection to determine additional section of works to expend grant funding to occur in next fortnight.

4. Cycling South Report

Cycling South update on activities:

- Tasman Bridge Eastern shore connection pathways progressing.
- City of Hobart CBD bike lanes assessment released.
- Derwent Valley Council investigating Maydena rail trail – concept is now out for consultation.

**CORRESPONDENCE**

Outgoing:

Letter to Minister of Infrastructure and Transport – *Walk Wheel Ride Strategy and Active Transport Funding* – 12 December 2025

**OTHER BUSINESS**

5. Update from Su Sprott – Tracks and Trails

- North West Bay River Trail Stage 2 is now open and suitable for mountain bikes. Includes 2 river crossings and 1 ford. A 3km section one way.
- Cottage Road Track Upgrade due to commence first or second week in March. Concreting the steep section through to Cottage Rd - 2m wide.

6. Bike week 1-8 March 2026

Bike week from 1-8 March 2026 was noted with a number of social rides, activities and events planned across the week.

**MATTERS OF GENERAL INTEREST**

**NEXT MEETING**

10 April 2026 – 9am at Council Chambers

**CLOSURE**

Meeting closed 10.04am

## Kingborough Bicycle Advisory Committee

### Meeting Dates for 2026

*Note meetings are held bi-monthly on Fridays at 9:00am  
in the Council Chambers, Kingston*

6 February 2026  
10 April 2026  
5 June 2026  
7 August 2026  
2 October 2026  
4 December 2026

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**15.3 APPENDICES****RECOMMENDATION**

That the Appendices attached to the Agenda be received and noted.

**16 NOTICES OF MOTION**

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At the time the Agenda was compiled there were no Notices of Motion received.

**17 CONFIRMATION OF ITEMS TO BE DEALT WITH IN CLOSED SESSION**

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**RECOMMENDATION**

That in accordance with the *Local Government (Meeting Procedures) Regulations 2025* Council, by absolute majority, move into closed session to consider the following items:

**Confirmation of Minutes**

Regulation 40(6) *At the next closed meeting, the minutes of the previous closed meeting, after any necessary correction, are to be confirmed as the true record by the council or council committee and signed by the chairperson of the closed meeting.*

**Applications for Leave of Absence**

Regulation 17(2)(i) *applications by councillors for a leave of absence*

**Taroona Bowls & Community Club Inc Lease**

Regulation 17(2)(g) *proposals for the council to acquire land or an interest in land or for the disposal of land.*

**CEO's Performance Review**

Regulation 17(2)(a) *personnel matters, including complaints against an employee of the council.*

In accordance with the Kingborough Council *Meetings Audio Recording Guidelines Policy*, recording of the open session of the meeting will now cease.

Open Session of Council adjourned at

**OPEN SESSION ADJOURNS**

**OPEN SESSION RESUMES**

**RECOMMENDATION**

The Closed Session of Council having met and dealt with its business resolves to report that it has determined the following:

| Item                                      | Decision |
|---|----------|
| Confirmation of Minutes                   |          |
| Applications for Leave of Absence         |          |
| Taroonna Bowls & Community Club Inc Lease |          |
| CEO's Performance Review                  |          |

**CLOSURE**

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**APPENDICES**

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- A Mayor's Activities 8 January 2026 to 25 February 2026
- B Environmental Services Activity Report - Quarter Two

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**A MAYOR'S ACTIVITIES 8 JANUARY 2026 TO 25 FEBRUARY 2026**

| <b>DATE</b> | <b>LOCATION</b> | <b>ITEM</b>   |
|-------------|-----------------|---|
| 8 January   | Blackmans Bay   | Attended funeral of Mr Rod Street   |
| 12 January  | Civic Centre    | Met with Lisa Bennetto, new Executive Officer for Greater Hobart Mayors   |
|             | Civic Centre    | Chaired Council workshop on operational updates.  |
| 14 January  | Hobart          | Attended Hobart International at the invitation of Tennis Tasmania's President.   |
| 19 January  | Civic Centre    | Met with Paul Wales, President of Kingston and Surrounds Neighbourhood Watch, with the CEO.   |
|             | Civic Centre    | Chaired Council Meeting   |
| 21 January  | Civic Centre    | Met with Mrs J Thompson re Kingston Beach traffic management  |
| 23 January  | Hobart          | Met with Minister for Infrastructure, Kerry Vincent, with the CEO and representatives of FLAG and Lesley Vale Community Association |
| 27 January  | Civic Centre    | Chaired workshop on AFL HPC   |
| 28 January  | Kingston        | Presided over citizenship ceremony.   |
|             | Civic Centre    | Met with Stevie Moran re Christmas Carols in Kingborough  |
| 29 January  | Kingston        | Attended Kingborough Land Users Body meeting.   |
| 2 February  | Civic Centre    | Meeting with Annette Moran, Ambulance Tasmania re: Proposed Snug Ambulance Station.   |
|             | Civic Centre    | Chaired Council Meeting   |
| 4 February  | Civic Centre    | Presented induction for new DIAAC members   |
|             | Kingston        | Guest speaker at Kingston and Surrounds Neighbourhood Watch meeting.  |
| 7 February  | Middleton       | Judged children's fashion parade at Middleton Day on the Green  |
| 11 February | Adelaide        | Attended ALGA Board Strategic Planning Day  |
| 12 February | Adelaide        | Attended ALGA Board Strategic Planning Session and ALGA Board meeting   |
| 13 February | Civic Centre    | Met with Mr Shane Harris, Bruny Island Bowls Club, with the Acting CEO  |
| 16 February | Civic Centre    | Chaired Council Meeting   |
| 18 February | Kingston        | Media event for LGAT re short stay accommodation  |
|             | Online          | Future Populations Network webinar on using ABS data  |
|             | Online          | LGAT Webinar on understanding TasCAT.   |
|             | Civic Centre    | Chaired DIAAC meeting   |
|             | Kingston        | Met with Mark Mugnaio, CEO of RACT re proposed bushfire hazards pilot project, with the CEO   |

| DATE        | LOCATION      | ITEM  |
|-------------|---------------|---|
| 20 February | Kingston      | Opening of Kingston Urgent Care Clinic  |
|             | Online        | Attended Australian Local Government Women's Association General meeting  |
| 21 February | Kingston      | Present certificates at the Great Kingborough Bake Off as part of Pride Month celebrations                      |
| 23 February | Online        | Attended LGAT General Management Committee Meeting  |
|             | Civic Centre  | Chaired workshop on asset management & Copping C Cell   |
| 24 February | Hobart        | Attended Greater Hobart Mayors Forum, with the CEO  |
| 25 February | Blackmans Bay | Met with Principal of Blackmans Bay Primary School re Rosie's Reading Tasmania program                          |
|             | Civic Centre  | Met with Alison Rogers, President of KLUB, with the CEO and Director of Environment, Development and Community. |
|             | Civic Centre  | Met with Minister Peter Adlem, St Clement's Church, Kingston  |

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**B ENVIRONMENTAL SERVICES ACTIVITY REPORT - QUARTER TWO**

**File Number: 8.76**

**Author: Liz Quinn, Manager Environmental Services**

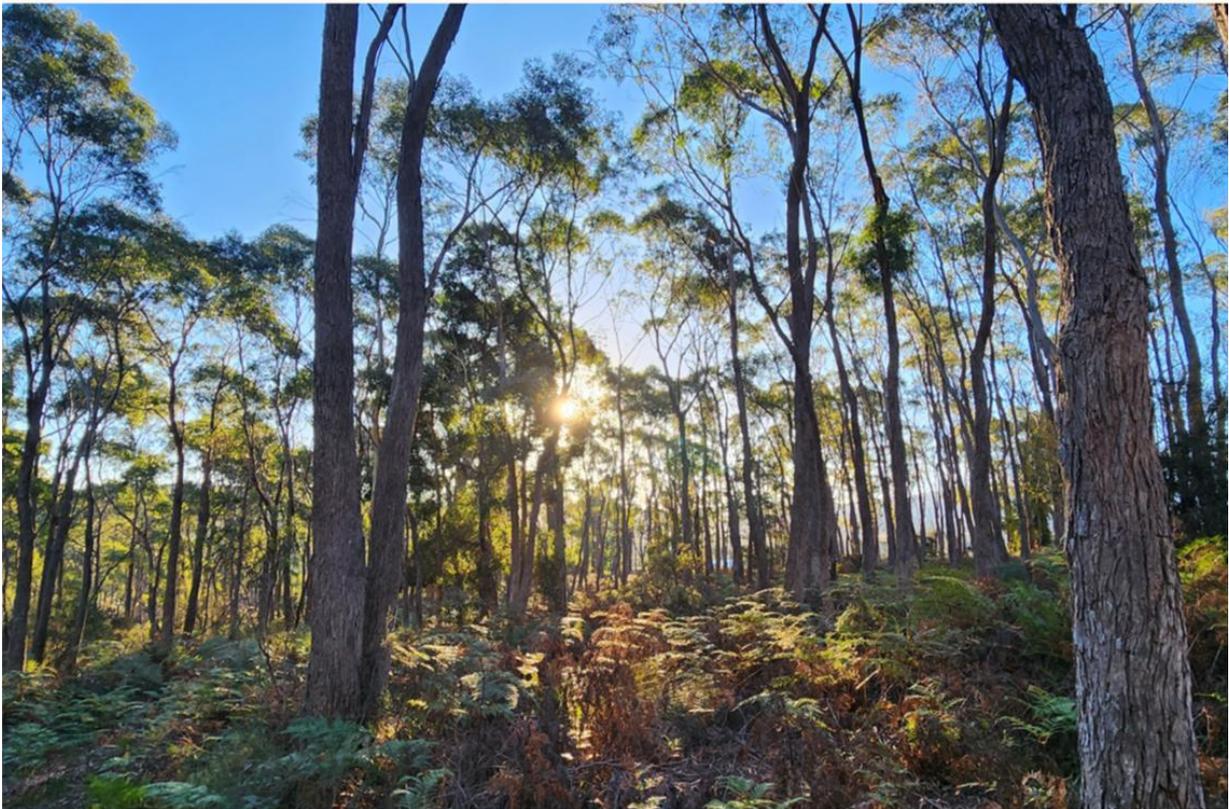
**Authoriser: Deleeze Chetcuti, Acting Chief Executive Officer**

# ENVIRONMENTAL SERVICES QUARTER TWO REPORT



## 2025 -2026

ENVIRONMENTAL SERVICES QUARTERLY REPORT OCT-DEC 2025



ENVIRONMENTAL SERVICES - QUARTERLY REPORT OCT-DEC 2025

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# Environmental Health



## RECREATIONAL WATER

### Beaches

Mandatory monitoring under the *Public Health Act 1997* for recreational beaches is conducted between 1 December to 31 March.

During December, two samples returned minor non-compliant results, both at Blackmans Bay South. All other beach samples taken during this period were compliant with the relevant guidelines.

### Water Quality Investigation at Blackmans Bay Beach - South

Council engaged Marine Solutions in 2025 to undertake a two-stage investigation into water quality at Blackmans Bay South, following its downgrade to a poor rating at the end of the 2024–25 recreational monitoring season. Stage One (completed June 2025) reviewed previous Council investigations and included additional beach and stormwater outfall sampling conducted over the past five years.

#### Key findings:

- Contamination is likely linked to stormwater originating from the residential area above the beach.
- Ageing water and sewer infrastructure may allow leakage into stormwater systems.

Stage Two involved field studies such as drogue tests, rainfall event sampling, and stormwater flow tracking. This work was undertaken in December 2025 and included the use of fluorescein, an environmentally friendly dye, to track stormwater movement from three outfalls at Blackmans Bay South. These tests aim to better understand how stormwater enters and moves within the bay.

Stage Two is scheduled for completion in early 2026, with findings to be analysed and reported to Council by Marine Solutions by March 2026.



Dye testing at Blackmans Bay Beach



## PRIVATE WATER SUPPLIERS

There are 5 registered private water suppliers (PWS) in Kingborough – these are commercial sites where water is supplied to a large number of people from a private water source (eg. Rainwater tanks). The Environmental Health team conducts quarterly bacteriological sampling and yearly metal sampling on behalf of 2 of the 5 registered PWS. The remaining 3 registered PWS are school facilities and they undertake their own mandatory sampling with results monitored directly by Department of Health.

TABLE 1: Private Water Supplier regulation 1 October to 31 December 2025

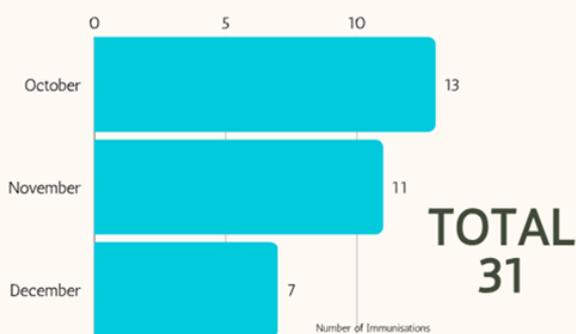
| Activity                           | Number | Outcome  |
|------------------------------------|--------|--|
| Renewals of Private Water Supplies | 5      |  |
| Routine Microbial Samples Taken    | 3      | 2 non-compliant; advisory (boil water) signage required until compliant resample result received. Resample taken compliant |
| Metal Sampling                     | 0      | No metal samples conducted this quarter  |

## IMMUNISATIONS

Council’s Environmental Health team delivers an immunisation program offering vaccines listed under the National Immunisation Program to community members. This program includes both a school-based program and a monthly community immunisation clinic.

During the Q2 reporting period (October–December), no school-based sessions were held as the 2025 school program had concluded. Council continued to run monthly community clinics, which were accessed by 31 community members. This program is an important public health initiative aimed at protecting the community from vaccine-preventable diseases.

CHART 1:  
Number of immunisations administered at community clinics between 1 October to 31 December 2025.



## FOOD SAFETY

As at 31 December 2025, there were 276 registered food businesses (both mobile and fixed) operating within the municipality. Under the *Food Act 2003*,

Council's Environmental Health Officers are required to inspect registered food premises to ensure compliance with the Food Safety Standards.

TABLE 2: Food Safety Regulation - 1 October to 31 December 2025

| Category                                       | Number |
|--|--------|
| Registered Food Businesses (as of 31 May 2024) | 276    |
| Temporary Food Applications Received           | 54     |
| Inspections Conducted                          | 32     |
| Notifiable Disease Interviews Conducted        | 0      |
| Enforcement Action Taken (infringement)        | 0      |

## COMMUNITY REQUESTS/COMPLAINTS

During the reporting period, Environmental Health received, investigated, and resolved 43 community concerns/enquiries. Three operations remain under active Environmental Protection Notices (EPNs).

TABLE 3: Community request and complaints overview

| Activity                                 | Number |
|--|--------|
| Community Requests/Complaints Received   | 43     |
| Abatement notices issued                 | 0      |
| Current Environmental Protection Notices | 3      |



Carrying out noise monitoring

# Natural Areas and Biodiversity

## HIGHLIGHTS

- Over 290 participants engaged across NAB education and community events.
- Local Bioblitz achieves state record for species and observation counts.
- Over 10800 weeds controlled across 60 reserves.
- Landcare volunteers worked across 23 Council reserves.

## INTERPRETATION AND EDUCATION PROGRAM

Oct-Dec 2025 Program Snapshot



290

People engaged



6

Reserves in focus



5

Presentations delivered



23

Guided activities

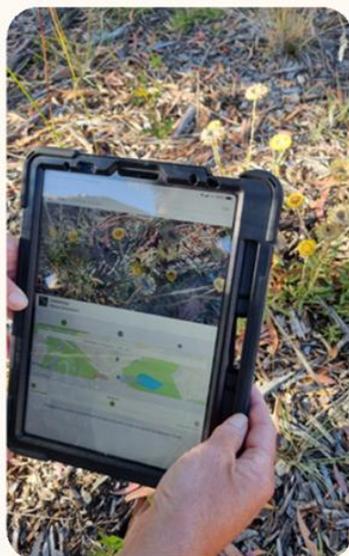


4

Citizen science events



Participants explore Wingara Gully during a guided Seasonal Walk.



Citizen Science events work to build knowledge and confidence in data collection methods.

### Community capacity building

Spring provides an ideal time to be outside and facilitate a range of vibrant community events. Citizen science was a focus this reporting period, with national and international initiatives used to energise and upskill members of the community.

Highlights include:

- Several hands-on events that provided a chance to deepen participants knowledge of local biodiversity while also contributing valuable data to broader monitoring efforts.
- Two well attended guided bushwalks provided an opportunity to enjoy and learn about nature.
- Hawthorn Reserve and its stunning biodiversity was showcased during the Aussie Bird Count and the Great Southern Bioblitz. One local resident has been credited with recording the highest number of species and observations within the state during the 3 day event. This incredible achievement reflects both the ecological values found in the reserve and the strength of local engagement.
- Kingborough Landcare members joined NAB staff for iNaturalist training, building confidence in the use of digital tools to support their important on-ground work.

### Community and School Presentations

The 2025 Nature Festival returned to the Community Hub, providing community members and visitors opportunities to get involved and learn about nature. The panel discussion on habitat gardening, facilitated by NAB staff, brought together the perspectives of wildlife and native plant experts, landscapers and a local aboriginal Landcare member. The discussion provided valuable insights and was reported to be a highlight of the festival.

Staff also connected with several local school groups to explore the values of local reserves and the real world challenges of managing natural environments.

### LANDCARE PROGRAM

Highlights include:

- Kingston Beach Coastcare and Council partnered to deliver two plant ID and iNaturalist workshops. Funded by Landcare Tasmania, 25 participants honed their flora and citizen science skills, building on the knowledge within our Landcare network and community stewardship of our natural areas.
- A guided 'Walk and Do' event in Wingarra Gully saw local Landcare members participate in flora and fauna identification training. The group also participated in a working bee to trial nature based solutions to combat tunnel erosion.
- Kingston Beach Coastcare supported minor erosion management works at the Tyndal Beach main access, helping to reduce further impacts over the peak summer holiday period.



📷 Landcare members help install trial tunnel erosion mitigation (above and below).



📷 Landcare community members enjoyed building on their flora identification with the guidance of a local ecologist and Council staff.

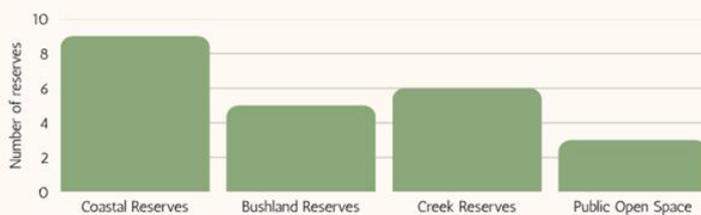


CHART 2: The number and type of reserves that benefited from Landcare Group conservation activities this reporting period.

## KINGBOROUGH ENVIRONMENTAL FUND

The biannual financial update was undertaken, with the total biodiversity offset contributions received into the KEF now \$1,424,293.91.

*Highlights include:*

- An agreement for the new private land reserve at Alonnah has been drafted for landowner review, with feedback also received from Natural Resources & Environment Tasmania. This reserve will be the 7<sup>th</sup> secured under the KEF and specifically targets the management and conservation of priority swift parrot habitat.
- A prospective KEF reserve containing critically endangered Black gum forest is being explored for inclusion in the program. Both Council and the landowner are hopeful that this North Bruny property will contribute to one of the most important remaining targets in the KEF.
- Following the release of the KEF information clip last year, work has commenced on a second film in this educational series. This short film will explore both the achievements of the KEF and the experiences of landowners who have secured reserves.



 Eucalyptus ovata (blackgum) woodland located on the potential Bruny Island private reserve.

## COASTAL MANAGEMENT

### Tyndall Beach

Staff have completed a review of the Tyndall Beach coastal stabilisation project, incorporating 5 years of site monitoring into the assessment. The work undertaken in 2020-21 has been largely successful and demonstrates how "soft" approaches including brush walls, coir mesh and native revegetation, can be used effectively in coastal erosion protection.

### Coningham Beach

Staff, supported by the Coningham and Greater Snug Landcare Group, undertook removal and repositioning of displaced sandbags in preparation for the busy summer season. An assessment will be undertaken to determine the next phase of erosion protection for the site.



 Stage 1 of the Tyndal beach project trialed a combination of products and techniques.



 Displaced and buried sandbags were repositioned ensure good access for the popular Coningham Beach.

Tree Assessment

TABLE 4: Request for tree removal assessments for the reporting period

| Request for tree removal assessment and associated outcomes July – September 2025 |               |   |
|---|---------------|---|
| Outcome   | Outcome rates | Description of outcome  |
| Exempt  | 13            | Proposed tree work would meet an exemption within the planning scheme, such as safety, boundary fencing, or proximity to approved infrastructure, and no consent is required under an existing covenant or Part 5 Agreement |
| No Permit Required  | 1             | A permit would not be required under the planning scheme, and no consent is required under a covenant or Part 5 Agreement   |
| Further Information Requested   | 5             | Available information is insufficient to determine if proposed tree work would meet an exemption  |
| Development Application required  | 1             | Proposed tree work would not meet an exemption and planning approval is required  |
| Multiple Outcomes   | 2             | Request involves multiple trees and differing outcomes for the individual proposed removals/modifications   |
| Not Valid   | 3             | Often a result where vegetation is located on land not owned by the applicant   |
| No longer required  | 1             | Often a result of where landowners seek advice about a tree and decide they no longer wish to pursue modification or removal  |
| Total requests received   | 26            |   |

THREATENED SPECIES AND WILDLIFE



**Ammonite Snail**

NAB staff attended a workshop held by NRM South, focusing on the development of management guidelines for the critically endangered ammonite snail (*Ammoniropa vigens*). Populations of the snail have been found in the coastal gullies of Taroona and Tinderbox.

 The small ammonite snail is particularly susceptible to habitat loss and degradation.

**Little Penguins**

Tinderbox landholders facilitated access to an important penguin colony to conduct annual surveys. Population numbers and breeding success was consistent with last year’s observations, and provided positive news as other local colonies have experienced declines.

The Derwent Estuary Penguin Advisory group meets in March to review the 2025-26 breeding season and plan colony maintenance activities.



 Penguin surveys undertaken in Tinderbox.

### STRATEGIC WEED MANAGEMENT

- Strategic weed control was conducted across 60 reserves this quarter, targeting both declared and environmental weeds.
- Weed notices were issued to landowners where required, encouraging pro-active weed management, providing technical advice and highlighting obligations under legislation.
- As part of protecting and improving biodiversity and habitat for local wildlife, Strategic Weeds Officers have been undertaking weed control in newly planted revegetation sites.



CHART 3: Number and type of weeds controlled by Strategic Weed Officers during the Oct - Dec 2025 reporting period.



Strategic Weed Officers attend to pampas grass control in Howden



Staff undertake another round of foxglove control in the Snug Tiers area.



Maintenance of revegetation and habitat plantings, such as Harts Hill Reserve, forms part of the Strategic Weed Program.

TABLE 5: Weed notices sent to landowners during the reporting period

| Category of Weed       | Species          | Type of Notification | Number Sent |
|------------------------|------------------|----------------------|-------------|
| Class A Declared Weeds | Pampas Grass     | Advice notice        | 1           |
|                        | Boneseed         | Advice notice        | 2           |
|                        | Paterson's curse | Advice notice        | 2           |
| Class B Declared Weeds | Spanish Heath    | Advice notice        | 2           |
|                        | Blackberry       | Advice notice        | 2           |

# Climate Change

## KEY HIGHLIGHTS

- Results from the community priorities survey for the Snug Climate Change Adaptation Options Project showed strong appetite for action.
- Council's Climate Change Risk Assessment has been finalised.
- Council was successful in a joint application for a Disaster Ready Fund grant project of \$987k to support coastal hazard risk reduction.
- An Emissions Inventory Report was finalised for Council operations in 2024-2025.

## GOVERNANCE ACTIVITIES

### RISK MANAGEMENT

Council completed its first dedicated **Climate Change Risk Assessment** in December. This assessment examined how projected 2050 climate scenarios may impact Council operations and assets, focusing on five key hazard areas:

- Heightened bushfire conditions
- Increased rainfall and riverine flooding
- Prolonged and more intense heatwaves
- Sea level rise
- Combined impacts.

The assessment identified **31 climate-related risks**, each with mitigation actions. This forms part of an ongoing risk management program and will be updated annually to ensure a consistent, organisation-wide understanding of climate exposure and resilience.

Council continues to advance the **Embedding Climate Risk into Council Finances project**, delivered in partnership with the Local Government Association of Tasmania (LGAT). The project is evaluating how effectively Council's financial systems and long-term planning processes account for climate risks.

Key outcomes expected in February 2026 include a prioritised action plan and a practical toolkit to help embed climate risk considerations across budgeting, asset management, and financial decision-making.

### ENGAGEMENT AND COLLABORATION

Council officers are contributing to a regional review of the Regional Climate Change Initiative (RCCI), a collaborative network of climate practitioners from the twelve southern Tasmanian councils. This review will strengthen how councils share expertise, collaborate on projects, and work together on climate challenges affecting the region

The Climate Change Program provided support for the Net Zero Channel group to host a community event with Dr Saul Griffith, Chief Scientist at Rewiring Australia. This event, The Faster and Fairer Tour, explored the electricity transition faced by the Australian electricity network and community electrification. There was an excellent turnout, with the Community Hub at capacity.

## ADAPTATION ACTIVITIES

### SNUG CLIMATE CHANGE ADAPTATION OPTIONS (CCAO) PROJECT



 Snug Beach vegetation dieback

The Snug CCAO Project has analysed and publicly shared the results of the Coastal Change on Snug's Foreshore Survey which received 90 responses. The survey explored the community's usage and value of the coastal area and assets, their experience and perceived urgency of coastal hazards, and their priorities for future coastal adaptation.

The results demonstrated a strong perceived importance for action on coastal hazards (84% of respondents), with the majority having seen physical changes to the shoreline in their time (41% had witnessed minor changes, 34% had witnessed major changes).

The survey provided information on which assets the community most want protected from coastal hazards in the foreshore area. This information is being considered as part of an internal workshop series with staff to explore the risk and vulnerability of these coastal assets, and to then develop potential adaptation options. The first of these four workshops was held in December 2025, with the remaining to be held between February and March 2026. This project is funded by the Natural Disaster Risk Reduction Grant Program.

### COASTAL HAZARDS RISK REDUCTION PROGRAM

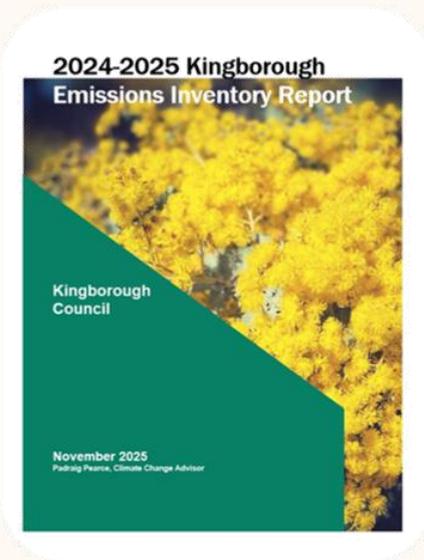
Council has been successful in a joint bid for a \$987,236 project funded by the Disaster Ready Fund to deliver a transformative approach to managing Tasmania's growing coastal vulnerability. Led in collaboration with Huon Valley Council, Brighton Council and the Local Government Association of Tasmania, this project aims to establish a coordinated statewide framework that maximises resource efficiency while delivering consistent, high-quality outcomes for coastal communities. Key project outcomes will include:

- Supporting standardised mapping of coastal investigation areas across participating councils.
- Develop standardised planning tools and development controls for coastal hazard risks.
- Creation of a framework for community engagement on coastal hazards.

The project involves participation of 16 of the 21 coastal councils and is a significant opportunity to strengthen action on coastal hazards to build resilience. The project is planned to commence in early 2026 and will run until 2028.

## MITIGATION ACTIVITIES

### EMISSIONS INVENTORY REPORT



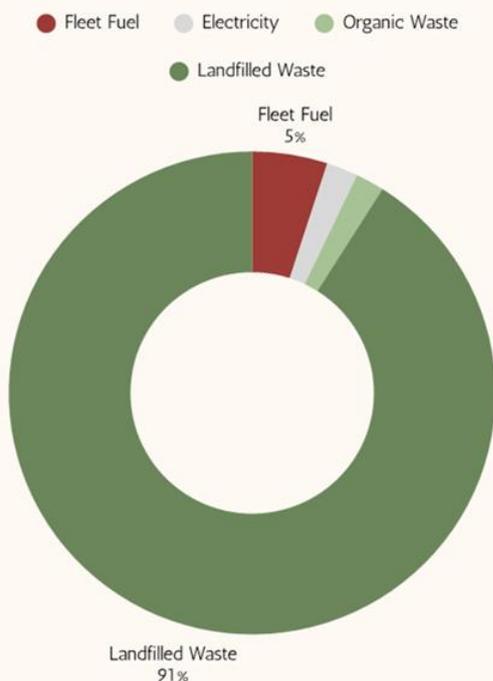
A detailed Emissions Inventory Report for Council operations was created for the 2024-2025 financial year. The report explores trends in emissions across Council for:

- Vehicle and plant fuel use
- Gas
- Electricity
- Waste and organics

There has been a reduction in emissions from all areas, with the exception of electricity, which rose due to both increased usage and a higher emissions factor for Tasmania that year. An emissions inventory report will be generated annually to support targeted actions on emissions reduction.

**NOTE:** The 2024-2025 Annual Report included a reduction of 1% corporate emissions to 12,705 tonnes of CO<sub>2</sub> equivalent (tCO<sub>2</sub>e), a reduction of 81 tCO<sub>2</sub>e. Additional information was received after the report was published from Southern Waste Solutions regarding amended data for emissions capture. Council corporate emissions instead reduced by 4% from 2023-2024 to 12,237 tCO<sub>2</sub>e (a reduction of 558 tCO<sub>2</sub>e).

Emissions Sources



Emissions source from Council operations by percentage

### MARGATE ELECTRIC VEHICLE CHARGING STATION

A contractor has been appointed to construct a new public electric vehicle charging station, funded by a \$100,000 election commitment from the Tasmanian Government. Following a competitive process, Energy ROI has been selected as the preferred operator and will install the charger in Margate, improving access to EV infrastructure south of Kingston.

# Bushfire Management



40 hours dedicated to community enquiries about bushfire risk in November

The start of the bushfire season brings with it an increase in activity for Council's Bushfire Program. The increase in bushfire risk marks the implementation of the fuel break maintenance contract, site inspections, stakeholder meetings, and greater community interest and requests for information about bushfire risk.

The Fuel Break Contract is one of the main ways Council limits the potential impact of a bushfire travelling from one of our reserves onto private land. The first cut of the fuel breaks and hazard management areas is timed for the start of October to align with the bushfire season. The Tasmanian Vegetation Fire Management Policy guides this risk-based approach Council implements on Council land.

## FUEL BREAK WIDENING AT TAROONA

The grant deed for the Tasmania Fire Service Fuel Break Grant is in the final stages of negotiation with works planned to be completed at Cartwright Reserve in Tarooma this financial year.



 Fuel break adjacent to Council Reserve in Tarooma